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# FINAL AIRPORT MASTER PLAN

## BUSSELTON MARGARET RIVER AIRPORT

*Prepared for City of Busselton*



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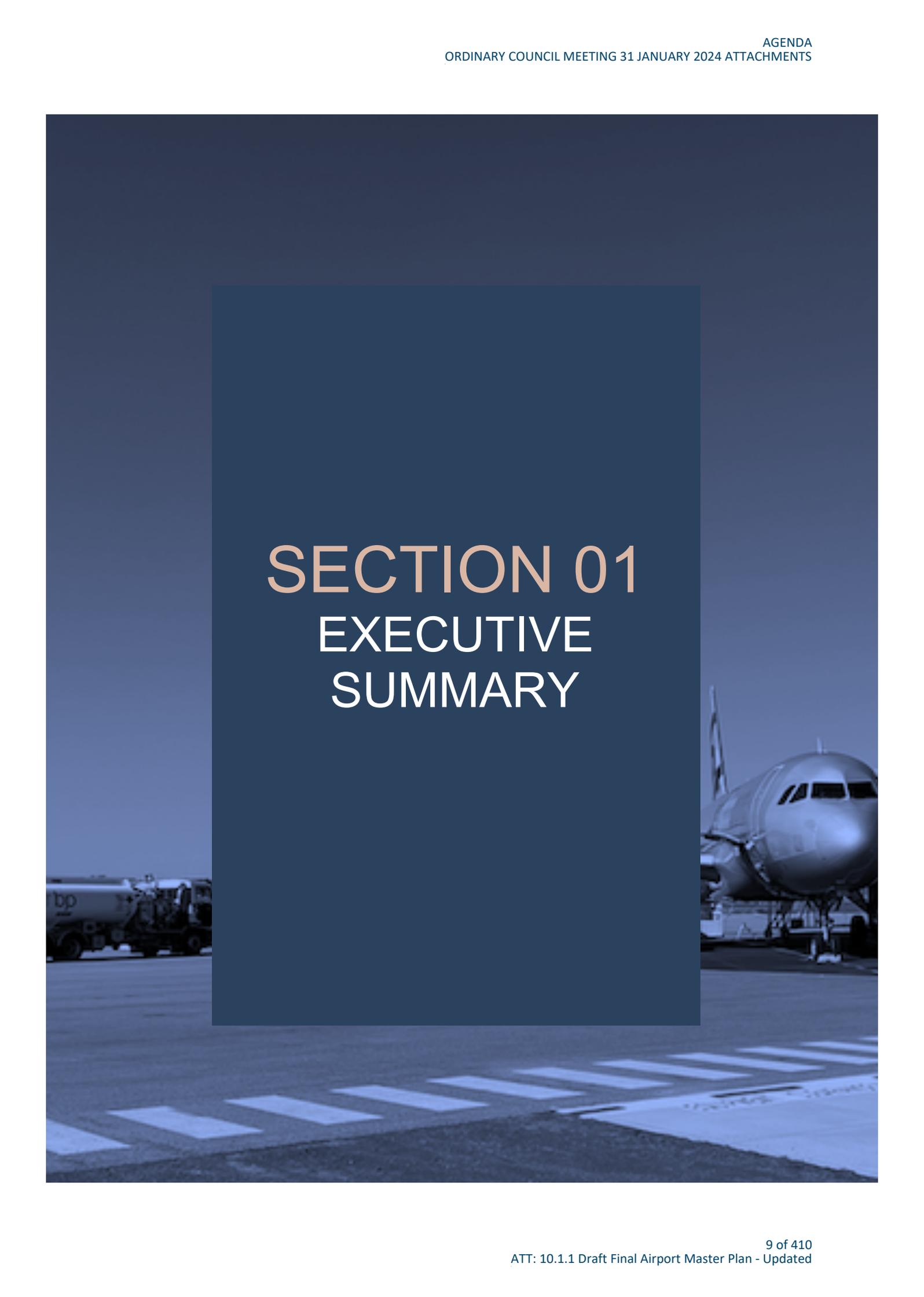
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# SECTION 01

## EXECUTIVE SUMMARY

# 01

## Executive Summary

In March 2022, it became possible for visitors to arrive directly into Busselton by air from Melbourne, with visitors no longer needing to drive from Perth adding hours to their journey. The sector has become extremely popular with visitors and residents such that growth of the route and additional services are planned. The ability for this to occur is a direct result of the efforts made by the City of Busselton (City) since 2016 to develop the Busselton Margaret River Airport (BMRA) as a regional aviation hub for both residents of and visitors to the region. It is therefore of critical importance to ensure that plans are established for the long-term future of the facility, so that it can continue to serve its purpose into the future. The BMRA Master Plan is the plan for the future of the airport. Some of the facilities and activities identified in the BMRA Master Plan will be developed in the near term, whereas some will take several years to justify and might be expected to be implemented in 10 to 15 years from now. The Master Plan identifies areas of the airport site that are reserved to enable the City to react to opportunities that might develop in the future but are not presently apparent thus giving the City flexibility to react to a changing landscape and business practices.

Since March 2022, a Jetstar Regular Passenger Transport (RPT) service using Airbus A320 aircraft has operated linking the Busselton Margaret River region directly to Melbourne. The RPT services started with 3 flights a week and the carrier will increase services in 2024 with direct flights to Sydney. The airport also services Fly-In-Fly-Out (FIFO) operations, ferrying workers to the various mineral extraction sites across the North West of Western Australia. The FIFO flights support the mining resources roster schedules, with swings that are currently concentrated on Tuesday, Wednesday and Thursdays. Traffic forecasts for BMRA were prepared as part of this Master Plan and identified growth in demand for both tourism and resident travel which shows increases in services throughout the week and so filling out the flight schedule.

“The BMRA Master Plan is the plan for the future of the airport.”

The forecasts also show demand for new sectors linking BMRA with Sydney, Brisbane, Singapore and Denpasar.

BMRA peak period passenger volumes are forecast to grow between present day and 2033. From that point onwards, additional passenger traffic growth would occur outside the peak period or fill out the week on non-peak days. Consequently, the Master Plan has addressed facility development driven primarily by the current deficiencies that exist in terms of capacity to reflect the nature of expected growth in passengers and flights, and the infrastructure required to serve this type of growth. Peak period demand on facilities is expected to grow until midway through the forecast period increasing demand on passenger facilities in the near term. Additional FIFO morning departures and RPT peak period arrivals will increase between 2025 and 2033, peak period demand will then level off and remain constant through to the end of the forecast period of 2043.

Opportunities such as additional facilities for Car Rental firms, a GA Precinct, Aircraft Maintenance, Fixed Base Operators, Airline Support organisations etc., were identified and explored to determine how the airport could contribute to the economic activities at and around the airport. Detailed analysis of responses to growing traffic or economic activity were examined and options for responding were developed. These options were considered and refined to reflect the journey from the present day BMRA to the Master Plan development for 2043. The Master Plan tells the story of this journey through three development phases. The BMRA Master Plan is depicted as a layout plan for the site in Exhibit 1.

The scope of development proposed for the BMRA through the Master Plan period has been separated into 3 phases, reflecting priorities determined through analysis of capacity and deficiencies, as well as perceived needs and opportunities.

As far as the proposed phases of development are concerned, these comprise the following:



## PHASE 1

The requirement for a new passenger terminal was identified in the previous Master Plan (2016) however due to the withdrawal of funding the terminal building was not constructed. As a result, the current Arrivals and Departures Passenger Terminals are operating beyond their capacity. As such, a new passenger terminal, located adjacent to the Southern Apron is required to be constructed in Phase 1. The terminal should be sized to accommodate the 2033 forecast peak demand as peak demand remains constant from 2033 to 2043. The passenger terminal also needs to accommodate international processing as international services are forecast to start in approximately 2028. International flights are anticipated to occur outside the peak period to fit into commercial scheduling patterns at the destination airports. Consequently, designers can then use operable walls and swing gates to minimise the building footprint. Phase 1 will see the GA Precinct lease established and initial hangars constructed for recreational and general aviation activates off Taxiway D1.

## PHASE 2

Introduction of international operations will present the opportunity for local businesses, producing high value goods, particularly in the seafood industry, to export produce to Asian markets quickly. To support the economics of operating an international flight, the carrier will want to fill any excess cargo space on the aircraft. For a narrow body flight using an A320 type aircraft, there will likely be between 7 and 14m<sup>3</sup> of additional cargo space available on each flight. To process and prepare the cargo for export, there needs to be an airfreight facility, which should be developed in time for the introduction of international flights.

Domestic operations tend to do most of the flight preparation at the major base or airport. For the current Melbourne sector, Jetstar conduct the cleaning and catering resupply prior to departing Melbourne, with no catering taken on in Busselton as the aircraft is carrying adequate supply for the return leg. Cleaning of the aircraft will be conducted but the level performed in Busselton will be minimal with a more thorough cabin preparation completed at the end of the operation in Melbourne. For an international operation, such as a flight to Singapore, the longer duration of the flight and the possibility of wishing to provide a more premium service, there is a probable need for a more thorough cleaning of the aircraft and inflight catering to be loaded at BMRA. This higher demand for servicing aircraft will result in the need for companies in the airline support industry to establish some presence at Busselton to support the developing international market. Introduction of international flights will therefore spur development in the Airline and Airport Support Precinct.

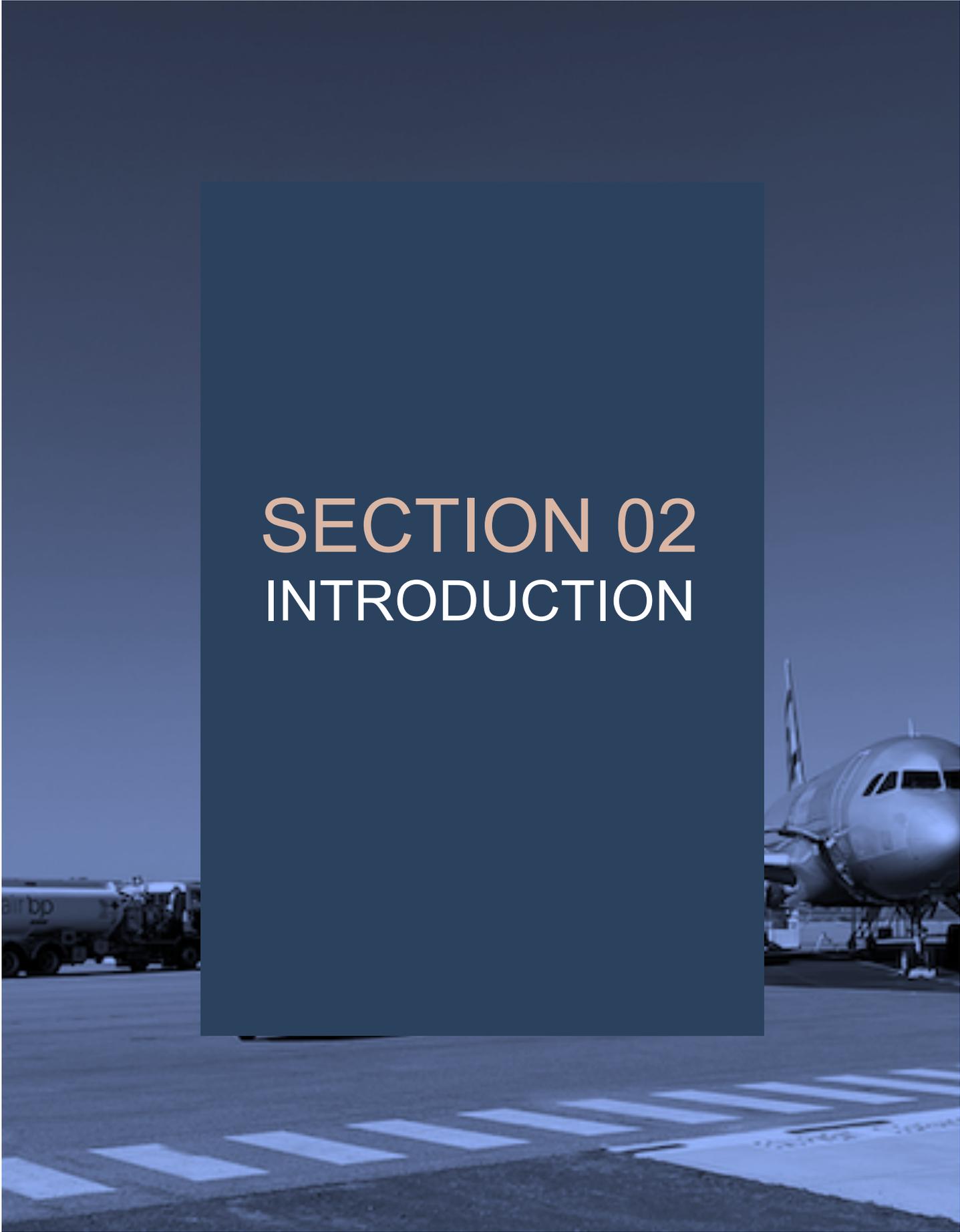
The forecasts identify the development of additional flight routes to major domestic city pairs during Phase 1 and Phase 2. The likely developments would be Sydney and Brisbane services. These flights will very likely target the same arrival and departure period as the existing Melbourne services. The forecasts identify up to five arrivals in the peak hour. For this to occur, at least one arrival must turnaround and depart within the hour and provide enough of a buffer to ensure the stand is prepared and available for the next arrival. As the demand for stands increases close to the capacity limit, it is recommended that the City expand the Southern Apron to five stands in Phase 2.

The BMRA is expected to reach the domestic passenger throughput numbers necessary for Level 2 Aircraft Rescue and Fire Fighting (ARFF) facilities during Phase 2. In addition, the introduction of international services will require the presence of Level 1 ARFF facilities. Therefore, during Phase 2 ARFF facilities are necessary and should be provided before either of these milestones are reached.

## PHASE 3

Forecast passenger traffic is expected to grow in Phase 3 with growth forecast to occur outside peak periods and on off-peak days with carriers adding services on additional days of the week, growing towards daily services. In addition, FIFO services are forecast to continue to grow, however the peak hour is not forecast to increase and therefore, the passenger terminal will not need further expansion during this phase.

With the potential growth of international services, a carrier may choose to operate a widebody aircraft to BMRA. If this was to occur, BMRA would need to provide a suitable aircraft stand equipped with a refuelling hydrant system. During this phase, the Southern Apron should be expanded further to provide a total of 6 Code C stands. This will relieve pressure on the apron in the event of off-schedule operations during the peak period. The additional stand will be capable of accommodating Code C or Code E aircraft. It would be equipped with hydrant refuelling to facilitate a reasonable turnaround time for a Code E aircraft. By providing the hydrant fuel system at the most southern stand, it reduces the distance to the new fuel storage facility and therefore reduces the cost to implement.



# SECTION 02 INTRODUCTION

# 02

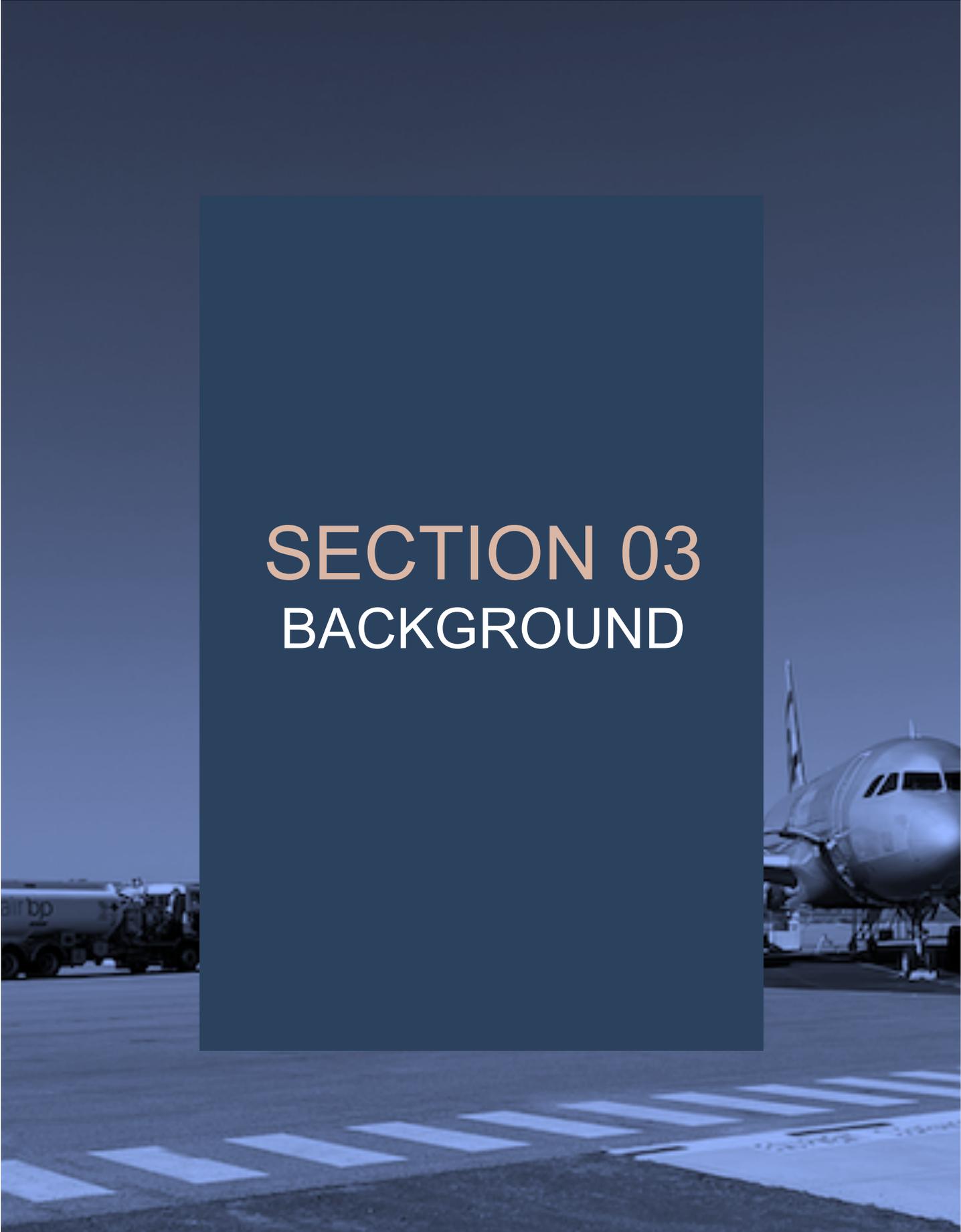
## Introduction

three consulting in association with Leading Edge Aviation Planning Professionals (LEAPP) were appointed to produce the 2023 Busselton Margaret River Airport Master Plan. This document examines the historic airport traffic, current demand, and forecasts the aviation activity through to 2043 for Busselton Margaret River Airport (BMRA). Most of an airport's operational focus and revenue generation is determined by passenger flows through the facility, making this exercise a key aspect of the master planning process. The BMRA Master Plan then examines the existing airport infrastructure and identifies what infrastructure would be needed to accommodate the 2043 forecast aviation activity.

In the period between 2016 and 2018, the airport experienced a significant airside upgrade with an extended and widened runway bringing the BMRA to Code E standard, new taxiways, general aviation aprons, a new Passenger Terminal Apron, upgraded central apron and new airfield lighting systems. With the level of development experienced during this period, the airport is in a good position to accommodate significant growth in air traffic. The main areas where further developments will need to take place revolve around attracting commercial tenants and generating economic activity for growth of businesses in the region.

This Master Plan outlines the forecast traffic over the next 20 years to 2043. It identifies and safeguards the airport for the development of future commercial activity and how that activity interacts within the BMRA precinct.

“This Master Plan outlines the forecast traffic over the next 20 years to 2043.”



# SECTION 03 BACKGROUND

# 03

## Background

The Busselton Margret River Airport (BMRA) originated as the Busselton Regional Airport which opened in 1997 with the primary objectives of improving tourism, business and emergency medical access to the South West region. The airport was served by short haul Regular Passenger Transport flights between Busselton and Perth, operated by Skywest Airlines, Maroomba and Skippers Aviation until 2001. At the time, the airport had an 1800m long by 30m wide runway serving turboprop aircraft operations.

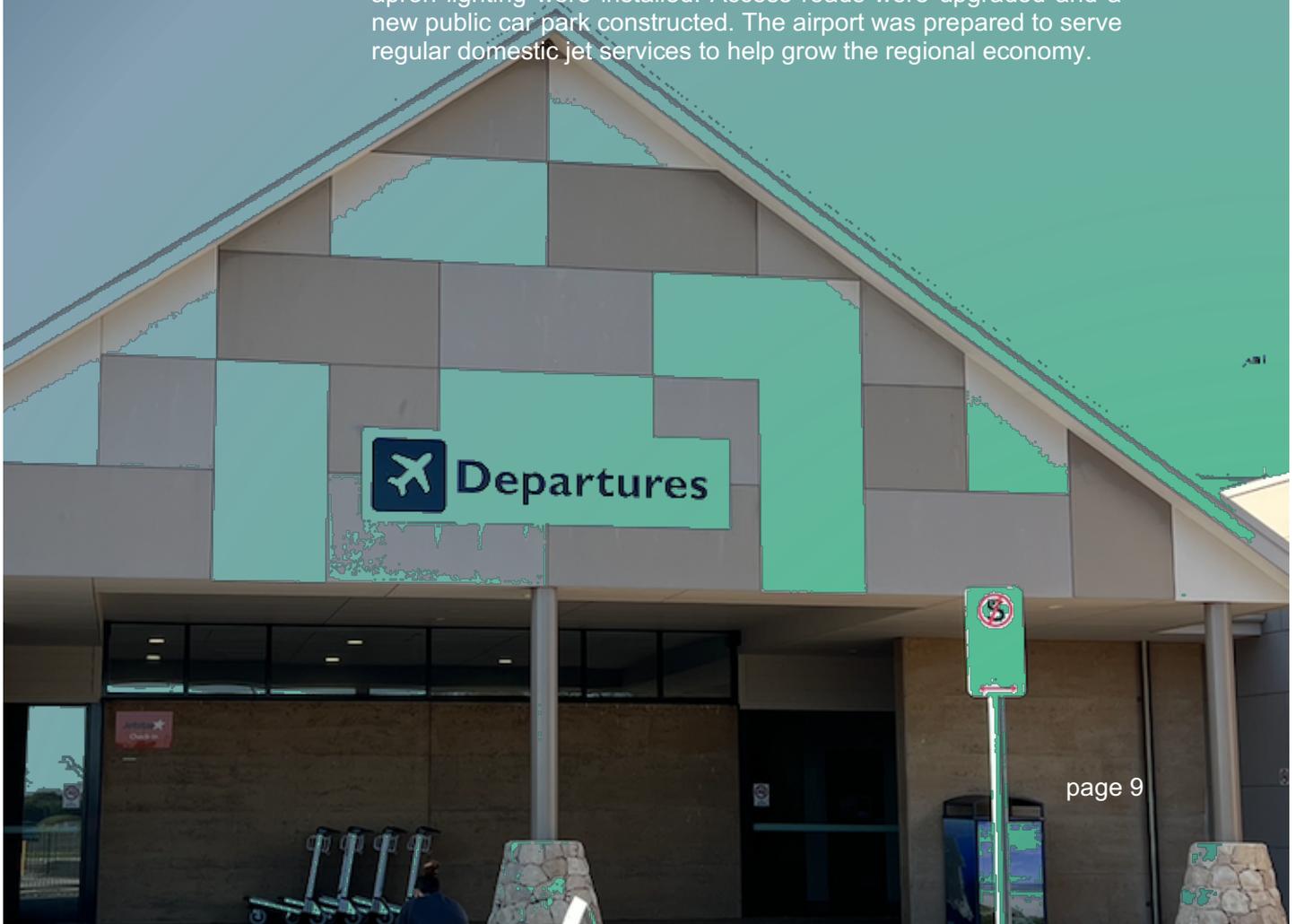
The first FIFO flight commenced in 2007 with Skywest operating services to the Pilbara for Rio Tinto. BMRA has seen a steady growth in FIFO traffic taking workers to various sites across the Pilbara, currently operating to eleven mine sites and regional airports with thirty-one flights per week. Busselton has successfully established itself as a source of FIFO workers and offers attractive and liveable lifestyle options for FIFO families. With the available housing and the lower cost of living in the Busselton region, BMRA is expected to see continued growth in FIFO traffic.

Starting in 2012, the City implemented a Noise Management Plan (NMP) for BMRA. The NMP was developed in response to community noise complaints and the need for the City to manage the level of noise generated from the airport resulting flight training. The NMP developed in coordination with the Office of the Environmental Protection Authority and extensive public consultation is reviewed and updated periodically. The NMP has resulted in some restrictions on certain types of activities such as the level of flight training, but has also given the City a means to manage noise levels and community expectations with respect to future noise levels, flight paths, hours of operations, Fly Neighbourly Agreements, noise complaint procedures, etc.

“With successive versions of the NMP, both the community and the City are now in a better position to control and manage noise impacts and aircraft operations.”

The initial NMP imposed significant restrictions on the type and extent of operations at the airport, which had an impact on commercial activity. With successive versions of the NMP, both the community and the City are now in a better position to control and manage noise impacts and aircraft operations. Further refinements of the NMP over time, are expected to result in a less restrictive NMP giving the airport better ability to manage the resources while growing tourism and business opportunities for the region and improve the noise environment for the community.

The 2016 Master Plan outlined the required infrastructure development and in 2016/17, BMRA underwent a significant upgrade with a widening, extension and strengthening of the runway. This would prepare the airport to accommodate regular service by Code 4E aircraft. The new Southern Apron was developed to accommodate three Code 4C jet aircraft and one stand that could accommodate up to a Code F aircraft. A new General Aviation/Commercial Aviation precinct was developed and the Central Apron was expanded. Runway, taxiway and high mast apron lighting were installed. Access roads were upgraded and a new public car park constructed. The airport was prepared to serve regular domestic jet services to help grow the regional economy.





# SECTION 04 AIRPORT INFRASTRUCTURE

# 04

## Airport Infrastructure

The primary airport infrastructure comprises:

- Runway
- Taxiways
- Aprons
- Terminals
- Hangars
- Roads
- Fuel Facilities
- Lighting
- Navigational Aids
- Wind Direction Indicators
- Utilities

The existing airport infrastructure was extensively upgraded by 2018 providing the BMRA with the ability to accommodate air traffic growth for the City of Busselton and surrounding South West region. The existing infrastructure and airport layout has been provided in Exhibit 2.



## Runway

The primary piece of infrastructure at the airport is the runway. BMRA has a single runway that is 2,460m long and 45m wide, oriented 028/208° magnetic and designated Runway 03/21. The runway pavement is grooved asphalt and has a strength of:

**PCN 58 /F/A/1500(218PSI)/T Grooved**

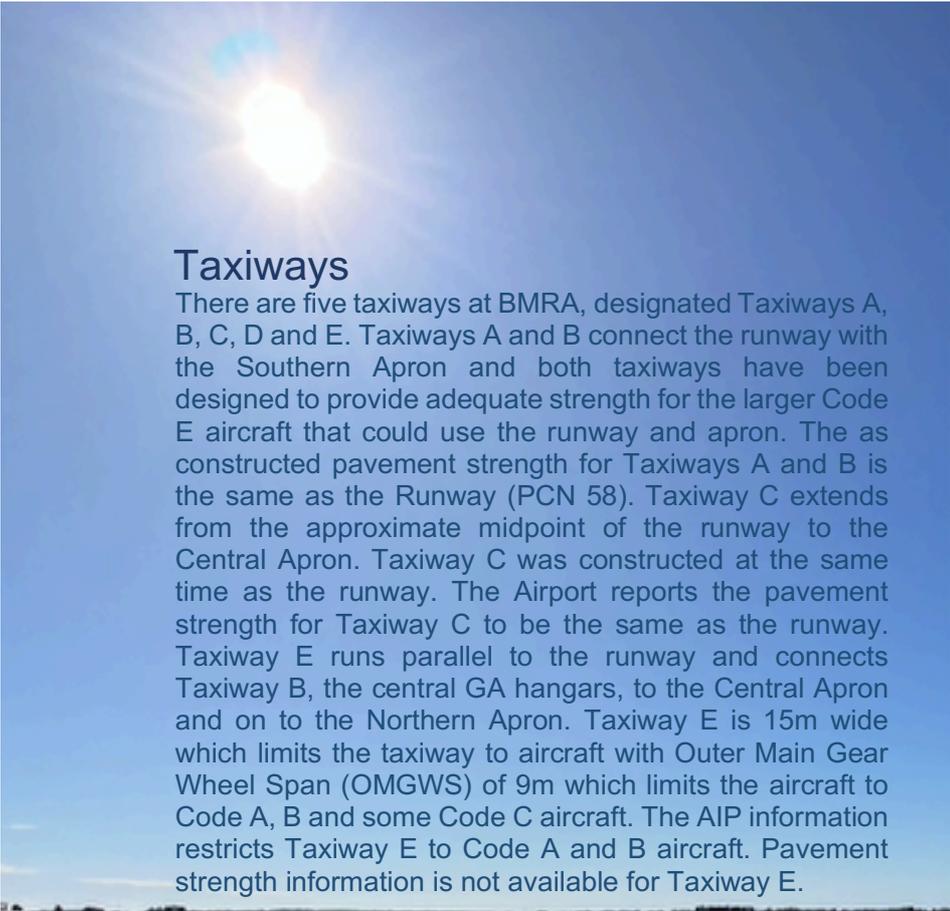
This means the pavement is a flexible pavement i.e. asphalt. The strength of the in-situ subgrade layer has a California Bearing Ratio (CBR) that is categorised as 'A'. An 'A' subgrade is considered as a high strength subgrade with a CBR value of 13 or higher. Determining the pavement strength is identified as 'T' which indicates that a technical evaluation was conducted to determine the PCN strength. A technical evaluation is conducted by testing the pavement which gives very accurate readings. Using the PCN/ACN system, the strength of the pavement is reported as a PCN of 58.

To assist the removal of water from the runway and ensure there is adequate friction, the surface of the runway is grooved. This process greatly reduces the potential for hydroplaning when the runway is contaminated with water.

Based on the current runway exit/entry positions, runway capacity would be approximately 8 to 12 movements per hour depending on the aircraft mix that is active on the runway. This capacity recognises that:

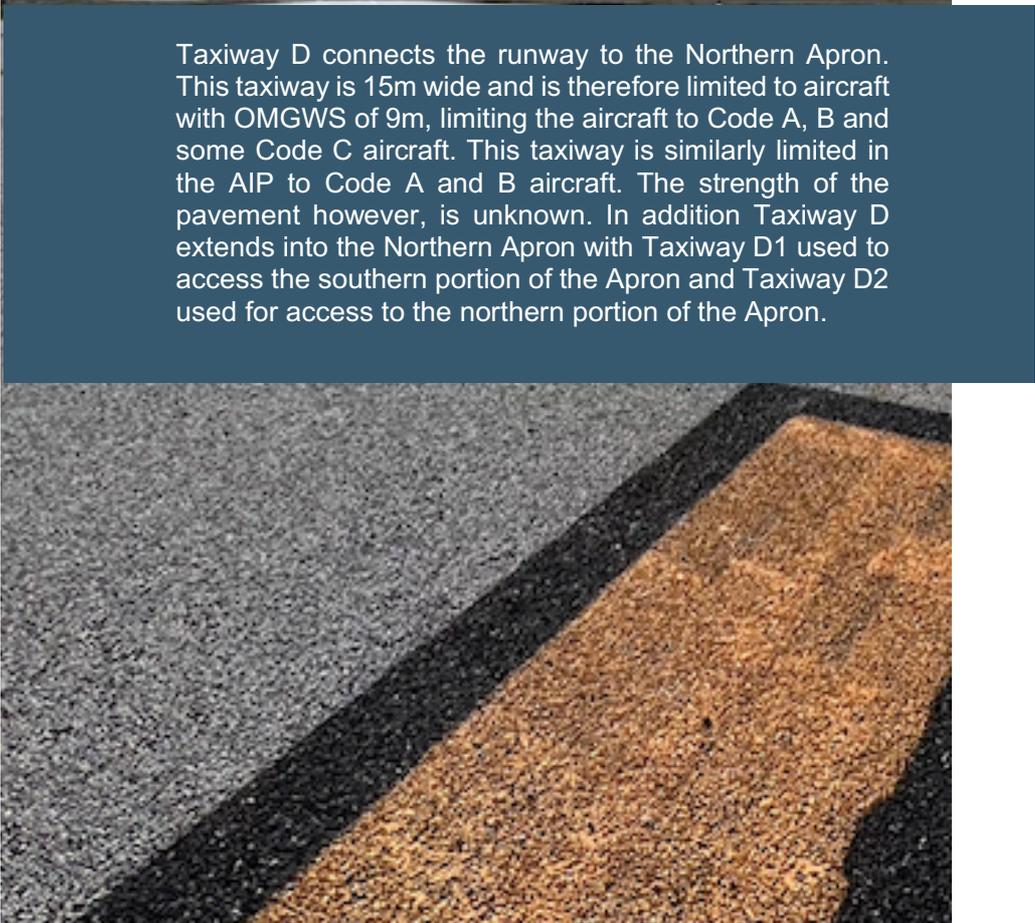
- the airport is an uncontrolled airport,
- aircraft need to turn around at the end of the runway before taxiing back to the apron,
- aircraft need adequate separation, and
- aircraft need time to taxi from the apron to the end of the runway and turn around before lining up and departing.

During times of common aircraft type operation such as the morning departure wave of FIFO services, or when only small aircraft are operating, higher throughput rates can be achieved.



## Taxiways

There are five taxiways at BMRA, designated Taxiways A, B, C, D and E. Taxiways A and B connect the runway with the Southern Apron and both taxiways have been designed to provide adequate strength for the larger Code E aircraft that could use the runway and apron. The as constructed pavement strength for Taxiways A and B is the same as the Runway (PCN 58). Taxiway C extends from the approximate midpoint of the runway to the Central Apron. Taxiway C was constructed at the same time as the runway. The Airport reports the pavement strength for Taxiway C to be the same as the runway. Taxiway E runs parallel to the runway and connects Taxiway B, the central GA hangars, to the Central Apron and on to the Northern Apron. Taxiway E is 15m wide which limits the taxiway to aircraft with Outer Main Gear Wheel Span (OMGWS) of 9m which limits the aircraft to Code A, B and some Code C aircraft. The AIP information restricts Taxiway E to Code A and B aircraft. Pavement strength information is not available for Taxiway E.



Taxiway D connects the runway to the Northern Apron. This taxiway is 15m wide and is therefore limited to aircraft with OMGWS of 9m, limiting the aircraft to Code A, B and some Code C aircraft. This taxiway is similarly limited in the AIP to Code A and B aircraft. The strength of the pavement however, is unknown. In addition Taxiway D extends into the Northern Apron with Taxiway D1 used to access the southern portion of the Apron and Taxiway D2 used for access to the northern portion of the Apron.

## Aprons and Stands

The BMRA has three aprons. The Central Apron is the primary apron for all FIFO and RPT flights. This apron can accommodate four Code C aircraft on independent self-maneuvring stands. The Passenger Terminals and the Jet A1 and Avgas refuelling bowsers are located next to the Central Apron. The BMRA has the Southern apron for large aircraft use and two aprons for small aircraft use to the north.

1

The **Southern Apron** was identified in the 2016 Master Plan and constructed as part of the BMRA Development Project. The 2016 Master Plan forecast the commencement of RPT traffic and recognised that a new larger apron and passenger terminal would be needed. The development of the passenger traffic precinct was planned to be located at the southern end of the airport. The Southern apron was constructed to accommodate 4 aircraft stands permitting 3 Code C aircraft and 1 Code E aircraft, accommodating aircraft up to the size of a B787. The designations for the stands are Bays 1 to 4 with Bay 1 capable of accommodating a Code E aircraft and Bays 2 to 4 sized for Code C aircraft. During the development project, the passenger terminal was not constructed as anticipated, resulting in the Southern Apron currently experiencing limited use. All FIFO and RPT traffic continue to use the Central Apron due to the proximity to the existing Passenger Terminals.

2

The **Central Apron** is located in front of the existing Passenger Departure Terminal. This apron is able to accommodate 4 Code C aircraft as well as a number of small aircraft accommodated along the southern edge of the Central Apron. This apron, being located in front of the current Departure Passenger Terminal, is used for RPT flights, FIFO operations as well as any itinerant charter aircraft operations. The 4 Code C stands are designated Bays 8 to 11. Bay 9 is directly in front of the Departure Terminal and Bay 10 is located directly in front of the Arrivals Terminal.

The current Jet-A1 and Avgas 100LL bowsers are located adjacent to the Central Apron, towards the southern portion of the apron. The bowsers are available for self-serve access and therefore adequate parking space is available on the apron to permit aircraft to park in front of the appropriate fuel bowser for refuelling. Larger jet aircraft such as the RPT and FIFO aircraft are served on stand via a mobile refuelling vehicle operated by AirBP

3

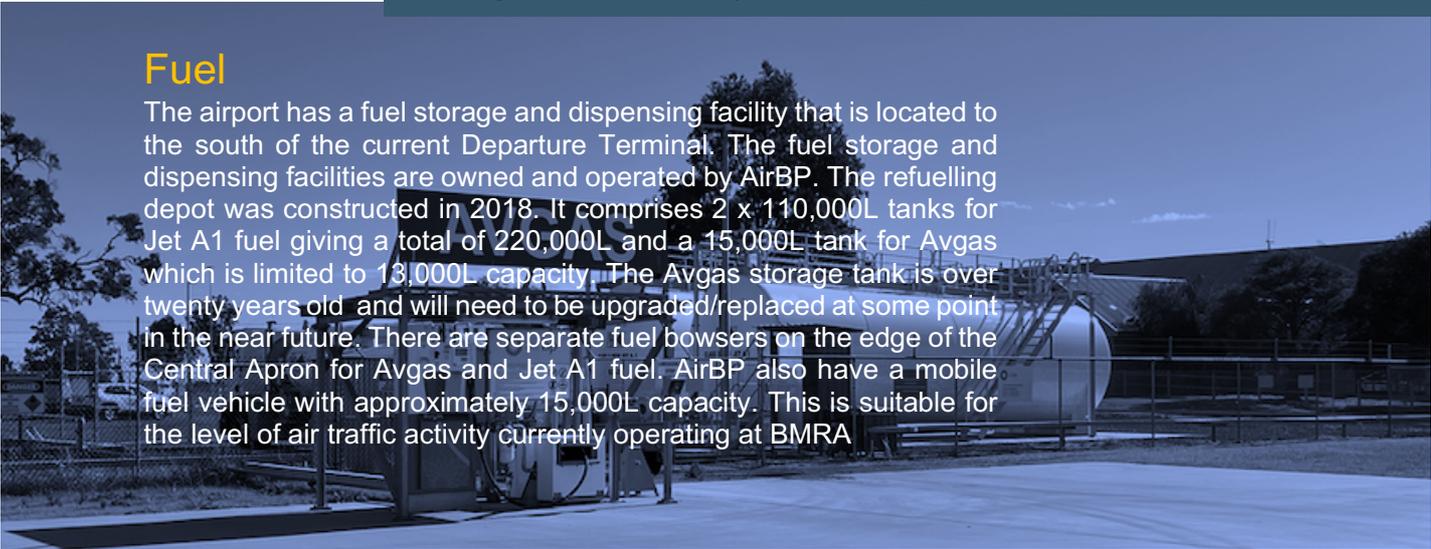
The **Northern Apron** Northern Apron was identified as part of the previous Master Plan and constructed during the Development Project. The Northern Apron is 43,158m<sup>2</sup> and is divided into 3 sections accessed by Taxiways D, D1 and D2. Plans are currently underway to develop the D1 apron area as the recreational and private hangar zone.

The apron accessed by Taxiway D2, is expected to be a more traditional arrangement of hangar lots. This apron is planned for Code B sized hangars that would be suitable for small GA businesses.

## Central Hangarage

Located immediately to the north of the Southern Apron, are 3 hangar buildings. The 3 buildings contain 17 individual leasable hangar spaces, with the northern and central hangars constructed in 1998 and the southern hangar in 2009. In the past tenants pursued a mix of private, recreational, commercial and business activities. The northern and central hangars were purchased by the City of Busselton in 2015 and converted to short term hangar hire arrangements. This resulted in a reduction in private and recreational GA activity at the BMRA. A consortium of aircraft owners privately owns the southern hangar under a ground lease arrangement with the City, which will expire in 2029.

## Fuel



The airport has a fuel storage and dispensing facility that is located to the south of the current Departure Terminal. The fuel storage and dispensing facilities are owned and operated by AirBP. The refuelling depot was constructed in 2018. It comprises 2 x 110,000L tanks for Jet A1 fuel giving a total of 220,000L and a 15,000L tank for Avgas which is limited to 13,000L capacity. The Avgas storage tank is over twenty years old and will need to be upgraded/replaced at some point in the near future. There are separate fuel bowzers on the edge of the Central Apron for Avgas and Jet A1 fuel. AirBP also have a mobile fuel vehicle with approximately 15,000L capacity. This is suitable for the level of air traffic activity currently operating at BMRA.

## Lighting

The Southern and Central Aprons are fully equipped with High Mast lighting. Blue taxiway edge lighting has been installed on the taxiways and the runway is equipped with white edge low intensity runway edge lights. These are operated remotely via radio.

Both runway directions have Precision Approach Path Indicator lighting systems (PAPI). The PAPI lighting system is used by pilots to ensure they have established the correct glide angle to the runway. In this case the PAPIs have been established at 3°. PAPI lighting systems have been installed on both sides of the runway and are operated remotely along with the runway edge lights.

## Navigational Aids

BMRA currently has a Non-Directional Beacon (NDB) used primarily for area navigation. There are NDB approach procedures for both runway directions. The Busselton NDB is part of the Backup Navigational Network (BNN) which was established to provide a land-based air navigation backup system in the event that the satellite based GNSS navigational network is not available to a pilot. The BNN was established in 2016 and it currently contains 200 backup land-based nav aids at approximately 124 airports around the country. Air Services have begun a Post Implementation Review of the BNN however have not identified what the next steps are and if the existing nav aids will be maintained, upgraded or decommissioned. The NDB does not provide significant improved access to the airport and its location restricts the ability for the airport to expand the Southern Apron. As such, BMRA will continue to lobby Airservices for the decommissioning of the NDB at BMRA.

“BMRA maintains a GNSS approach as well as multiple satellite based Required Navigational Performance (RNP) approaches to each runway”

BMRA maintains a GNSS approach as well as multiple satellite based Required Navigational Performance (RNP) approaches to each runway. These satellite approaches provide BMRA with non-precision instrument approaches for both runways providing increased accessibility for the airport. Approach minima varies for each RNP approach, however they provide for minima as low as 107.8m (354 feet) above the aerodrome elevation.



### Wind Direction Indicator

There are 3 Wind Direction Indicators (WDI) located at BMRA. There is a WDI located near to the Runway 03 threshold on the west of the runway. The primary WDI is located to the east of the Southern Apron. The final WDI is located north of the Central Apron, in between the Central and Northern Aprons. Of these, only the WDI near the Runway 03 threshold and the WDI near the Southern Apron are illuminated.

### Passenger Terminal

When the Busselton Regional Airport opened in 1997, a small passenger terminal was constructed. The introduction of FIFO flights in 2007 serving Rio Tinto operations and RPT (Perth – Busselton- Albany return services) in 2011, resulted in the terminal building being expanded. With the growth in FIFO services, it became apparent that the terminal was not large enough for the traffic experienced and the passenger terminal was further renovated and expanded in 2014. The terminal was able to support FIFO operations until 2019 when a further expansion of the Departures terminal and construction of the arrivals terminal was completed in preparation for the commencement of interstate RPT flights. The introduction of RPT flights in 2022 identified that the volume of passengers exceeded the capacity of the departures terminal and the departures sterile lounge was expanded again.

The Departure Terminal has a floor area of approximately 1322m<sup>2</sup> and the Arrival Terminal of 755m<sup>2</sup> giving a total of 2077m<sup>2</sup>. This floor area would provide an approximate Level of Service (LoS) C for a 2-way flow of 148 passengers in the peak hour or a 1-way capacity of approximately 74 passengers. The existing peak departure demand comprises of 2 Code C FIFO departures in the morning with a passenger volume of up to 243 passengers per hour consisting of either two A320 aircraft or a single A320 and F100 aircraft. The peak hour arrivals period is similarly comprised of 2 FIFO arrivals comprising of up to 243 passengers. FIFO arrival passengers tend to have carry-on luggage only and do not linger in the terminal. As such, the FIFO arrivals do not have a large impact on the terminal sizing. The RPT arrival from Melbourne, however, does have an impact on terminal sizing and LoS as a single operation of the A320 operated by Jetstar would have a demand for 288 passengers (2-way) at an 80% load factor. That demand could reach 360 passengers if the flight were to operate at 100% load factor. As such the terminal system is operating well above capacity and the need for a new larger terminal remains critical.

## Sewerage and Water Supply

The airport is not currently connected to the mainline sewerage network that extends partway down the Vasse Highway. Therefore, all sewerage is treated onsite. The existing onsite septic system is currently operating beyond its capacity and designs are underway to upgrade the system to service the existing terminal and the new terminal once built.

## Local Transport and Access Roads

The airport is accessed via Neville Hyder Drive from the Vasse Highway. The Vasse Highway links into the Bussell Highway which is in the process of being upgraded and Stage 2 is due to be completed in 2024.

## MET Facilities

There is a Bureau of Meteorology facility containing meteorological instrumentation on the airport. This facility is located to the north of the North Aprons. Continually updated weather is provided by an AWIS service that is available via phone or broadcast on 128.05Mhz.

## Car Parking

The City constructed a 422 bay public car park in 2019 which primarily provides parking for FIFO passengers. An extension to the public car park was completed in November 2022 with an additional 296 car parking bays added. Uptake of the facility has exceeded expectations and the City is exploring the development of additional long term parking to accommodate the growing demand for car parking spaces.

## Aircraft Rescue and Fire Fighting

Air traffic at BMRA has not reached a level where the need for Aircraft Rescue and Fire Fighting (ARFF) services has been reached. Currently, an airport needs to have 350,000 passengers per annum for Level 2 coverage or 350,000 domestic passengers and international services for Airservices to implement Level 1 ARFF. As there is no ARFF at the airport, and emergency cover is provided by the local Busselton Fire and Rescue Services (BFRS) located on Harris Road. The BFRS facility is not a 24-hour facility and therefore the closest 24 hour fully staffed facility is the DFRS in Bunbury, approximately 40 minutes away. Ongoing discussions are taking place with Airservices Australia (ASA) to determine how best to provide ARFF cover such that BMRA can be used as an alternate aerodrome as early as possible.

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# SECTION 05 USABILITY

# 05

## Usability

### Aerodrome Reference Temperature

Pilots use current temperature readings to adjust their aircraft performance to ensure they are within safe limits. However, temperatures fluctuate throughout the day and throughout the year but planning requires the use of a temperature for facilities planning, that, although not the maximum recorded, is suitably high, to ensure pilots can reliably operate safely. The Aerodrome Reference Temperature is established to ensure that the facilities at the airport will be suitable most of the time, for the design aircraft. The Aerodrome Reference Temperature is calculated by taking the monthly mean of the daily maximum temperatures for the hottest month of the year. For current recordings, the aerodrome reference temperature at BMRA is 31.6°C.

### Wind

Aircraft take-off and land into the wind. As such, the direction and strength of the wind dictates the ability of aircraft to operate. Wind directly down the runway aids the ability of the aircraft to generate lift and therefore reduces the length of runway used to take off. Winds blowing across the runway pose problems for aircraft and the difficulties are more pronounced for smaller aircraft than for larger aircraft. Airport planners will analyse wind data and determine what proportion of the historic wind records, aircraft of different sizes are able to operate.

Analysis of wind data for a 10-year period from 2013 to 2023, recorded by the Bureau of Meteorology at BMRA was conducted. The wind data obtained, recorded wind speed and direction every 30 minutes. The result is provided in Exhibit 3 and shows the prevailing wind strength and direction at BMRA. From Exhibit 3, it can be seen that winds from directly south, comprise the largest wind component, followed by easterly winds. Neither of these dominant wind conditions are the same as the runway alignment.

To accommodate smaller aircraft and to make operations of all aircraft easier, runways are often aligned as much as possible, according to the prevailing winds. However, it isn't always possible to construct lots of runways to give wind coverage for all wind conditions.

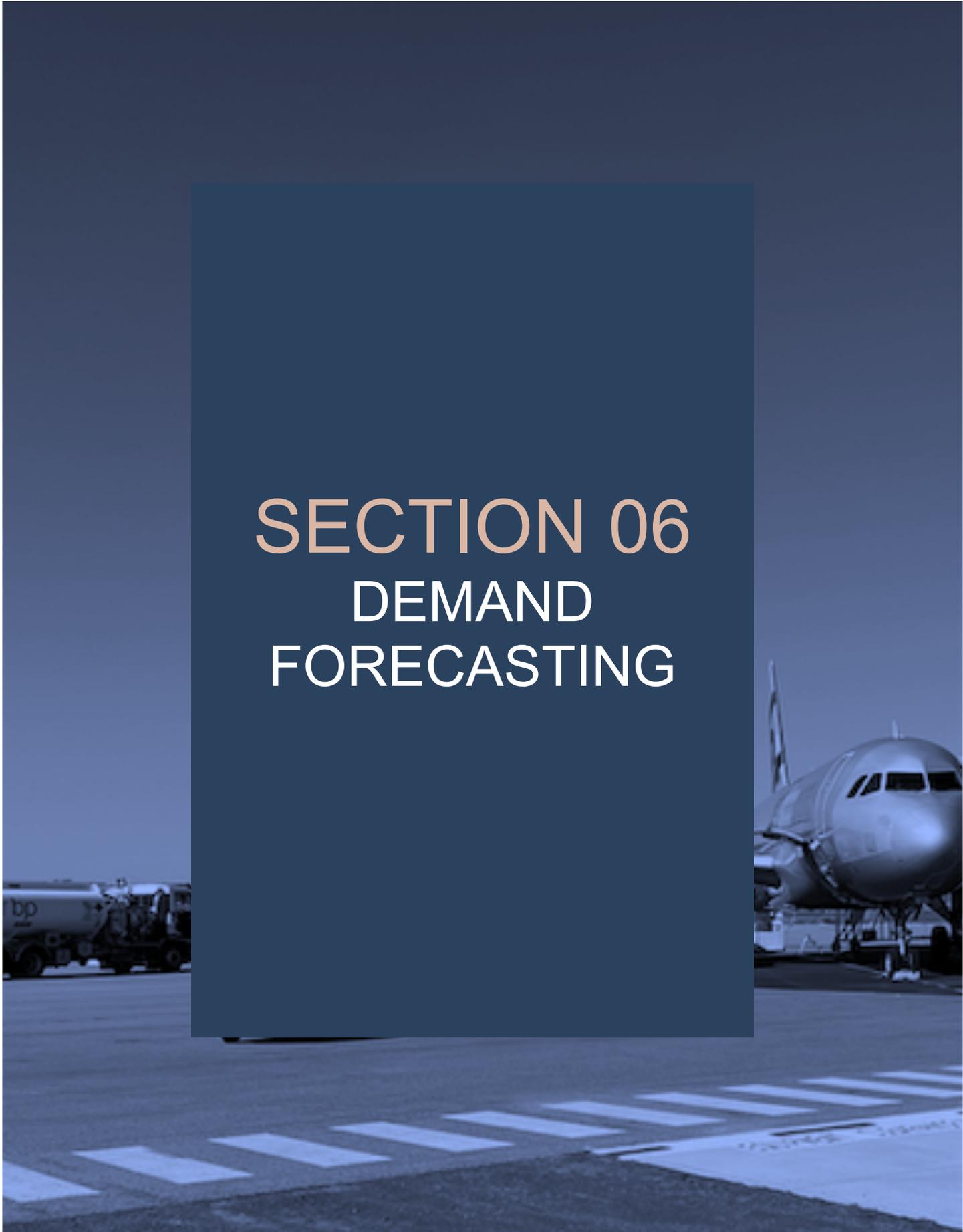
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Therefore, it is important to determine what level of usability is available within the existing infrastructure. Although CASA no longer provides a target wind coverage, ICAO still identifies that an airport should provide runways that collectively provide 95% wind coverage for the crosswind limitation of the design aircraft or dominant aircraft type in use at the airport. To analyse the crosswind component in relation to the wind data, a windrose is created as shown in Exhibit 4 with bands identifying the typical crosswind limitations for different aircraft types. The typical crosswind limitations<sup>1</sup> used for this type of analysis and sample aircraft in each category are:

Crosswind Limitation	Example Aircraft	Runway 03/21 Wind Coverage
10 kts	Small Single Engine Aircraft eg: C150, Microlights, Jabiru 120, PA-18	83.77%
13 kts	Single Engine GA eg: C172, Tecnam P92, Flight Design CTLS	90.97%
20 kts	Twin Engine aircraft eg: Beechcraft C350, Saab 340b and larger	99.51%

Large twin engine jets such as the A320 used by Jetstar, have maximum demonstrated crosswind limits well above the 20kts in the above chart. However, planners use a maximum of 20kts for determining wind coverage for a runway. As can be seen from Exhibit 4 and the table above, Runway 03/21 has a wind coverage of 99.51% for crosswinds of 20kts or below. This is well above the ICAO recommended coverage for the design aircraft i.e. RPT Code C twin jet aircraft. However, the wind coverage does not reach the coverage for smaller GA or recreational aircraft types. As such, these aircraft types will find operating at the airport more challenging or they will not be able to fly as frequently, due to crosswind conditions.

<sup>1</sup> The crosswind limitation is in fact the maximum demonstrated crosswind from the aircraft's Pilot Operating Handbook. It is not necessarily a maximum that can be handled but a maximum experienced during the flight testing phase for the aircraft type. Experienced pilots may operate in higher crosswinds but inexperienced and student pilots are recommended to abide by the limitation.



# SECTION 06

## DEMAND FORECASTING

# 06

## Demand Forecasting

The development of air traffic at BMRA has shown itself to be dynamic and should be considered in the larger geographic context with Perth Airport (PER). Perth Airport is the state-capital facility, benefiting from superior network breadth (number of destinations) and network depth (number of frequencies) located approximately 2 ½ driving hours to the North.

Our approach was to define aviation demand for BMRA's catchment area, split into outbound residents and inbound visitors, and then further split into intra-state, inter-state and international visitors. These discrete demand buckets were formed based on long-term regressions where demand was correlated to economic and demographic drivers over a 20-year period. Resulting overall demand was then split into traffic passing through BMRA and traffic passing through PER, based on observations at similar airport pairs, such as Brisbane / Sunshine Coast, Sydney / Newcastle-Williamstown, and Melbourne / Avalon. Over time, as BMRA's aviation offer improves, BMRA's capture ratio (percentage of overall traffic passing through BMRA) will also improve.

### The Busselton Airport Catchment Area

In keeping with past project work and the Master Plan 2016, BMRA's catchment area was defined as depicted on the map below, which is a function of drive times to neighbouring and competing airports, PER to the North and Albany (ALH) to the South East. In effect, BMRA's catchment area's Northern border lies between Waroona and Harvey and its South East border just East of Windy Harbour.

“Our approach was to define aviation demand for BMRA's catchment area, split into outbound residents and inbound visitors, and then further split into intra-state, inter-state and international visitors.”

Population by LGA within BMRA Catchment

	2001	2006	2011	2016	2021
Harvey	17,224	19,556	23,237	26,920	28,567
Bunbury	28,564	29,702	31,348	32,670	32,987
Dardanup	8,332	10,339	12,405	14,390	14,686
Capel	6,517	10,206	14,638	17,510	18,175
Collie	8,387	8,614	9,127	8,900	8,812
Donnybrook-Balingup	4,297	4,741	5,320	5,940	6,155
Boyup Brook	1,536	1,480	1,588	1,690	1,834
Busselton	21,868	25,354	30,330	37,760	40,640
Bridgetown-Greenbushes	3,924	3,953	4,319	4,665	5,238
Augusta-Margaret River	9,655	10,353	11,761	14,700	16,791
Nannup	1,171	1,192	1,262	1,350	1,538
Manjimup	9,880	9,256	9,183	9,370	9,093
BQB Catchment Area	121,355	134,746	154,518	175,865	184,516
Western Australia	1,832,008	1,959,088	2,239,170	2,560,320	2,660,026
<b>BQB Ratio</b>	<b>6.6%</b>	<b>6.9%</b>	<b>6.9%</b>	<b>6.9%</b>	<b>6.9%</b>
	2026	2031	2036	2041	2043
BQB Catchment Area	200,295	217,054	234,633	253,246	260,993
Western Australia	2,880,046	3,113,748	3,357,651	3,615,082	3,722,003
<b>BQB Ratio</b>	<b>7.0%</b>	<b>7.0%</b>	<b>7.0%</b>	<b>7.0%</b>	<b>7.0%</b>

Based on ABS Census data and WA State Population Forecasts, the BMRA catchment resident population was around 185,000 in 2021 and is expected to grow to around 261,000 by the end of the forecasting period, in 2043. This represents a compound annual growth rate (CAGR) of 1.6%, just ahead of Western Australia's forecast CAGR of 1.5% over this time period.

Put into a national context, the BMRA catchment area is Australia's 15th largest, larger than Darwin's and Albury-Wodonga's, however BMRA's RPT traffic generation ranks at around 75th, highlighting substantial traffic development opportunities over the forecasting period.

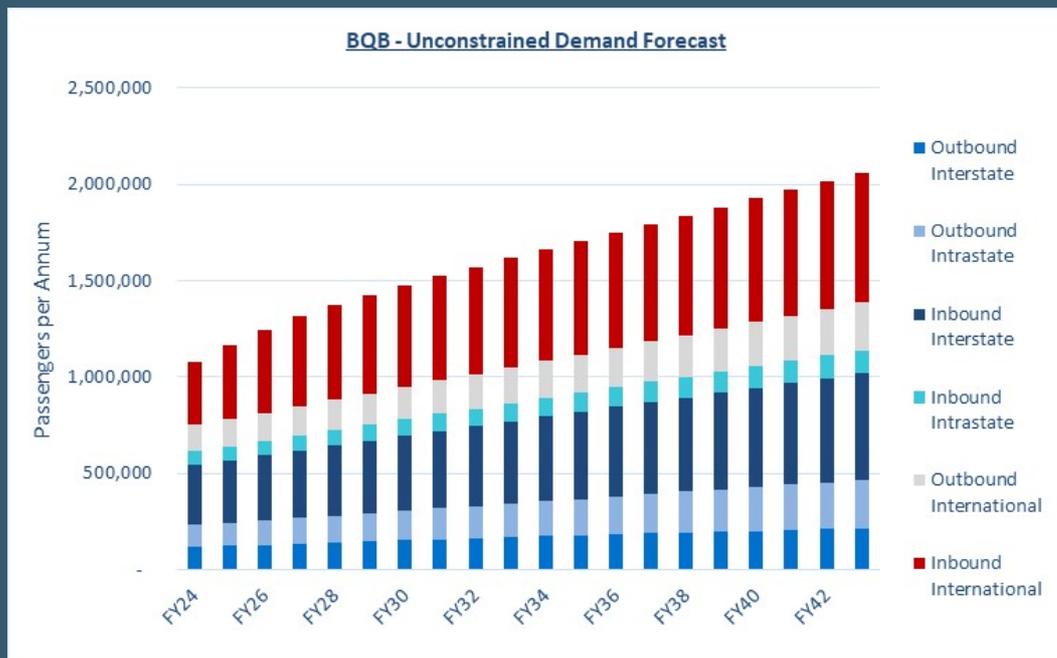
BMRA Catchment Area



## Unconstrained Demand Forecast

As outlined above, the forecasting process was started with establishing unconstrained demand levels for the BMRA catchment area. Results for the Base Forecast are shown below, showing demand levels of just over 1 million passengers in FY24, increasing to just over 2 million passengers in FY43. Data clearly shows that Australia’s South West is more of an inbound destination than an outbound origin, with demand by international visitors producing the largest demand component. This highlights the importance to carefully navigate from unconstrained demand to actual traffic forecast as BMRA will never rival PER as an international gateway.

BMRA Catchment Area Unconstrained Demand Forecast



“Data clearly shows that Australia’s South West is more of an inbound destination than an outbound origin, with demand by international visitors producing the largest demand component.”

## Traffic Forecast

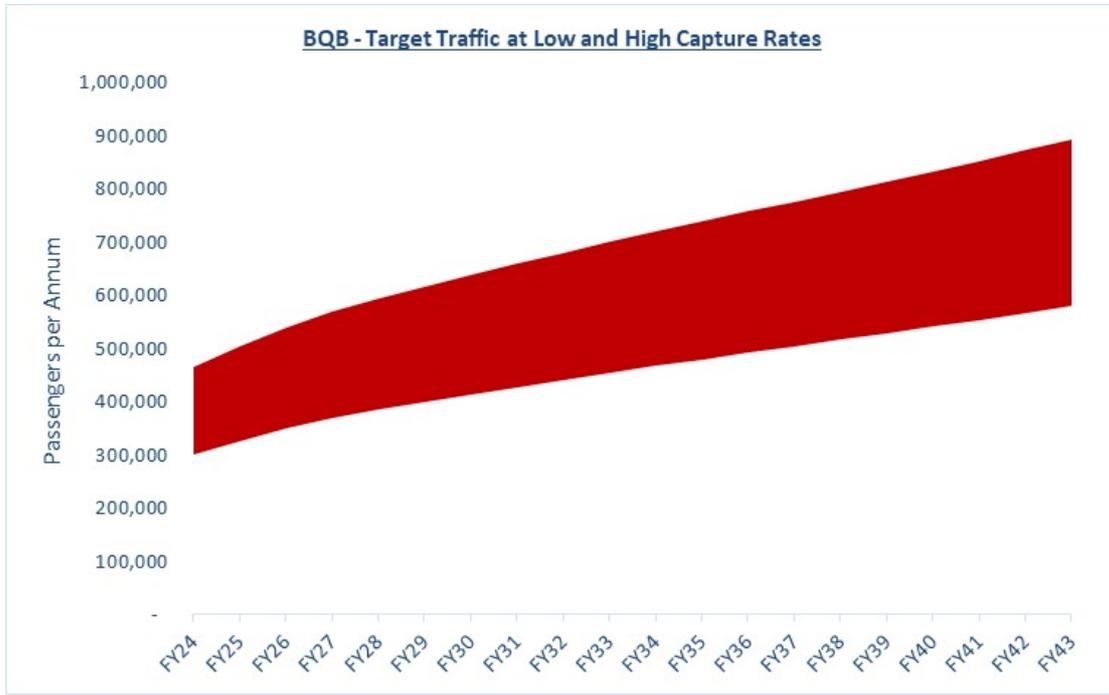
Demand and traffic forecasts differ and the situation at BMRA is a perfect example why that's the case. Traffic leakage is a common phenomenon in commercial aviation, it relates to passengers travelling to and from an airport's catchment area via a different airport, typically due to a more comprehensive aviation offer (network breadth and depth) and/or more attractive air fares. As pointed out before and due to BMRA's location in relatively close proximity to PER, leakage to PER is inevitable. The challenge is to create a meaningful and realistic traffic capture scenario, based on the demand assessment presented above.

We analysed similar airport configurations, wherein a smaller, regional airport competes against a larger state-capital airport in relative proximity in detail and were able to establish two ratios:

- When comparing traffic statistics of secondary airports with demand levels generated in their catchment areas over longer term periods, secondary airports appear to be able to capture around 40% of that demand
- When combining catchment populations for both the primary (state-capital) and secondary airports, the smaller airports are typically able to secure a traffic share of around one quarter of their population share. e.g. Avalon has a catchment area population of around 460,000; Greater Melbourne including Avalon has a catchment population of around 5.5 million, in theory Avalon's passenger throughput should be 9% of the total but in effect Avalon facilitates around 2.3% of Greater Melbourne area passengers, a quarter of its population share.

Applying these measures or capture ratios to the situation at Busselton suggests traffic outcomes over the forecasting period as shown below:

BMRA Catchment Area Unconstrained Demand Forecast

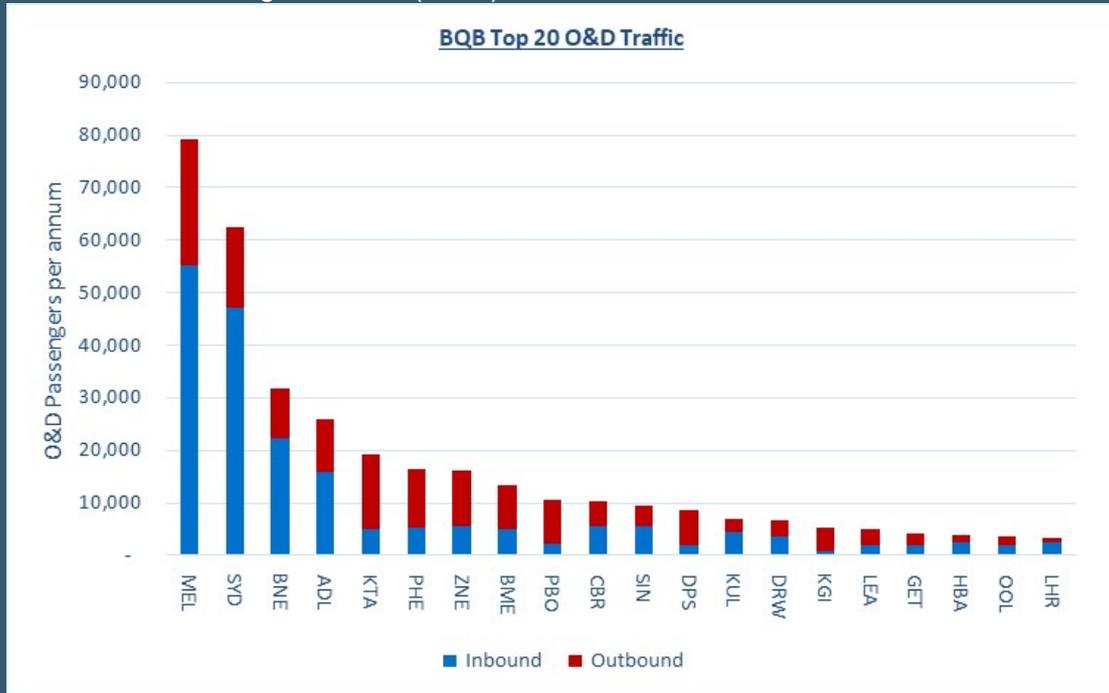


Analysing available data a step deeper highlights that capture ratios don't apply uniformly across a catchment area's demand profile but that different demand buckets are captured at different levels. In a steady-state scenario, the following ratios apply:

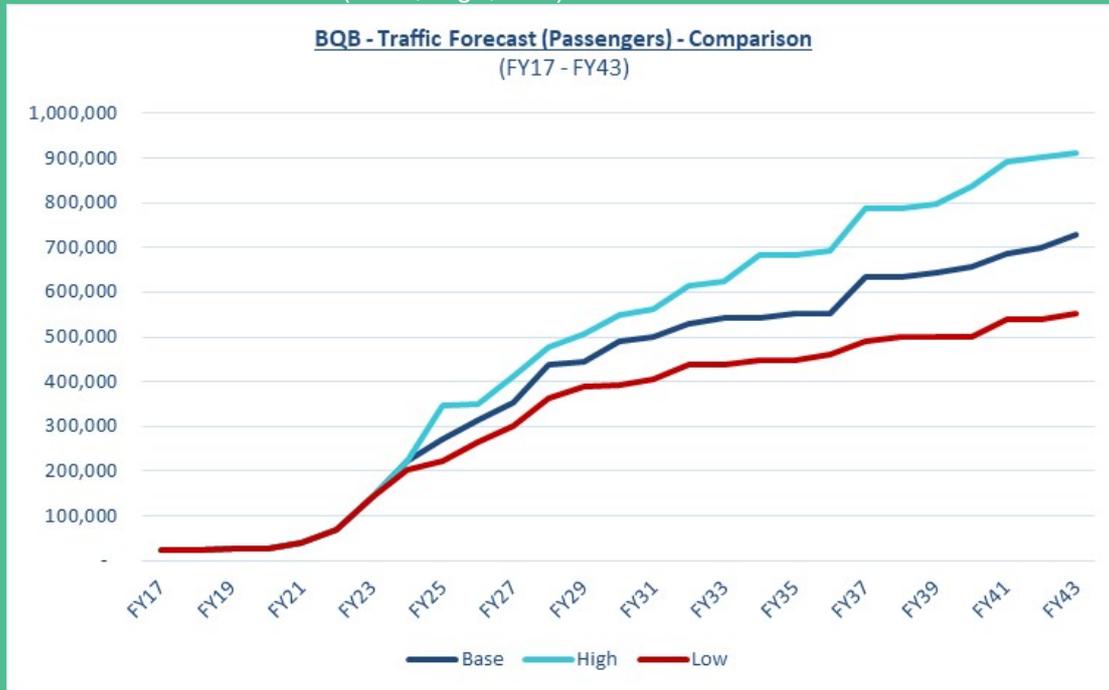
- Domestic Inbound: 50%
- Domestic Outbound: 60%
- International Inbound: 10%
- International Outbound: 20%

The combination of the above allowed us, as a first step, to build up an O&D market ranking for BMRA, and, ultimately to produce base, high and low traffic forecasts for the airport.

O&D Market Ranking for BMRA (FY23)

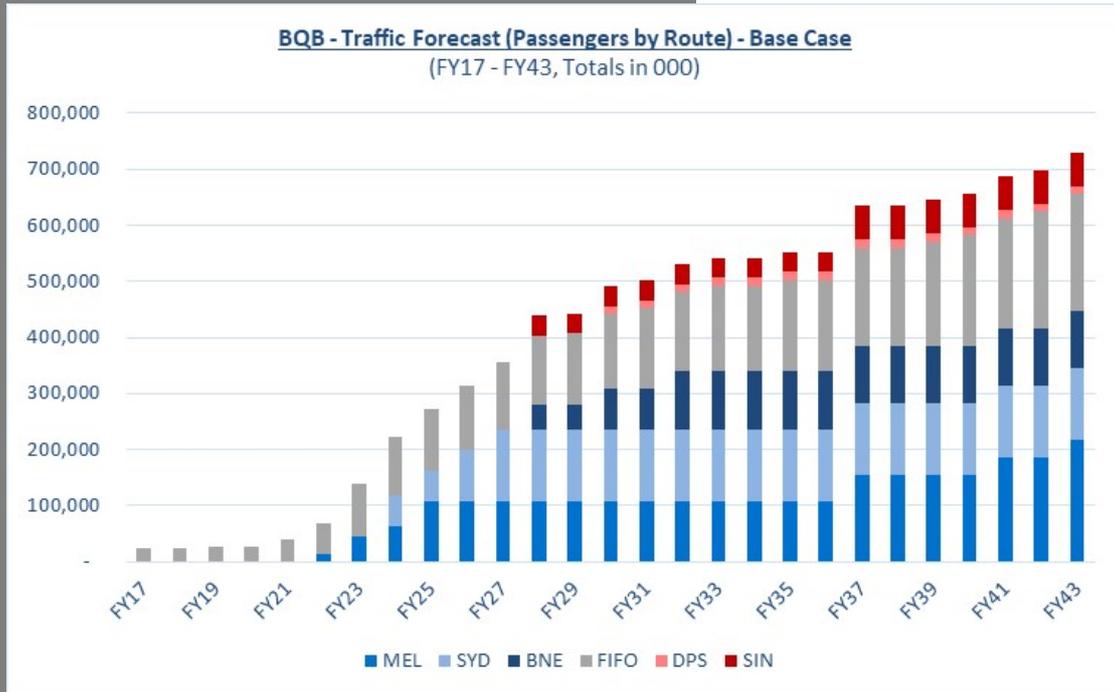


Traffic Forecast for BMRA (Base, High, Low)



Finally, the graph below shows how traffic is expected to develop across BMRA's route portfolio over the next 20 years:

Traffic Forecast by Route (Base Case)

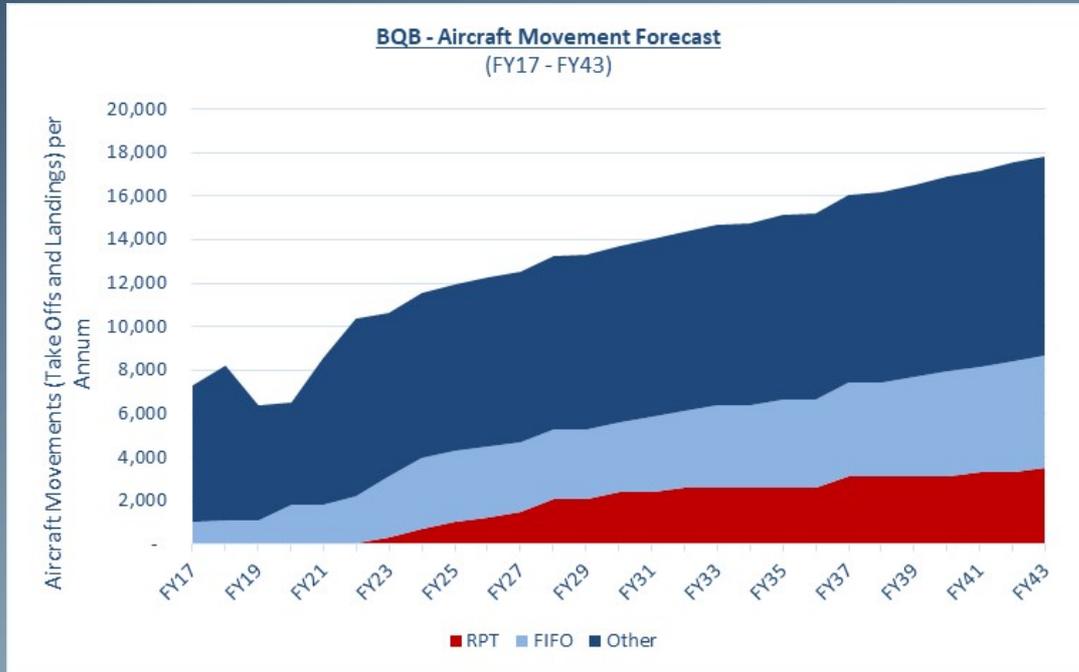


### Aviation Activity Forecast

FIFO and RPT forecasts were translated into annual aircraft movements associated to these two categories. Aircraft movements in relation to other purposes (e.g. GA) have been in decline over recent years which is understood to have been primarily a function of many GA operators relocating to Bunbury Airport which offers more appropriate and better priced infrastructure to this segment.

As this historic lack of focus on the GA sector is set to be corrected during the current planning period, we see other aircraft movements return to historic levels and then increase at an annual rate of 1%.

Aviation Activity/ Aircraft Movement Forecast (Base Case)



All of the above is summarised on an annual level in the table below:

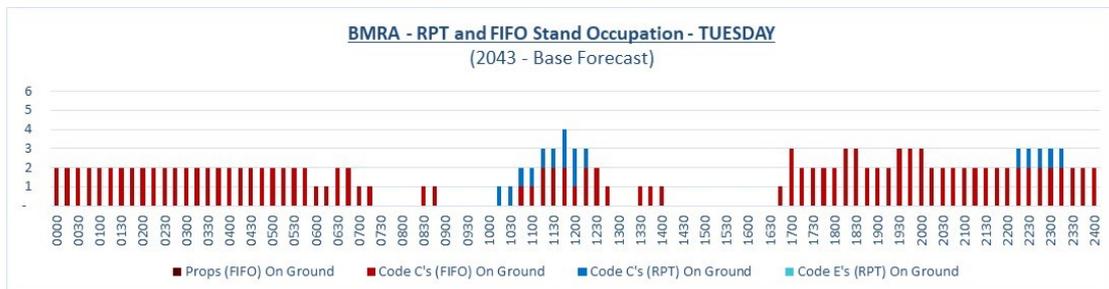
	Low Scenario - Traffic			High Scenario - Traffic			Base Scenario - Traffic			Base Scenario - Movements			
	RPT	FIFO	Total	RPT	FIFO	Total	RPT	FIFO	Total	RPT	FIFO	Other	Total
FY17	-	23,333	23,333	-	23,333	23,333	-	23,333	23,333		1,036	6,274	7,310
FY18	-	23,964	23,964	-	23,964	23,964	-	23,964	23,964		1,068	7,144	8,212
FY19	-	25,241	25,241	-	25,241	25,241	-	25,241	25,241		1,074	5,310	6,384
FY20	-	26,321	26,321	-	26,321	26,321	-	26,321	26,321		1,810	4,724	6,534
FY21	-	40,206	40,206	-	40,206	40,206	-	40,206	40,206		1,810	6,712	8,522
FY22	13,951	55,553	69,504	13,951	55,553	69,504	13,951	55,553	69,504	80	2,132	8,160	10,372
FY23	45,526	94,368	139,894	45,526	94,368	139,894	45,526	94,368	139,894	343	2,763	7,532	10,638
FY24	116,814	87,360	204,174	116,814	104,832	221,646	116,814	104,832	221,646	728	3,224	7,607	11,559
FY25	132,289	91,000	223,289	236,455	109,200	345,655	163,239	109,200	272,439	1,040	3,224	7,683	11,947
FY26	168,897	94,640	263,537	236,455	113,568	350,023	199,847	113,568	313,415	1,248	3,224	7,760	12,232
FY27	202,676	98,280	300,956	295,028	117,936	412,964	236,455	117,936	354,391	1,456	3,224	7,838	12,518
FY28	260,646	101,920	362,566	345,104	132,496	477,600	316,421	122,304	438,725	2,080	3,224	7,916	13,220
FY29	284,670	105,560	390,230	369,128	137,228	506,356	316,421	126,672	443,093	2,080	3,224	7,995	13,299
FY30	284,670	109,200	393,870	407,795	141,960	549,755	359,747	131,040	490,787	2,392	3,224	8,075	13,691
FY31	284,670	120,120	404,790	407,795	152,880	560,675	359,747	141,960	501,707	2,392	3,493	8,156	14,041
FY32	318,449	120,120	438,569	451,122	163,800	614,922	389,034	141,960	530,994	2,600	3,493	8,238	14,330
FY33	318,449	120,120	438,569	451,122	174,720	625,842	389,034	152,880	541,914	2,600	3,761	8,320	14,681
FY34	318,449	131,040	449,489	497,547	185,640	683,187	389,034	152,880	541,914	2,600	3,761	8,403	14,765
FY35	318,449	131,040	449,489	497,547	185,640	683,187	389,034	163,800	552,834	2,600	4,030	8,487	15,117
FY36	318,449	141,960	460,409	497,547	196,560	694,107	389,034	163,800	552,834	2,600	4,030	8,572	15,202
FY37	347,735	141,960	489,695	580,508	207,480	787,988	459,483	174,720	634,203	3,120	4,299	8,658	16,077
FY38	347,735	152,880	500,615	580,508	207,480	787,988	459,483	174,720	634,203	3,120	4,299	8,744	16,163
FY39	347,735	152,880	500,615	580,508	218,400	798,908	459,483	185,640	645,123	3,120	4,567	8,832	16,519
FY40	347,735	152,880	500,615	606,794	229,320	836,114	459,483	196,560	656,043	3,120	4,836	8,920	16,876
FY41	377,022	163,800	540,822	661,706	229,320	891,026	490,434	196,560	686,994	3,328	4,836	9,009	17,173
FY42	377,022	163,800	540,822	661,706	240,240	901,946	490,434	207,480	697,914	3,328	5,105	9,099	17,532
FY43	377,022	174,720	551,742	661,706	251,160	912,866	521,384	207,480	728,864	3,536	5,105	9,190	17,831

## Busy Day and Busy Hour Assessment

Finally, we developed synthetic schedules for the last year of the forecasting period, 2043, which enabled us to define busy day and busy hour schedule and passenger throughput patterns. Whilst, at times, this is done via applying multipliers to busy day and busy hour patterns in the base year, we have long maintained, that this doesn't do the issue full justice. Schedules need to be created in a realistic manner, keeping the nature of the operation (e.g. FIFO and RPT), departure and arrival times at outstations as well as aircraft rotational parameters in mind.

Tuesday turned out to be the busy day in 2043 with up to four Code C aircraft on the ground simultaneously. However, the development of flights that results in the peak would develop between 2028 and 2033. As such the situation with 4 aircraft on the ground in the peak period would be seen from 2033

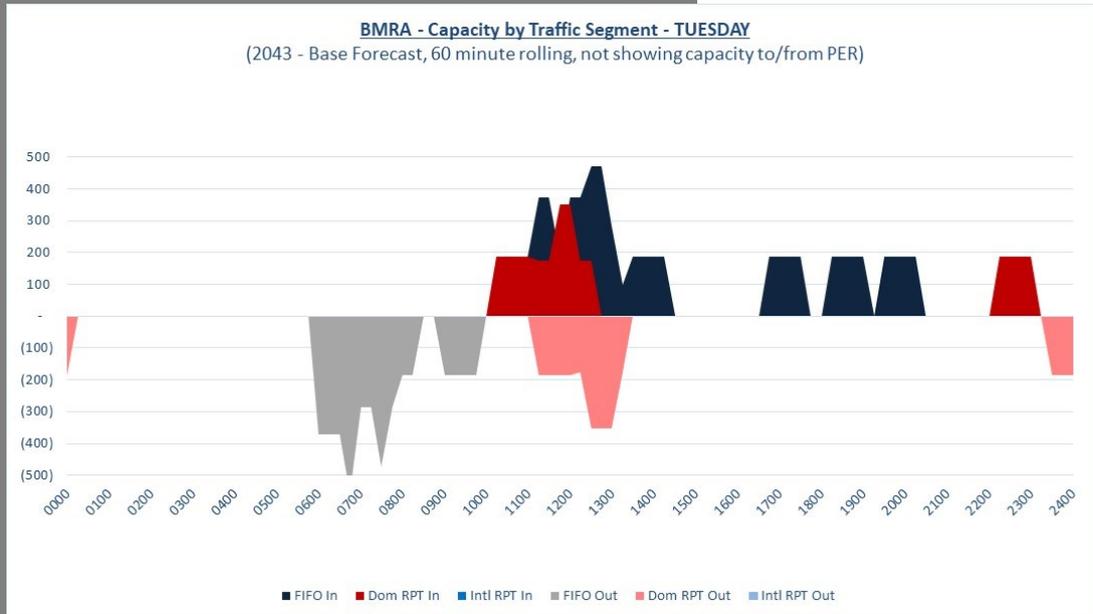
### Busy Day Assessment (Base Case)

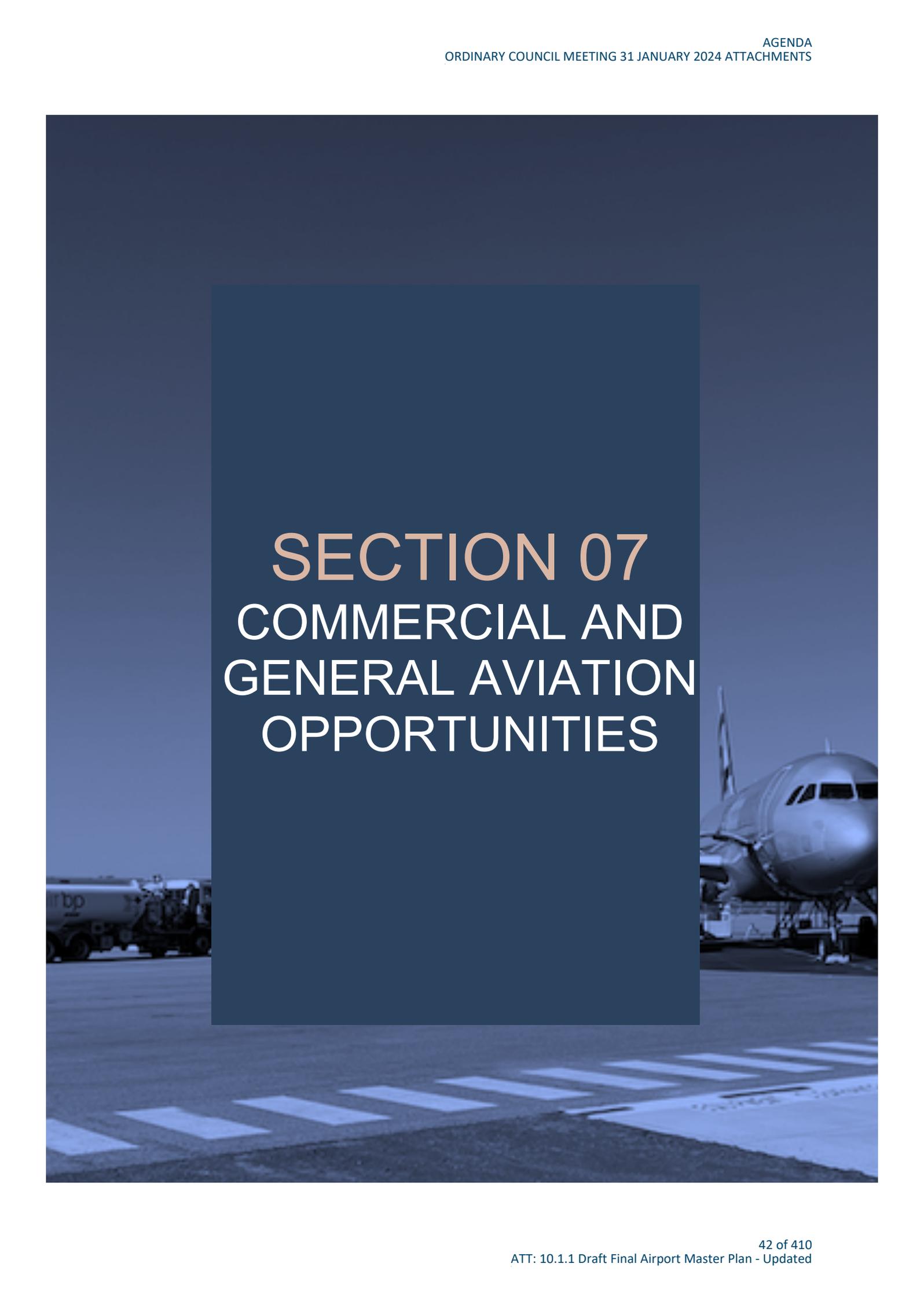


“Schedules need to be created in a realistic manner, keeping the nature of the operation (e.g. FIFO and RPT), departure and arrival times at outstations as well as aircraft rotational parameters in mind.”

Whilst FIFO and RPT departure passenger loads are separated across the day (FIFO departures occur in the morning whereas RPT departures are concentrated around lunchtime), there is a distinct arrival peak of up to 800 passengers, driven by both FIFO and RPT operations, between 1200 and 1300hrs. With the short turnaround times of Code C aircraft used for FIFO and RPT, aircraft are able to arrive and depart within an hour and therefore the peak hour passenger demand is a result of up to 5 aircraft operations but only 4 aircraft happen to be on the ground at any one time during the peak hour. The FIFO aircraft would arrive and deplane the inbound FIFO passengers. The aircraft would then turn around and depart empty. The departure peak period develops to 3 departing FIFO aircraft in 2033 with up to 502 passengers. The nature of FIFO flights is such that flight schedules work around the shift changes at the mine sites. The departure peak is therefore expected to grow. Whilst shift changes currently occur on Tuesdays, Wednesdays and Thursdays only, we do expect Mondays to be added over time and, consequently, added Monday FIFO movements to our longer term forecast.

Busy Hour Assessment (Base Case)





# SECTION 07 COMMERCIAL AND GENERAL AVIATION OPPORTUNITIES

# 07

## Commercial and General Aviation Opportunities

The expansion of BMRA in 2018 has resulted in undeveloped land being prepared for commercial development. The new General Aviation aprons present a large number of airside lease lots and the Central Apron is planned to be transformed into a commercial precinct once the new terminal has been constructed. Increased number of Code C aircraft movements and the prospect of international flights will present the opportunity for a number of businesses to establish themselves at the airport to support the airline activities.

The term General Aviation refers to all types of aviation activity that is not scheduled or Regular Passenger Transport. As such, a considerably wide variety of aerial activity and commercial aviation activity falls into the General Aviation category.

### Private and Recreational Aviation

The BMRA is home to the Busselton Aero Club. The Aero Club conducts flight training for Recreational Aviation Australia (RAAus) pilot certificates as well as the CASA Recreational Pilots Licenses (RPL) and CASA Private Pilots Licence.

BMRA with its long runway and non-precision instrument approaches, was a popular airport for flight training, both for the local Busselton Aeroclub as well as other flight training schools based in Perth and Jandakot. With restrictive environmental conditions placed on the BMRA and community noise complaints reaching a high in 2010, the then Shire of Busselton restricted flight training at the BMRA with the exception of the Busselton Aeroclub. The BMRA Noise Management Plan, endorsed by the Environmental Protection Authority in 2012 placed strict limits on the nature and type of flight training that can be performed at the BMRA, which for the most part remain in place today. The overall impact of the restrictions on flight training and acquiring of the available BMRA hangar space, is that General Aviation at BMRA has dramatically reduced over time. However, with the high number of FIFO flights and RPT services, the BMRA is not reliant on General Aviation activity.

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The Busselton Aero Club does have plans to grow its membership and increase small aircraft activity at the BMRA. More importantly, the Aero Club activity will provide the base market demand for other aviation businesses that will be important to the development of aviation business and commercial activity in the General Aviation Precinct.

## Aerial General Aviation Business

General aviation business activity at BMRA is currently very low with a few businesses operating from the GA hangars on the north side of the Southern Apron.

The airport has 3 primary GA business areas; part of the Northern Apron, the Central Apron and the original GA area. No development has yet taken place on the Northern Apron.

## Aerial Fire Fighting

The Western Australia State Government Department of Fire, Emergency and Rescue Services (DFES) have contracted a private aviation service provider to provide aerial firefighting with Large Air Tankers (LAT) based at the BMRA during the firefighting season. BMRA is ideally located to protect the South West region and accommodate the LATs as the airport has unconstrained airspace and good runway capacity for the LAT operations and space for associated facilities.

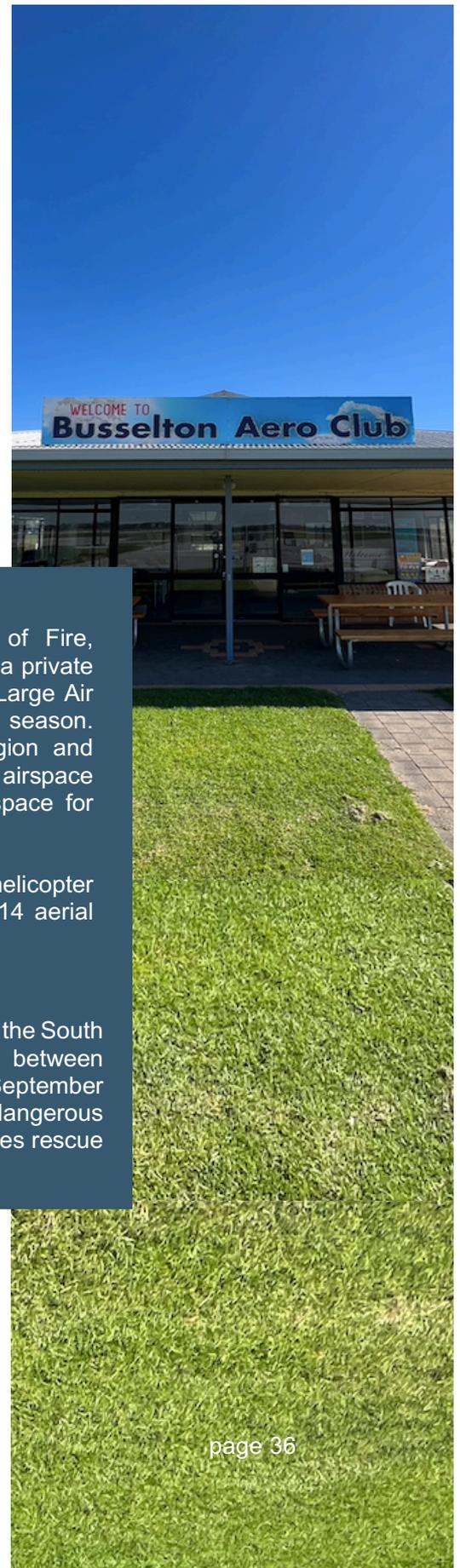
DFES also contract an aviation service provider to provide helicopter firefighting aircraft based at the BMRA, including 2 Bell 214 aerial firefighting helicopters

## Lifesaving

Westpac Rescue Helicopter operates out of BMRA as part of the South West service. This service provides aerial surveillance between Bunbury and Hamelin Bay. The service operates between September and April to identify sharks near popular tourist areas and dangerous sea conditions. The WestPac Helicopter Service also provides rescue services as needed.

## Charter Flights

A small number of private charter companies operate out of the BMRA offering sightseeing tours around the Busselton Margaret River region, particularly into the wineries. This includes helicopter operators, a Tigermoth flight experience and smaller GA style charters



## Crop Spraying

Currently, there is no active aerial crop seeding/spraying operations from the BMRA. Given the proximity of the wine region and crop growing to the west and south of Busselton, there is an opportunity for aerial work related activities to base themselves at BMRA

## Light Aircraft Maintenance

At present, aircraft maintenance is not offered at the BMRA. With the development of the Aeroclub, there is significant potential for businesses to support this activity. Light aircraft maintenance businesses would not only support the Aero club but also provide services for surrounding aerodromes and in particular Bunbury Airport which is a busy RAA and GA flight training airport. Note that a good and reputable AMRO, particularly one that has developed specialised services or supports unique and specialist aircraft, will attract business from across the country, rather than being limited to neighbouring towns and cities. For example, specialist aircraft restoration, recovering and painting services are not offered by many AMRO organisations. Organisations offering such services can have significant waitlists and therefore, there is scope for additional capacity through new organisations establishing themselves at airports where such services are currently not offered.

## Heavy Aircraft Maintenance

Larger passenger carrying aircraft such as those used for RPT activities will undergo various types of maintenance activity. Airlines will carry out large aircraft maintenance at their primary base of operations or at an airport that has significant aircraft movements, reducing aircraft downtime and transportation costs. For the larger RPT aircraft, the market for heavy maintenance is world-wide. Carriers are able to ferry their aircraft considerable distances to other countries in order to save on the cost of maintenance. However, the aircraft being serviced need to be able to fly the distances necessary to get to the maintenance base. As such, overseas maintenance is possible for larger RPT aircraft such as the B737/A320 or larger, but not for the smaller RPT aircraft such as Q400 or Saab 340b. The smaller RPT aircraft therefore need to have all maintenance activity conducted domestically.

Most small carriers will provide some level of maintenance at or near their primary base of operations. Generally, RPT operators have route networks that extend from the state capital cities and therefore their preferred base for maintenance activity would be at that capital city airport. However, space at Perth Airport and Jandakot Airport are becoming increasingly constrained and therefore expensive.

AMROs in the greater Perth area may look to BMRA as a potential base for maintenance activity where there is plenty of available land and flying distances are short. As of writing this Master Plan, two such organisation have approached the City with interest in basing some of their maintenance activity at BMRA for large turboprop aircraft. With increased activity at Perth Airport, there is additional scope for BMRA to attract more aircraft maintenance activity and therefore establish itself as a maintenance hub with plenty of expansion capability, also attracting skilled labour to the region.

### Spare Parts and Aircraft Sales

Supporting the light aircraft maintenance businesses as well as aircraft owners, firms specialising in the sale of aircraft parts, engines and avionics often choose airports as suitable locations. These businesses, unless also providing aircraft services, can be located in lots and facilities that do not have airside access. With online sales increasing, aircraft parts suppliers do not necessarily need to be located on an airport or on a busy airport.

The sale of new light aircraft has been low for a significantly long period in Australia. However, the introduction of the Light Sport Aircraft (LSA) category has started to see the introduction of new aircraft into the market at a lower price than traditional certified aircraft. With the lower cost of ownership with the LSA category, the number and variety of aircraft available for this market is likely to increase. In addition, the LSA category is expected to undergo a major overhaul in the next few years as the US FAA works through its Modernisation of Special Airworthiness Certification (MOSIC) which is expected to expand the LSA category to larger aircraft. If successful, there will be pressure worldwide for regulators in other countries including CASA to adopt some or all of the changes implemented in the US. If so, it could see more and larger aircraft become less costly to purchase, resulting in more demand and sales. The opportunity for BMRA, comes from an organisation looking for airside hangar space where they can set up hangar/showrooms for aircraft sales as well as take prospective buyers on test flights as part of the sales programme. Although, an organisation selling aircraft, may wish to locate, near large population centres, they will also want cheaper rent, a less busy airport and easy access to a training area where they can demonstrate the capabilities of the aircraft to a prospective buyer. BMRA has all these characteristics and is not far from the large population centres of WA.

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## Fuel Sales

There is currently a fuel farm supplying Jet-A1 and Avgas operated by AirBP. The fuel facility is located beside the old passenger terminal on the Central apron. The Avgas facility is now older than twenty years and due for replacement in the near future. With the Aero Club expressing interest in establishing a fuel facility to provide Avgas, closer to their future club rooms and hangars situated on the Northern apron it is likely to assume that the current Avgas facility would be decommissioned and a new facility located in the GA precinct.

The Jet-A1 tanks and bowser are relatively new but are not fixed permanently to the current location and therefore could be moved to make way for hangar development on the current site or to accommodate growth in aircraft size using the Southern Apron.

The current location for Jet-A1 fuel is suitable and a good location based on the current type and mix of traffic. RPT and FIFO aircraft take-on fuel at BMRA and therefore, as long as the RPT and FIFO aircraft operate from the Central Apron, the current location should remain. Once the FIFO and RPT traffic move to the Southern Apron, the AirBP fuel vehicle will need to shuttle back and forth. At the point when widebody aircraft start to operate to BMRA, refuelling via vehicles is not practical and therefore hydrant refuelling will be needed on any aircraft stand supporting widebody aircraft. At this point, due to the cost of piping between a fuel facility and the hydrant, a new fuel facility would need to be established.

## Freight and Freight Forwarding

With regular scheduled flights developing and growing, there is an opportunity for air service providers to carry freight. With narrowbody aircraft, there is not a significant business opportunity for freight activity but as aircraft size increase or lucrative freight opportunities develop such as a Singapore flight, the need for freight facilities will increase. To support freight activity, an organisation will need to develop a suitable freight processing facility in an airside lease lot. With the potential for international freight, a facility will need to be developed such that a freight processing centre can be secured. In addition, as the high value outbound freight is likely to be some form of seafood and fresh perishables, suitable cold storage would be needed. It is possible for the City to develop the freight processing warehouse with individual freight forwarding firms to take up other leases on the airport. However, as the low volume of freight to be processed for domestic activity may not justify the cost of the facility, the City may seek to encourage a private organisation to develop the freight processing facility or develop it in partnership with the City.

## Mail and Courier

As mail and parcel shipping increasingly occurs by air, it has become more likely for courier and mail businesses to take advantage of RPT services. Either a new freight operator or a well established organisation may take up space at the airport to support the mail and courier activity in the area. With regular RPT flights a reliable network can be established and therefore providing an opportunity for a courier, mail or freight operator to take up a lease on the airport.

## Airline Support

There are a number of businesses that support RPT aircraft operations such as Flight Catering, aircraft cleaning, line maintenance, etc. These businesses would need to have space on the airport at some point in time. However, the need for many of these businesses, will not develop until international services start or towards the end of the Master Planning period or beyond. Facilities for these businesses have few special requirements and will be in the form of lease lots either landside or airside

## GSE Maintenance

BMRA currently has a single RPT operation conducted by Jetstar who operate 4 services a week on an Airbus A320 aircraft. In addition to the Jetstar service, BMRA has 31 departures to the Pilbara region a week operating FIFO traffic on a mix of different aircraft types including turboprop aircraft through to A320. Therefore, ground handlers need to have a wide variety of GSE equipment on hand to accommodate these aircraft types. A constraint identified with the current operation is the amount of suitable GSE storage around the Central Apron to accommodate the GSE equipment when not in use. Although not a problem yet, space and facilities for the ground handling organisations needs to be identified to maintain the GSE equipment.

## Aircraft Owner and Pilot Support

Many airports that are destinations for pilots or aircraft owners have been successful at developing what is known as a Fixed Base Operator (FBO). This can take many forms but, in this instance, would be an organisation that attends to the needs of aircraft owners, pilots or individuals travelling on their own usually in business jets or turboprop aircraft. An FBO would typically offer a lounge for the clients to use while waiting, a pilot briefing room, restroom facilities, catering, permit processing and even local transport. Some will also offer business services and meeting room rental. An FBO may also provide aircraft ground handling, lounges and support to pilots and clients waiting for their aircraft but may also provide hangarage for visiting aircraft and aircraft rental.

## Specialist Aviation Business

With the large amount of space available for rent in the Northern Apron, there are opportunities for specialised aviation businesses. Specialised aviation businesses such as aircraft restoration, interior refurbishment, painting or aircraft parts manufacturing, have a nationwide market and therefore could locate at BMRA. Busselton is an attractive location to live with a growing workforce and the airport has ample space for growth of a business. The City has an opportunity to attract such businesses to the region.



# SECTION 08 FUTURE DEVELOPMENT REQUIREMENTS

# 08

## Future Development Requirements

### Design Aircraft

The Design Aircraft is a concept that is used to assist in the planning of airports to ensure facilities are suitable and appropriate safety standards are used. The characteristics of the Design Aircraft set the requirements for the future development such as runway length required for operating, Obstacle Limitation Surfaces, pavement strength, taxiway widths and clearances. The Design Aircraft for the 2016 Master Plan was a progressive growth of aircraft used for design starting with the A320/B737 in Stage 2, growing to the A330/B787 in Stage 3 and ultimately the Boeing B747-800 in Stage 4, a Code F aircraft. As such, many of the decisions concerning development of the BMRA in the 2016 Master Plan were based on accommodating the Code E or the Code F aircraft. Some of the development that took place was based on the needs of the Stage 2 Design Aircraft. However, where facilities would need to be moved, rather than expanded, the Stage 3 or 4 Design Aircraft was chosen, allowing the airport to safeguard future development.

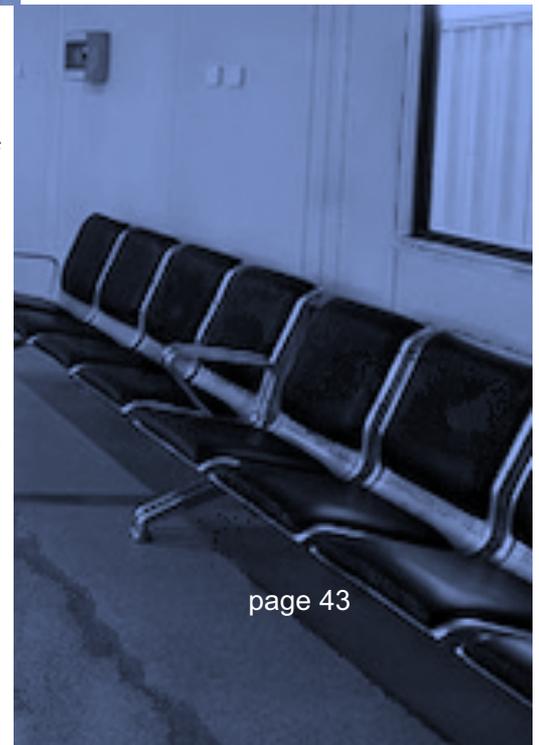
For this 2023 Master Plan, the largest aircraft forecast for operation in the Base Case Forecast, is the A320/B737 sized aircraft or a Code C aircraft. As such, for most facilities the Code C aircraft is used for planning purposes. However, the High Forecast discussed in Section 6, identifies the possibility of international carriers initiating services using the larger A350 and B787 aircraft which are both Code E aircraft. To accommodate the possibility of an international service using a Code E aircraft and to safeguard the ability for BMRA to be used as an alternate aerodrome, some consideration and accommodation of these aircraft have been made. Use of a Code C as the design aircraft but also ensure where necessary, full Code E clearances have been applied, ensures some flexibility for future planning.

“For this 2023 Master Plan, the largest aircraft forecast for operation in the Base Case Forecast, is the A320/B737 sized aircraft or a Code C aircraft.”

The 2016 Master Plan identified a potential for the A380 or B747-800 to use BMRA as an alternate airport and therefore aprons were setback from the runway to accommodate the Code F aircraft operations. The A380 and B747-800 aircraft have failed to generate significant sales numbers and have not been able to operate at seat costs lower than aircraft such as the A350 and B787; consequently, Airbus and Boeing have ended production of their very large Code F aircraft types. Most carriers prefer to operate smaller Code E, twin-engine aircraft rather than the high-capacity Code F aircraft. This is not to exclude the potential for Code F aircraft in the future but with the decline in A380 operations and the lack of new very large aircraft entering the world's fleets, the need to have Code F facilities to operate as an alternate airport diminishes. The forecast traffic does not indicate such an aircraft would operate to BMRA and therefore it is not being used in the future design. However, where facilities have been established on the basis of Code F capabilities, those safeguards will remain in place. In the event a Code F aircraft does need to land at BMRA, safeguarding for that aircraft can be handled operationally and procedurally, rather than having facilities constructed and maintained for an arrival that may not occur. It is therefore recommended that BMRA have a set of procedures which include what facilities are Code F compatible and which could not be used for such an operation.

## Passenger Terminal

The 2016 Master Plan identified the need for a new terminal. The new terminal was to be designed to accommodate 350 passengers in the peak hour and be capable of accommodating the demands for 2 Code 4C aircraft operating simultaneously. The final design resulted in a 4900m<sup>2</sup> terminal building. The Jetstar flight from Melbourne has since been established (2022) and has demonstrated good demand for the sector. Further route sectors are forecast for the BMRA such as to Sydney and Brisbane. Demand for a BMRA - Sydney flight is sufficient for a service to start in the 2024/5 period and BMRA to Brisbane in approximately 2028. For the peak day, it is expected that 3 FIFO arrivals will also occur during the peak hour. The Peak Hour is forecast for 4 aircraft on the ground at one time but over the entire hour, it is expected that there would be 2 RPT flights and 3 FIFO flights, operating simultaneously.



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This equates to a peak hour arrival flow of 837 total passengers and a departure flow of 335 passengers. The arrival flow however, is comprised of 335 RPT passengers and 502 FIFO passengers, The terminal requirements for FIFO arrivals are minimal as most FIFO passengers have hand baggage only, do not wait around collecting baggage or renting vehicles and therefore only corridor/walkway capacity is required to accommodate the additional 502 FIFO passengers during the RPT peak arrival period. As such, the passenger terminal needs to be sized for 335 RPT passengers plus corridor/walkway and some terminal facility usage for the FIFO arrivals. The departure peak is defined as a morning FIFO peak comprising 3 aircraft leaving within 60 minutes. This departure peak presents a demand for 502 passengers in the peak hour. The passenger terminal should therefore be sized to accommodate 335 arrival passengers and 502 departing passengers for the 2033 year. Based on general expectations of suitable Level of Service (LOS) C for domestic passenger terminals, a passenger terminal of 11,718m<sup>2</sup> would therefore, be required.

The forecasts identify a demand for international flights starting in approximately 2028. The international flights would likely not coincide with domestic or FIFO peaks due to the sector flight times and schedule patterns at the destination airports. As such, the terminal needs to be able accommodate a single Code C international arrival with a capacity of 180 to 200 seats resulting in 167 to 180 international arrival passengers and 167 to 180 international departure passengers. To optimise the terminal building area, design should consider the use of operable walls and doors as well as swing gate concepts. This will provide the separation needed between arrivals and departures, including passenger processing for immigration and customs for the international flights without interfering with domestic traffic or having significant space dedicated to international passenger processing.

With peaky demand patterns that are typical at smaller airports, the terminal will undoubtedly experience periods of high passenger flows, quickly followed by periods of no activity. Fully accommodating the peak period demand for the forecast horizon year, will result in a significant build cost for the terminal. With the peak demand for the planning horizon being achieved in 2028 to 2033 and remaining constant throughout the rest of the planning period, the initial build of the terminal should be able to accommodate the long term 2043 demand. Expansion of the terminal would not be required until additional flights occur in the peak periods resulting in a need for additional terminal space. This expansion would not be needed until some point beyond the Master Plan. Expansion of the Passenger Terminal has been shown in Exhibit 5 which provides the Passenger Terminal Precinct area and identifies the expansion reserves for the longer term Passenger Terminal.

## Quantity or Area for Airport Facilities- Departures

<p>412m<sup>2</sup></p>  <p>Check-In Hall</p>	<p>8 units</p>  <p>Check-In Counters</p>	<p>325m<sup>2</sup></p>  <p>Security Screening</p>	<p>2 units</p>  <p>Passenger X-Ray &amp; Magnetometers</p>
<p>850m<sup>2</sup></p>  <p>Departure Lounge</p>	<p>432 units</p>  <p>Seats</p>	<p>40m<sup>2</sup></p>  <p>Emigration<sup>2</sup></p>	<p>2 units</p>  <p>Emigration Positions<sup>2</sup></p>
<p>283m<sup>2</sup></p>  <p>Int. Departure Lounge<sup>3</sup></p>	<p>144 units</p>  <p>Seats<sup>3</sup></p>	<p>146m<sup>2</sup></p>  <p>Baggage Screening and Make-Up Area</p>	

<sup>2</sup> Only needed for international flights

<sup>3</sup> Due to the offset between domestic peak and international peak, may be shared facilities for both Domestic and International by using swing gates or operable walls

## Quantity or Area for Airport Facilities- Arrivals

<p>120m<sup>2</sup></p>  <p>Immigration Hall<sup>2</sup></p>	<p>3 units</p>  <p>Immigration Positions<sup>2</sup></p>	<p>244m<sup>2</sup></p>  <p>Baggage Hall</p>	<p>1 unit</p>  <p>Baggage Carousels</p>
<p>183m<sup>2</sup></p>  <p>Arrivals Hall</p>	<p>76m<sup>2</sup></p>  <p>Baggage Break-down</p>	<p>244m<sup>2</sup></p>  <p>Int. Baggage Hall<sup>3</sup></p>	<p>1 unit</p>  <p>Int. Baggage Carousels<sup>3</sup></p>
<p>41m<sup>2</sup></p>  <p>Customs and Quarantine</p>	<p>2 units</p>  <p>Customs and Quarantine Counters</p>		

<sup>2</sup> Only needed for international flights

<sup>3</sup> Due to the offset between domestic peak and international peak, may be shared facilities for both Domestic and International by using swing gates or operable walls

## Southern Apron

Constructing the Passenger Terminal beside the Southern Apron will move all RPT and FIFO operations to the Southern Apron. Current peak aircraft stand demand occurs between 6:00am and 7:35am on a Tuesday morning with 2 FIFO aircraft operations. The forecast morning departure peak is expected to grow by a single FIFO departure resulting in 3 Code C FIFO departures within a 60 minute period. The Peak Hour arrival period is forecast to have 2 RPT arrivals from Melbourne and Sydney, as well as 3 FIFO arrivals. Code C turnaround times are short with typical schedules of 30 to 45 minutes on stand. As such, the peak stand demand is for 4 Code C aircraft parking positions. To account for potential delays or aircraft arriving early and possible technical issues causing aircraft to be unable to depart, there is a high probability for 5 aircraft to be on stand. Therefore, the BMRA needs to have 5 aircraft parking stands available. It is also possible for carriers to schedule a turnaround at a low activity airport such as BMRA, to be longer than normal to minimise holding and fuel burn when arriving back at the busy hub airport. If this was to occur, the stand demand could quickly increase to 5 aircraft on the ground during the peak period, necessitating 6 aircraft stands be available.

The current Southern Apron has a maximum capacity of 4 Code C aircraft stands. Therefore, the Southern Apron should be expanded before the demand for additional stands is reached in 2033, when 5 aircraft stands should be available. Exhibit 6 illustrates the expanded Southern Apron accommodating 5 aircraft stands. As identified in the Demand Forecasting section of this report, the peak hour forecast, reaches 4 aircraft stands and then growth occurs through infilling additional days and expanding outside the peak period. As such, from the 2033 period through to the end of the forecast period, the peak demand remains at 4 Code C aircraft on stand with a single stand available to accommodate delays or technical issues.

The forecasts have identified the possible introduction of international flights as early as 2028. The aircraft used for these flights in the Base Case forecast are narrowbody aircraft i.e. Code C. However, the high forecast identifies the possible introduction of a widebody aircraft for international routes. The apron was designed to accommodate Code E aircraft and therefore, the future apron should retain the ability for 1 Code E aircraft to park on the apron in the event that the international sectors grow faster than forecast or the carrier chooses to introduce a widebody aircraft on the route. With the international flights being out of the peak periods, the use of multiple Code C stands to accommodate a Code E aircraft is possible.

The City needs to provide the existing 4 Code C stands plus a contingency of a single stand by 2033 as identified. However, from the forecast of aircraft in the peak period, it is conceivable that delays to more than 1 aircraft would quickly result in a demand for more than 5 aircraft stands or carriers choose to have longer turnarounds at BMRA, increasing the stand demand. In addition, the City should consider how much additional infrastructure they intend to construct at once and whether the development of 6 Code C stands should be developed for the 2033 period rather than the 5 necessary. Exhibit 7 shows the development of 6 Code C aircraft stands and how a single Code E stand could be accommodated. The Code E aircraft stand has been located on the new pavement, pushing it as far south as possible and close to the fuel storage depot, as it is not practical to refuel a Code E sized aircraft using mobile refuelling vehicles.



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## New Fuel Facility

The current fuel storage and dispensing facility would be suitable throughout the forecast period. However, with RPT and FIFO flights shifting to the Southern Apron when the Passenger Terminal opens, mobile fuel vehicles would need to travel back and forth from the refuelling facility to the Southern Apron. This distance is not excessively long but if it becomes inconvenient for the refuelling operator and potentially adds to refuelling times. A new location has been identified south of the Southern Apron for a future fuel storage compound.

The Base Case Forecast discussed in Section 6 of this Master Plan, only identified Code C aircraft operating to BMRA. However, the High Forecast identified the need to accommodate widebody aircraft should the international flights be operated using such aircraft. If so, Council should review the installation of a hydrant system and consider costs at the next major forecasting exercise. Widebody aircraft burn considerably more fuel and therefore would require the uplift of more fuel at BMRA. Such a prospect would likely cause an increase in the volume of fuel stored and the need for hydrant refuelling. A mobile refuelling vehicle is able to uplift approximately 1,000L per minute and therefore, Code C aircraft taking on approximately 10,000L, can be refuelled using a mobile refuelling vehicle without delays or refuelling impacting aircraft turnaround times. However, a widebody aircraft operating to Singapore would uplift 50,000L to 60,000L of fuel. Using a mobile refuelling vehicle at 1,000L per minute, the refuelling process would become a limitation on the turnaround time. A fuel hydrant system, can uplift fuel at a rate of approximately 2,300L per minute, thereby shortening the turnaround time drastically. As such, if the demand increases to a point where widebody aircraft are going to operate to BMRA, the City will need to look at the construction of hydrant refuelling in any new aprons. To facilitate the future potential for hydrant refuelling, and thereby reducing the distance from fuel storage to the apron fuel hydrant, the future fuel storage facilities compound has been relocated to the south of the Southern Apron. Exhibit 8 illustrates the new fuel storage facility located and hydrant refuelling location in the apron for Code E aircraft.



## Aircraft Rescue and Firefighting

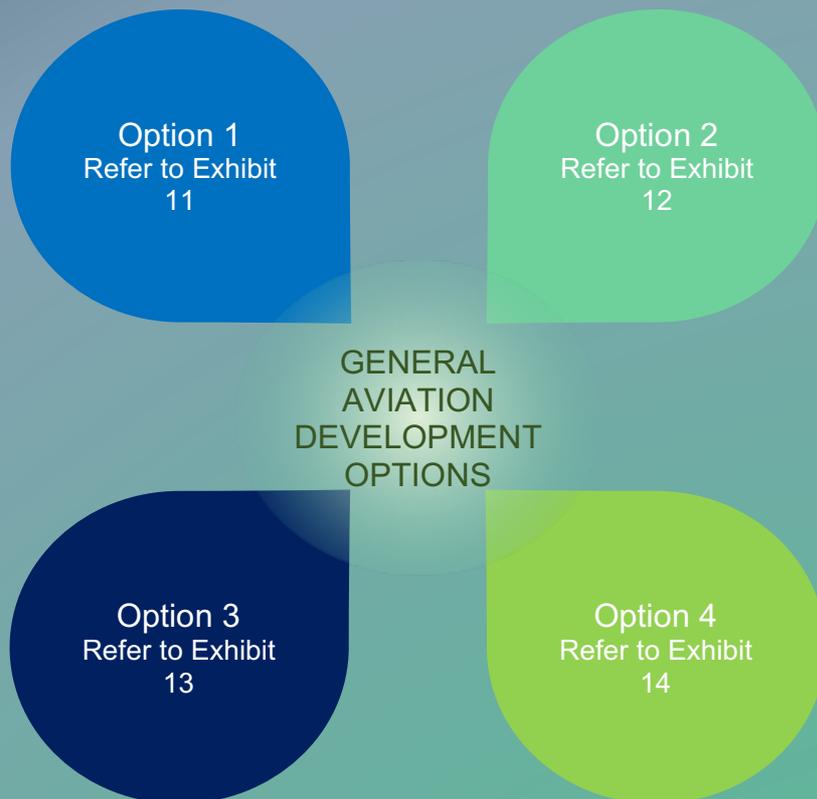
Currently BMRA do not have the passenger volumes necessary to justify establishing an ARFF facility at the airport. However, international services are forecast to occur from 2028 which will trigger the need to have suitable Level 1 ARFF to support aircraft operating the sector. The forecast identifies that initial international services would likely be narrow body operations which would result in the sector being served by a A320/B737 sized aircraft. This size of aircraft would require BMRA to have a Category 6 or 7 ARFF facility and therefore provision would be needed for such a facility. It should be noted however, that the High Forecast identifies the international sector would be operated by a larger widebody aircraft and therefore the eventual size of the ARFF facility may need to be either expandable or built to provide Category 8 or 9 ARFF cover. For Airservices to implement Level 2 ARFF, domestic traffic volumes need to reach 350,000 passengers per annum. The forecasts identify this will be reached early in 2028. A location for the ARFF facility has been identified in Exhibit 9. However, Airservices is currently reviewing the criteria they use to trigger the establishment of an ARFF service at an airport which may change the threshold passenger volume necessary for ARFF. As the international obligation for ARFF is triggered at approximately the same time as the domestic passenger volume reaches the current trigger point, it may be the international flight requirement for ARFF that dictates when an ARFF facility is established.

## GSE Storage and Staging

With the increasing use of Code C aircraft at BMRA, there has been an increasing need for GSE storage and staging areas. The Central Apron was not anticipated as providing the long-term aircraft stands for these aircraft and therefore, was never designed to accommodate the demand for staging and storage of GSE equipment. Where possible, the airport has been able to stage GSE close to the aircraft stands but has had to spread the staging along the face of the apron. As no development has yet taken place on the GA aprons, the ground handlers have been able to store GSE equipment on the GA Apron. Once development starts on the GA Apron, the apron can no longer be used for GSE Storage. With the opening of the Southern Apron, there needs to be both GSE staging near to the apron and a dedicated GSE storage area. As shown in Exhibit 10, the GSE storage area has been located beside the passenger terminal. Initially, GSE storage and staging would be in the areas beside the terminal, along the apron edge. However, expansion of the terminal will reduce the GSE storage and Staging areas over time. Eventually, GSE staging will be along the apron edge and in small areas to the north and south of the terminal with GSE storage located behind the extended wings of the long term development of the terminal building. Storage of GSE has been located beside the terminal with access to GSE Maintenance in the Airline Support Precinct

## Commercial Development Options

With the development of the 2 northern aprons accessed from Taxiway D1 and D2, there are a number of options available for both Recreational and General Aviation. Development of the new Passenger Terminal will move the RPT and FIFO traffic from the Central Apron to the Southern Apron. This will enable the redevelopment of the Central Apron for Commercial Aviation opportunities. As the activity that would take place on the Central and GA Aprons could be similar, there are a number of options that can be considered.



Option 1  
Refer to  
Exhibit 11

The current General Aviation zone located between the South Apron and the Central Apron, is inadequate to accommodate the future GA requirements. The new aprons accessed from Taxiway D1 and D2 have been developed to accommodate all GA activity. Option 1 presented in Exhibit 11, illustrates the GA and Commercial arrangement from the 2016 Master Plan. This arrangement provides 6 Code B sized hangars.

These can be developed as small Code A hangars if that suits the lease arrangements. The 2016 Master Plan developed the 2 aprons in different stages with the apron accessed off Taxiway D2, subject to expansion of the property. The apron accessed off Taxiway D2 can accommodate 19 Code B sized hangars. All hangars in the option are arranged to front the taxilane i.e. perpendicular to the taxilane. They have also been set back to permit an apron in front of each hangar while maintaining suitable clearance requirements.

The 2016 Master Plan included 8 general aviation hangar lots and one apron area and illustrated in Exhibit 11. The Development Project completed in 2018 resulted in two northern aprons and additional hangar lots constructed with access from Taxiway D1 and D2. A revised precinct plan was prepared in 2021 which showed the two northern apron areas and hangar lots. Since, then the layout and size of the hangar lots has been revisited to include a smaller number of lots but of increased area. Option 1 illustrates 19 Code B sized hangars. All hangars in the option are arranged to front the taxilane i.e. perpendicular to the taxilane. They have also been set back to permit an apron in front of each hangar while maintaining suitable clearance requirements

Option 2  
Refer to  
Exhibit 12

The City has been in discussion with the Aero Club to develop the Apron off Taxiway D1 for Aero Club activities, leaving the apron off Taxiway D2 to be progressed by the City for light commercial aviation businesses. A proposal has been developed for the apron accessed off Taxiway D1 that increases the potential number of hangars to 27 Code A hangars.

This also includes a refuelling apron as part of the development. The proposal requires an increase in apron area as the hangars are arranged in a series of cul-de-sacs. The proposal also restricts 17 of the hangars to a maximum wingspan of 12m. For the Aero Club activity, a maximum wingspan of 12m is unlikely to present any restrictions as most LSA and private GA aircraft have wingspans of less than 12m. As the proposal also includes 10 hangars that can accommodate the full Code A clearances, they can accommodate members who need hangars with wingspans up to 15m.

The proposal developed and shown in Exhibit 12, allows the apron accessed from Taxiway D2 to be suitable for small GA business activity such as light aircraft AMROs and aircraft painting.

**Option 3**  
Refer to  
Exhibit 13

Option 3 uses the Code A hangar development proposed in Option 1 for the hangars accessed via Taxiway D1. This provides for 27 Code A hangars for GA and Recreational aircraft. To accommodate GA businesses needing hangars to service aircraft up to Code B sized aircraft, the south side of Taxiway D2 can accommodate 6 Code B sized hangars.

To accomplish this, the rear face of the lease lot has been moved to the south. The northern part of Road 12 has been removed to increase the airside land.

This option also includes Code C hangars permitting smaller Code C aircraft such as the DHC-8 aircraft. Full Code C clearances have been used to separate the hangars however, the property limitation would restrict the depth of aircraft that could be accommodated on the lease lot. In addition, the pavement structure was designed to accommodate small light aircraft. Aircraft accessing the hangars would however, have a weight limit due to the limited pavement strength. When empty, the Q400 has an ACN of between 7 and 8 depending on tyre pressure and therefore the pavement should be able to support empty Q400 aircraft being brought onto Taxiway D2. The option therefore includes 4 limited width Code C hangar lots for aircraft up to the Q400. Having access to Code C accessible lots would enable the City to respond to inquiries and interest in larger lease lots quickly. As this option is prepared to permit Code C aircraft with weight restrictions, to access Taxiway D2, Taxiway D and part of Taxiway E should be widened to accommodate an aircraft with an OMGWS of up to 9m.

Beside the Code C hangar lots, on the eastern side, a facility for RFDS patient transfers has been identified. An area including a building for ambulance parking while waiting for RFDS aircraft to arrive and an apron for both fixed wing and rotary wing aircraft for patient transfers.

The hangars facing Taxiway E are Code B and have been positioned to ensure they do not restrict the airport from developing a full parallel taxiway along Runway 03/21. The safeguarding for a future potential parallel taxiway is beyond the Master Planning horizon and is discussed in Section 11 of this report. To facilitate accessing the potential future parallel taxiway, Taxiway E would need to remain active but clearance between the parallel taxiway and Taxiway E would require Taxiway E is moved north-west. Therefore any hangars that would face Taxiway E would need to be positioned far enough back to ensure clearance from the long term future parallel taxiway alignment rather than the current Taxiway E alignment.

Option 4  
Refer to  
Exhibit 14

Interest has been expressed for larger maintenance hangar space at BMRA. The specific requests were to accommodate large turboprop aircraft such as the DHC-8 series aircraft. The option shown in Exhibit 14 presents a series of larger hangars constructed along the Central Apron, small GA and Recreational Hangars on the more southern of the North Aprons (D1) and Code B hangars on the northern apron (D2).

The pavement strength for the taxiways and apron is high and therefore there are no limitations on aircraft weight taxiing to the lease lots. However, the land along the edge of the Central Apron is not available for immediate release as the current use is for passenger terminals, fuel facilities and Aero Club. In addition, until the Passenger Terminal is constructed adjacent to the Southern Apron, FIFO and RPT operations need to be able to use Bay 8 to 11 restricting a lessee from accessing their hangar at times.



## Freight Precinct

With increasing demand for RPT activity, there is an increase in regular spare capacity travelling between city-pairs. This spare capacity can be used by carriers for freight. With the current Jetstar service operating 4 times per week to Melbourne, there is some capacity available that may open a market for freight activity. The size of that market, however will be small as the aircraft used is the A320 which has that can be sold as freight of approximately 7 to 14m<sup>3</sup>. With forecast international services, local produce, particularly high value perishables such as seafood, can be air freighted to international markets. Any international service will be looking to sell all spare capacity in this way. The Base Case forecast identifies initial international services are likely be narrowbody aircraft such as the B737/A320 aircraft, this will constrain the available capacity for freight on these sectors. Once international sectors are introduced with widebody aircraft, the potential freight market increases considerably as outbound freight will make up a large part of the financial proposition for the aircraft used.

The Margaret River area is known for its quality wineries and wine production. It is these wineries that are so attractive for tourism to the region. However, transportation of wine is not a commodity that justifies air freight. Usually, commodities that are air freighted are those that have a very high value, very low weight or are time sensitive. Products such as wine are often sea freighted as they are heavy and not time sensitive. Any crops that are able to be transported to Perth could be air freighted from Perth as there is a large air freight volume departing Perth daily producing a more regular, diverse and more competitive market. Seafood and floriculture are potential candidates for air freight as they are extremely time sensitive. Therefore, the seafood industry in the Busselton Augusta Margaret River region, Nannup Shire, and Manjimup Shire, would likely benefit from cargo capacity to destinations such as Singapore as seafood is a time sensitive commodity.

To support the long-term freight activity, an airside freight terminal is required with suitable Custom, Quarantine and Security as well as cold storage facilities. An initial facility should be available for the introduction of international services. This facility would be supported by the development of freight forwarding operations that can develop landside. As the long-term volumes remain low, the location of the freight terminal does not need direct freighter aircraft parking however, 2 options have been developed to provide long term flexibility for freight operators to develop facilities that they can operate with adequate expansion options and efficiency.

page 55

### Freight Option 1

The first option for a freight terminal is to utilise some of the space on the Central Apron that becomes vacant once the passenger terminal activities move to a new passenger terminal. Freight forwarding activity would be located behind the terminal in the Northern Commercial Precinct as shown in Exhibit 15. In this option, all freight would be loaded onto tugs and dollies and transported to the Southern Apron to be loaded into the belly of passenger aircraft.

### Freight Option 2

With the development of the Passenger Terminal adjacent to the Southern Apron, there is a large precinct further south with ample commercial lease land available for development of a freight facility and freight forwarders. Such a location is close to the Southern Apron for easy transfer of freight to the passenger aircraft and if demand for freight were to increase to the point of a dedicated freighter aircraft operation, the area to the south of the Southern Apron can be developed into a cargo apron. This option is shown in Exhibit 16



## Aircraft and Pilot Support (FBO) Precinct

Some aircraft owners require support when preparing their aircraft departure, as well as servicing the aircraft and providing ground handling services when they arrive at an airport. The extent of services provided vary greatly from airport to airport and depends on the type of clients at each airport. To support these pilots and aircraft owners, organisations known as Fixed Base Operators (FBO) have developed providing services such as ground handling, pilot briefing support, lounges, permits, local ground transportation, etc. Based on interest that has been expressed to the City, the opportunity exists for a facility to provide support to the pilots and owners as well as hangar space. To support the potential for an FBO type business at BMRA, 2 options have been developed to safeguard for future facilities.

### FBO Option 1

The old terminal presents an opportunity for an organisation to utilise the building as their FBO facility and the Central Apron for aircraft parking. This option is presented in Exhibit 17.

### FBO Option 2

With the Aero Club relocation to the Northern Apron, Option 2 as shown in Exhibit 18, positions a future FBO opportunity on the Northern Apron with a prime view of the runway and the apron in front of the FBO for visiting aircraft. A limitation of Option 2 is the unknown strength of the apron and the limited width of Taxiway D. Please note that some business jets exceed Code 2 but have OMGWS of less than 9m and due to the high tire pressures used, can still have a high ACN.

## Airport and Airlines Support Precinct

With the growth of RPT Code C jet activity, there is a need to provide space for carriers or organisations that provide support to airline activity. Whilst the primary services are domestic, the level of support opportunities would remain low but once international services start, it is likely that there will be increased need to provide additional services to the carriers. The Airline Support Precinct would comprise organisations providing:

- GSE Maintenance
- Flight Catering
- Airline Offices
- Aircraft Cleaning
- Ground Handling Services

As shown in Exhibit 19, the Airport and Airline Support Precinct has been located close to the Passenger Terminal. The area provides opportunities for commercial organisations to construct facilities that would provide support to the carrier operations.

## Retail, Commercial and Accommodation Precinct

With the growth of aviation related activity at BMRA, there will eventually be demand for commercial activity in the form of hotels, petrol stations, office/business centre and retail opportunities. To accommodate demand for these activities developing, the Retail, Commercial and Accommodation Precinct has been identified. Demand for this area would likely start towards the end of the planning horizon or beyond. Therefore, this area should be safeguarded for development of this type as shown in Exhibit 20.

## Car Rental Precinct

The Car Rental Precinct is located to the north east of the public car park. The car rental firms needing space to store cars in preparation for future rentals as well as cleaning and in some cases, vehicle maintenance would use the Car Rental Precinct. To support the growing demand for car rental space, the Car Rental Precinct will provide space for firms to establish their rental bases, cleaning and storage facilities. The Car Rental Precinct, is located as shown in Exhibit 21, beside the Northern Commercial Precinct. The Car Park near the passenger terminal will have sections specifically for Car Rental firms to stage cars that have been booked by inbound passengers or accept car returns.

## Airport Maintenance

Airport maintenance is an important and ongoing function at the airport. Airport maintenance includes functions such as mowing, drain clearing, equipment storage, building maintenance and pavement repair. To support the airport maintenance function, there needs to be a location to store the various difference pieces of equipment, vehicles, tools and materials. The airport maintenance facilities have been located in the Northern Commercial Precinct as shown in Exhibit 22.



# SECTION 09

## MASTER PLAN

# 09

## Master Plan

A Master Plan represents an orderly plan for development for the airport site and its facilities over the defined horizon. In this case, as is typical for airports, the time period used for the planning horizon is 20 years. The full Master Plan development therefore identifies development that is expected to be required over the 20 year period to 2043. Passenger and freight traffic development may not, of course, develop exactly as forecast, regulations may change, or geopolitics may result in changes to use of the airport facilities that can impact how the airport develops. Consequently, the Airport Master Plan will need to be updated periodically to ensure that land reserved on the airport site is aligned with Aviation trends and the development of the BMAR. The Airport Master Plan has been presented in Exhibit 23.

Current passenger traffic at BMRA is predominantly served with FIFO flights as the region is becoming a popular location for the families of FIFO workers. The RPT operations include a 4 times a week service between BMRA and Melbourne. The FIFO flights represent the majority of passenger departing and arrival BMRA, however FIFO arrival passengers have little to no checked baggage, don't avail themselves of rental cars and transit through the arrivals processes quickly. The RPT arrival represents the peak demand for arrival terminal spaces with the collection of checked baggage and use of car hire services or transport options. Peak departure demand is during the peak FIFO departure period occurring early in the mornings. The air traffic forecasts prepared for the Master Plan identified two patterns to growth in air passenger traffic. The first pattern is the introduction of new services on the peak day with a forecast introduction of a service to Sydney and Brisbane as well as Singapore and Bali. Services are also forecast to grow through additional services being added on other days throughout the week.

As a result, the airport is forecast to reach a peak period demand between 2028 and 2033 and then forecast to remain constant throughout the forecast period as the additional growth occurs outside the peak period and peak day.

“A Master Plan represents an orderly plan for development for the airport site and its facilities over the defined horizon”

For future facility demands, this means that while overall annual passenger traffic is forecast to increase, the number of passengers passing through the terminal building in the peak period of the day will not increase significantly. Therefore, the passenger processing facilities need to be planned to accommodate at an acceptable level of service, defined at the passenger demand levels represented by the 2033 forecast. This peak demand is for 3 morning FIFO departures and an arrival peak of 2 RPT arrivals and 3 FIFO arrivals using A320/B737 sized aircraft. The FIFO arrivals would deplane and walk straight through the terminal so the space demands caused by the large number of FIFO arrivals is minimised. The international flights by virtue of their restrictions and markets at the destinations, would operate outside the peak periods. As such, the facility demands for international passenger processing can be assisted with the use of swing gates and operable walls, to reduce the overall building footprint.

Where development has been identified in Section 8 of this document, the Council have reviewed the options available and identified which approach to development they prefer to follow to accommodate future operations or to safeguard for potential future changes or opportunities. The primary facility issues addressed earlier in the document, and for which development

## Passenger Terminal

The need for a new passenger terminal has been evident for some time with the 2016 Master Plan identifying the requirement for a new terminal building. The requirement the new terminal has since grown in importance with the introduction of the RPT services and the success of these services is likely to result in additional services to Sydney and Brisbane. To facilitate the existing and forecast growth the new terminal is required and development should begin as soon as possible. The passenger terminal should be sized to ensure the 2028/2033 peak period arrivals and departures demand can be accommodated. The terminal should then be able to operate through the forecast period without the need for further expansion.

## Commercial Aviation Development

The BMRA Development Project completed in 2018 has provided the City with airside leases for aviation commercial and business opportunities. Once the new passenger terminal is constructed, the land adjacent to the Central Apron will be available for development. A number of options for developing the land adjacent to the Central Apron and northern aprons were explored with Option 4 (Exhibit 14) selected as the preferred option for Commercial Development. On the Central Apron and Option 3 was selected for the development of the North Apron. Using a combination of Option 3 and Option 4 allows the City to rapidly respond to interest provided the aircraft weight can be accommodated on Taxiway D2, in the near term. In the longer term, the City can lease Code C hangar lots along the Central Apron after the new Passenger terminal is constructed.

## Air Freight Facilities

There is currently no demand for air freight. However, with the increase in RPT services, carriers will want to generate additional revenue from the excess capacity they carry on flights. This is particularly true when international services start operating and producers from the region can benefit from air freight, including seafood produced in the area that would find an enthusiastic market in Singapore where tourism demand is forecast to generate passengers to develop and sustain international flights. Carriers operating international services will be looking to fill all available aircraft belly space and therefore the introduction of international passenger flights will cause an increased demand for freight operations. To support freight operations, 2 options were considered for the development of a freight terminal. Option 2 was selected as this places the freight facilities close to the Passenger Terminal and the RPT aircraft stands on the South Apron.

## Aircraft and Pilot Support Precinct

There were 2 options for the development of an FBO facility. The first was to position the FBO on the Central Apron and the second, was to position the FBO on the North Apron. Option 1 was selected as an FBO would be able to make use of the old passenger terminal for their FBO services. The site also permits the organisation to develop hangars beside the FBO Terminal for hangarage or maintenance activities for their customers. In addition, the apron strength of the Central Apron is understood to be a PCN of 58 and therefore is capable of accommodating any sized itinerant aircraft or business jet that might wish to fly into BMRA.

## Phasing

The Airport Master Plan identifies the infrastructure needs for the future development of the BMRA. The Master Plan also identifies areas that should be safeguarded for development beyond the planning horizon or ensure that businesses can take advantage of opportunities as and when they manifest, benefiting the BMRA and all airport users. As such, the Master Plan has looked at the forecast traffic and what infrastructure is needed to accommodate the forecast demand. The Master Plan has looked at potential business opportunities and what would be needed for those opportunities to develop. Some can be developed quickly with interest already expressed by businesses and interested groups. However, some will develop over time and the City needs to monitor how conditions change and when those opportunities would be suitable to encourage. To assist the City in planning for the longer-term development of the airport and to monitor traffic development and how that will influence some of the opportunities identified, the forecast planning horizon used in this Master Plan has been divided into 3 phases:



## PHASE 1

The initial phase of development for the airport is forecast to see the introduction of new RPT routes and an increase in FIFO traffic. The Phase 1 development has been shown in Exhibit 24. The existing airport infrastructure is suitable for significant growth in air traffic with the primary exception being the Passenger Terminal capacity. A new Passenger Terminal was identified as being required in the 2016 Master Plan and has been confirmed as being needed in the Phase 1 development. Current passenger demand exceeds the capacity of the existing passenger terminal system and therefore the need for the new terminal is immediate. Initial planning, design, funding and construction of the new terminal will take between 2 and 4 years.

The City is receiving interest for aviation lease lots. It is important for the City to respond quickly to these requests for sites. Commercial activity at the airport generates revenue through leases and aviation fees and creates employment with associated economic multipliers that result in the region. The apron accessed from Taxiway D1 is to become the GA and Recreational Aviation home with 27 small hangars. This area is ready for development and the Aero Club are an interested party in developing this area. The apron accessed from Taxiway D2, is ready for development and the City can respond to requests immediately.

There has also been some interest in larger hangars to give aircraft maintenance organisations the ability to develop facilities and expand their businesses. Developing the lease lots as per Option 3 would permit the City to respond quickly to interest. Any interest expressed after passenger operations move to the Southern Apron, can be accommodated on the Central Apron as per Option 4.

The car rental industry was quick to identify the opportunity presented with the introduction of the Jetstar service to Melbourne. With increased services to Melbourne during Phase 2 and the forecast introduction of a Sydney service in 2024/5, the demand is going to continue to grow. Car rental organisations are looking to grow their services at the airport and during Phase 1, space would be made available for vehicle storage area, valet and cleaning services.

## PHASE 2

From 2028 to 2033, the concentration of development is to encourage commercial aviation activity as shown in Exhibit 25. The new Passenger Terminal is to be developed during Phase 1, resulting in the Central Apron no longer utilised for RPT and FIFO aircraft. Similarly, the old passenger terminal buildings will not be required for this purpose. During Phase 2, the old passenger terminal can be repurposed into the FBO and larger hangar lots made available can be leased along the face of the Central Apron.

The level of aircraft activity and number of aircraft on the Southern apron stands in the peak period is forecast to reach 4 aircraft. As the aircraft in the peak period are Code C aircraft and are able to turnaround in 30 to 45 minutes, the actual demand during the peak period is for 5 arrivals with one departure occurring during the same hour. Therefore, any technical issue or delay to the flight schedule will quickly result in 5 to 6 aircraft on stand at once. The Southern Apron can accommodate 4 aircraft at any one time and therefore accommodate the peak demand provided there are no delays. During Phase 2, it is recommended that the City expand the Southern Apron to include 5 Code C stands and if international air service providers indicate the introduction of widebody aircraft, the City should expand the apron to include 6 stands in Phase 2 rather than Phase 3.

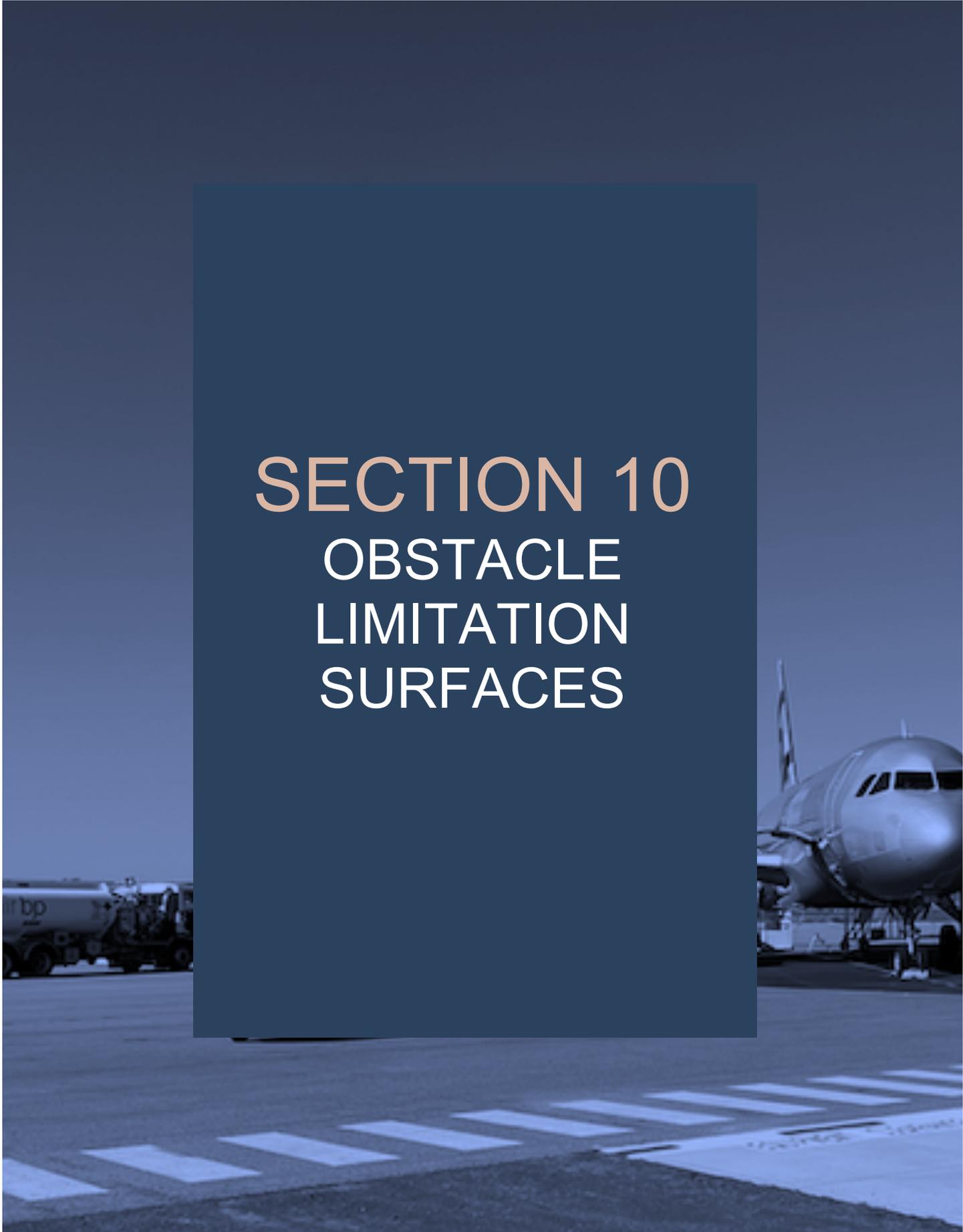
During Phase 2, the forecasts identify the commencement of international services. The City should prepare for the development of freight facilities that will help to encourage an international carrier to start the BMRA to Singapore sector and provide an export opportunity for airfreight the region.

The introduction of international services triggers a need for airline support services. During Phase 2, the development of services including aircraft cleaning and catering would be required. Initially, it is expected that this may be provided by Perth based firms providing the services but as the international schedule develops over the week, the opportunities for businesses to provide these services with locally based staff and facilities develops

## PHASE 3

The period from 2033 to 2043, Phase 3, is for a concentrated effort to encourage the development of the existing route network including RPT and FIFO operators. Increase in air traffic activity during Phase 3 is expected to occur outside the peak period with additional frequencies for both RPT and FIFO flights occurring on additional days. The infrastructure developed in Phase 1 and 2 should be able to support the air traffic growth. Exhibit 26 illustrates the airport development in Phase 3. As the level of activity increases, the BMRA can continue to develop the commercial activities in the Airport and Airline Support Precinct. Initial activity in the Retail, Commercial and Accommodation Precinct can start being developed.

The Southern Apron would have been expanded to 5 aircraft stands in Phase 2 and it is recommended the apron be expanded further to a capacity of 6 stands in Phase 3. The 6th stand at the southern end of the apron would be able to accommodate a Code E aircraft. The stand should be equipped with a refuelling hydrant system to allow rapid refuelling of the Code E aircraft. To accommodate the hydrant refuelling the refuelling storage facility would be moved to the reserved location near the Southern Apron



# SECTION 10

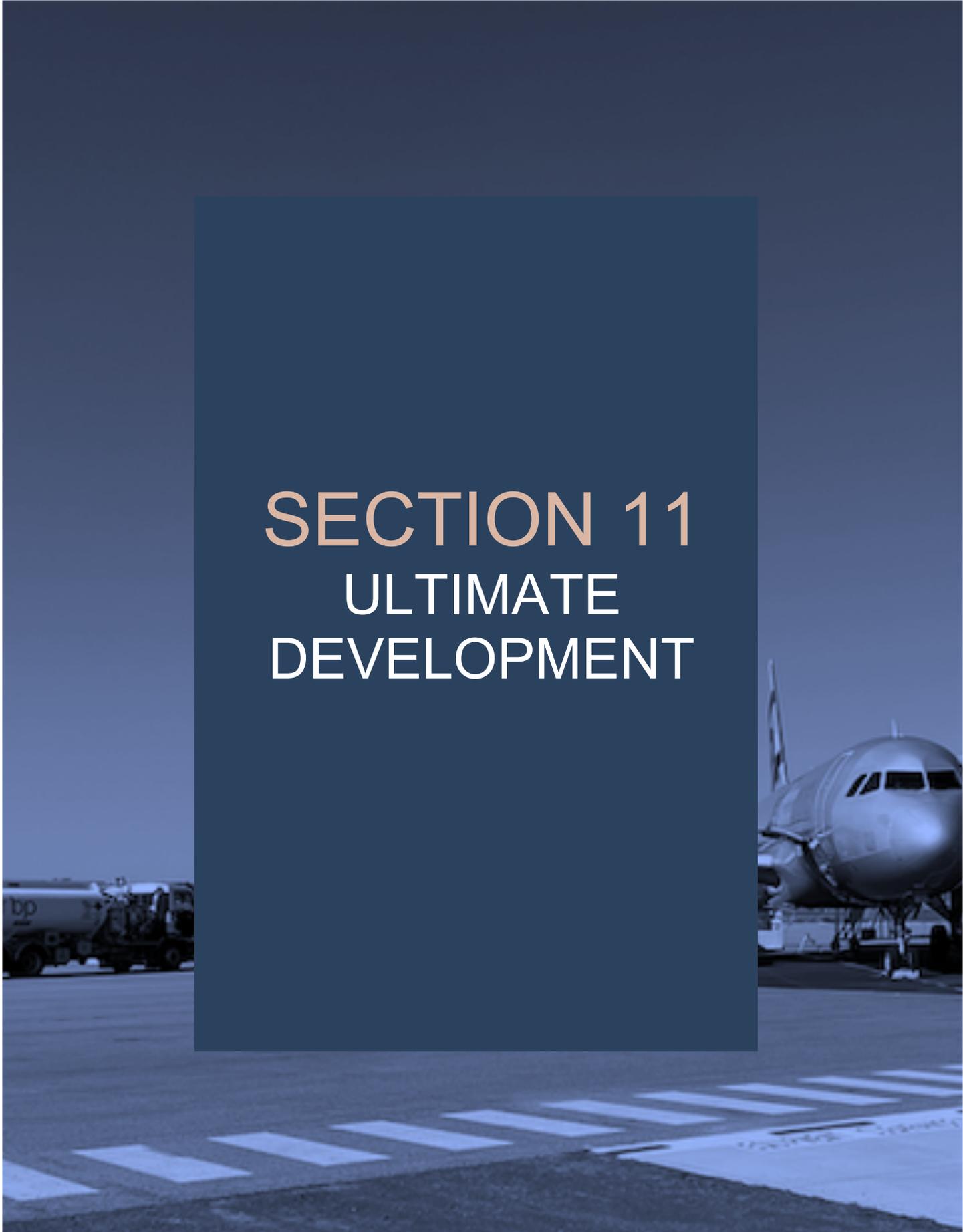
## OBSTACLE LIMITATION SURFACES

# 10

## Obstacle Limitation Surfaces

CASA sets forth a set of imaginary surfaces that extend upwards from the ground referred to as Obstacle Limitation Surfaces. These surfaces protect the airspace above an aerodrome as well as around a runway, to ensure obstacles do not present safety hazards for aircraft flight operations. The criteria for the airport OLS are established and detailed in MoS 139 Chapter 7 giving the technical criteria for heights and location of each surface. The established OLS must be monitored by the airport to ensure all obstacles are identified and controlled. Upon establishing the OLS, they become part of the National Airports Safeguarding Framework (NASF) which established guidelines for the development of infrastructure near to airports and associated flight lines that enable the continued growth of aviation. The City will also use the OLS to establish building restrictions that ensure development can take place without causing limitations to the current and future airport. Development planning applications for tall structures made to the City will be assessed against the airport OLS according to the NASF Guidelines. An illustration of the OLS has been provided in Exhibit 27.

“Development planning applications for tall structures made to the City will be assessed against the airport OLS according to the NASF Guidelines.”



# SECTION 11 ULTIMATE DEVELOPMENT

# 11

## Ultimate Development

The Master Plan is created to provide the City with a plan for the development of the airport over a 20 year period. As part of the Master Plan, air traffic is forecast over the 20 year period and the document identifies the infrastructure necessary to respond to that forecast demand. To safeguard for the longer term development and activity of the airport, the ultimate development of the airport has been shown in Exhibit 28. Over the course of the next 20 years, the City will periodically update and revise the Master Plan. It is hoped that the Ultimate Development shown in Exhibit 28 will help to guide future planners to ensure the long term airport business is safeguarded.

The Passenger Terminal Precinct has room for considerable expansion of the Passenger Terminal and associated car parking. The overall Passenger Terminal could ultimately reach 24,692m<sup>2</sup> on the existing site which would translate into approximately a peak hour 2-way passenger demand of 1763 passengers per hour.

The peak capacity for 6 aircraft stands reached in Phase 3 of the Master Plan, reflects a peak demand for 4 operational stands as well as two non active stands. The initial expansion of the Southern Apron beyond the planning horizon would be to the south. Ultimately, this expansion could accommodate up to 11 stands. When this is reached, the central hangarage would be removed and expansion to the North would be possible. Tenants in the Central Hangar zone would need to relocate to the North Apron.

“The Master Plan is created to provide the City with a plan for the development of the airport over a 20 year period.”

To accommodate significant growth in the peak hour, land has been reserved for a parallel taxiway. The point where the parallel taxiway is implemented will depend on the aircraft types in the peak hour but would likely occur when peak hour demand reaches approximately 12 movements per hour. The Parallel taxiway is planned at a distance of 172.5m (appropriate for Code E aircraft) from the runway centreline as per the current CASA standards. The reduction of the separation from the previous 2016 Master Plan separation of 190m is to permit a reconfiguration of the Central Apron, allowing a Code C taxiway on the apron, for access to any hangars that are developed.

Retaining Code F clearances would have required additional taxiways constructed to join each hangar to the parallel taxiway. The portion of Taxiway E from the Central Apron to Taxiway D would have to be downgraded to Code B as it would not be possible to provide full Code C clearance in front of any development of the Northern Apron. The reduction of the planned parallel taxiway from Code F to Code E would limit Code F aircraft operations at BMRA but would not prevent the Code F aircraft from using the BMRA. During Code F aircraft operations, the parallel taxiway would not be available to other aircraft until the Code F aircraft has either taken off or has landed and exited the runway at either Taxiway A or B.

“To accommodate significant growth in the peak hour, land has been reserved for a parallel taxiway.”



**BUSSELTON  
MARGARET RIVER  
Airport**



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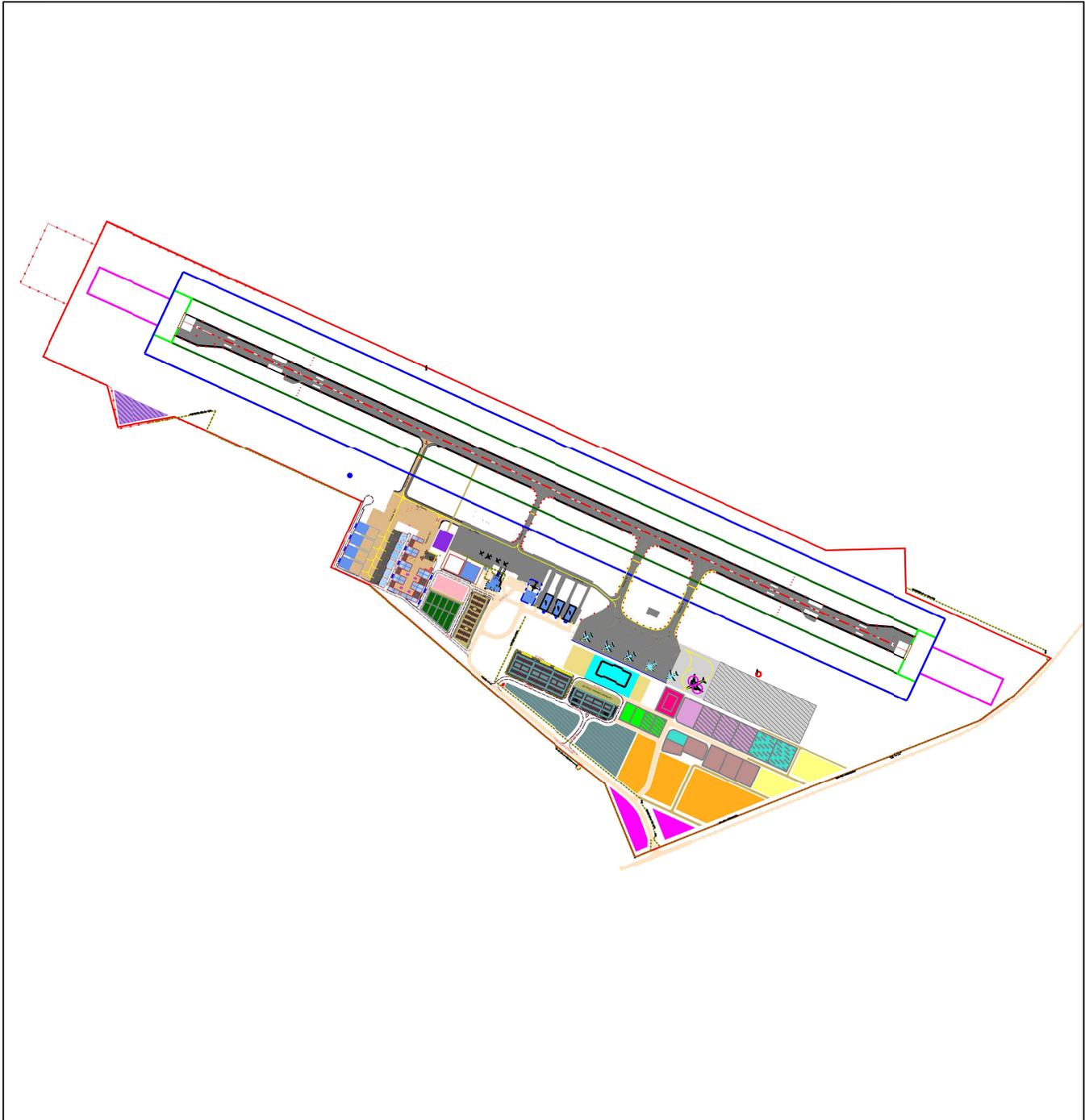
  

**Legend**

- Airport Property Boundary
- Runway Strip
- Clearway
- RESA
- Future Passenger Terminal
- Future Apron Expansion
- Future Long Term Apron Reserve
- Future Fuel Farm
- ARFFS
- Future Airside Commercial Development
- Future Airside Commercial Development Rsv
- Future Airport and Airline Support
- Future Airport and Airline Support Reserve
- GSE Maintenance / Storage / Staging
- Future Cargo / Freight
- Future Cargo / Freight Reserve
- Future Retail Commercial Reserve
- Future Accommodation Reserve
- Future Public Car Park
- Future Long Term Public Car Park Reserve
- Future Rental Car Parking
- Future Northern Commercial Development Rsv
- Future Airport Maintenance Rsv
- Future Utilities Reserve
- Airport Commercial Reserve
- ARFFS Training Area
- FBO

**Busselton Margaret River Airport  
Master Plan**

Exhibit E-1  
 2043 Airport Master Plan



**BUSSELTON MARGARET RIVER Airport**

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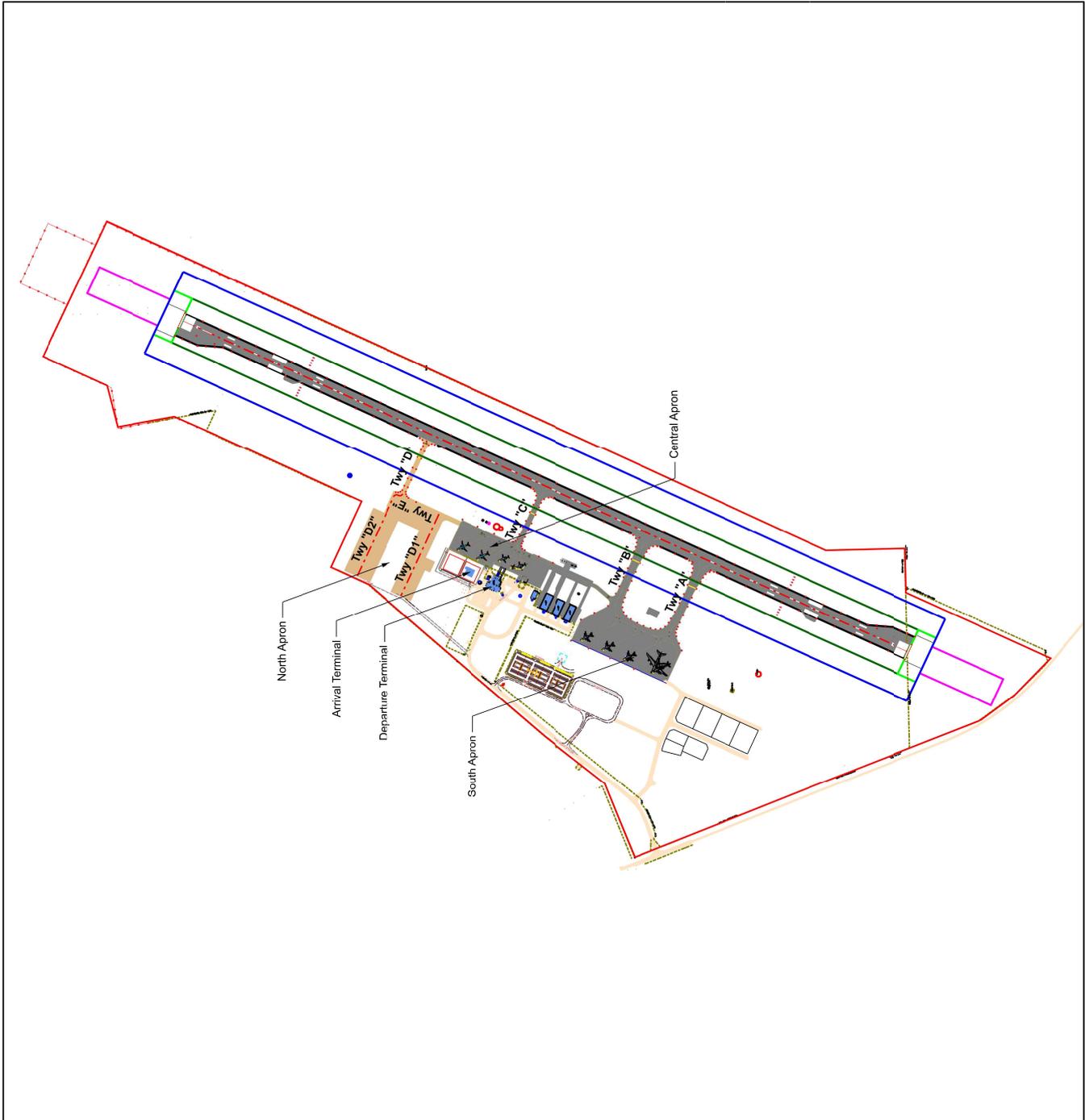
**Legend**

- Airport Property Boundary
- Runway Strip
- Cleanway
- RESA

**Busselton Margaret River Airport Master Plan**

Exhibit 4-1  
Existing

Busselton Margaret River Airport







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MARGARET RIVER  
Airport**



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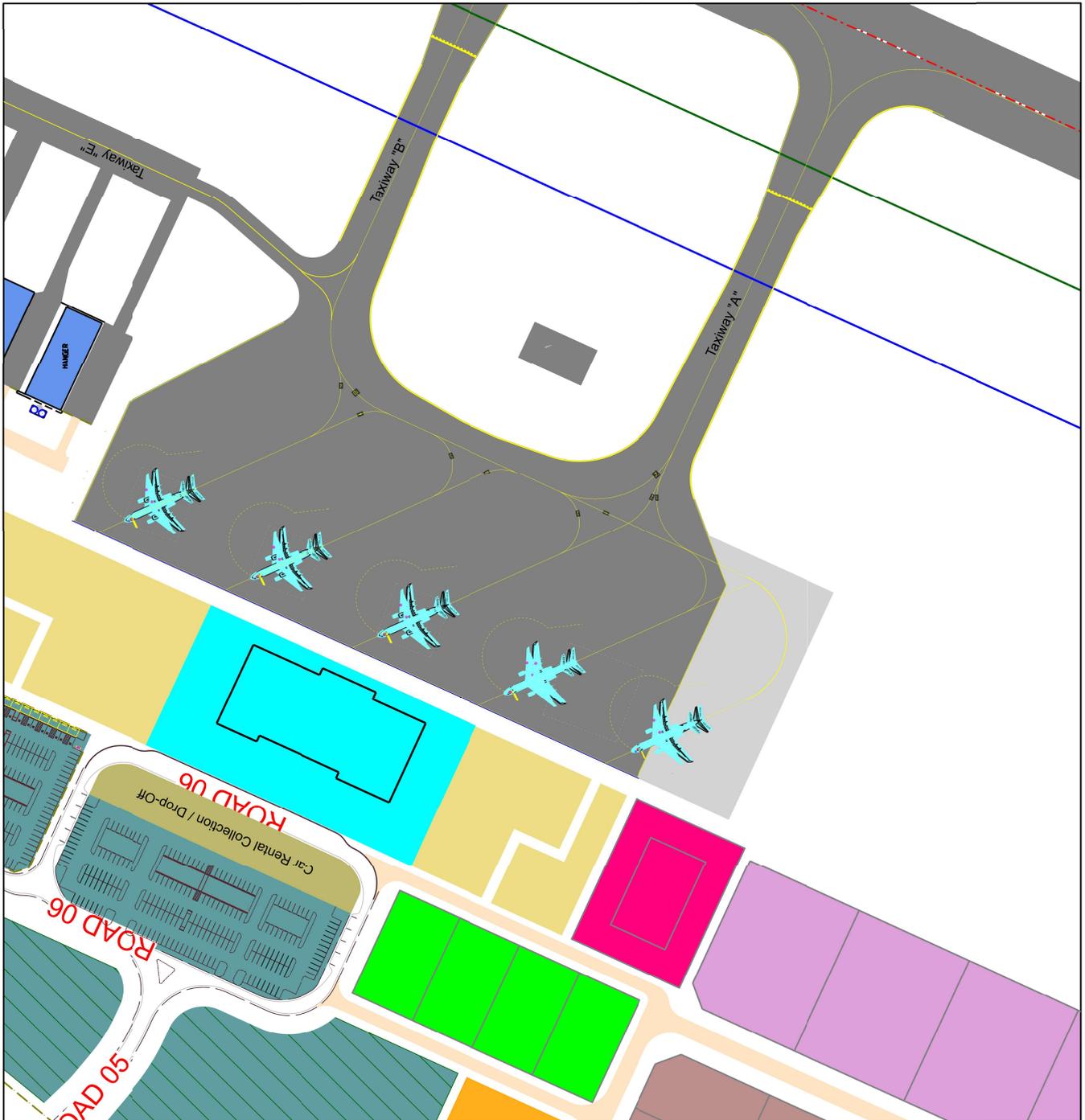
  

**Legend**

- Proposed Terminal (11,718m<sup>2</sup>)
- Future Apron Expansion
- Future Fuel Farm
- Future Airside Commercial Development
- Future Airport and Airline Support
- GSE Maintenance / Storage / Staging
- Future Retail Commercial
- Future Public Car Park
- Future Long Term Public Car Park Reserve
- Future Rental Car Parking
- Future Cargo / Freight

**Busselton Margaret River Airport  
Master Plan**

Exhibit 8-2  
South Apron Expansion  
(5 Aircraft Stands)





**BUSSELTON  
 MARGARET RIVER  
 Airport**



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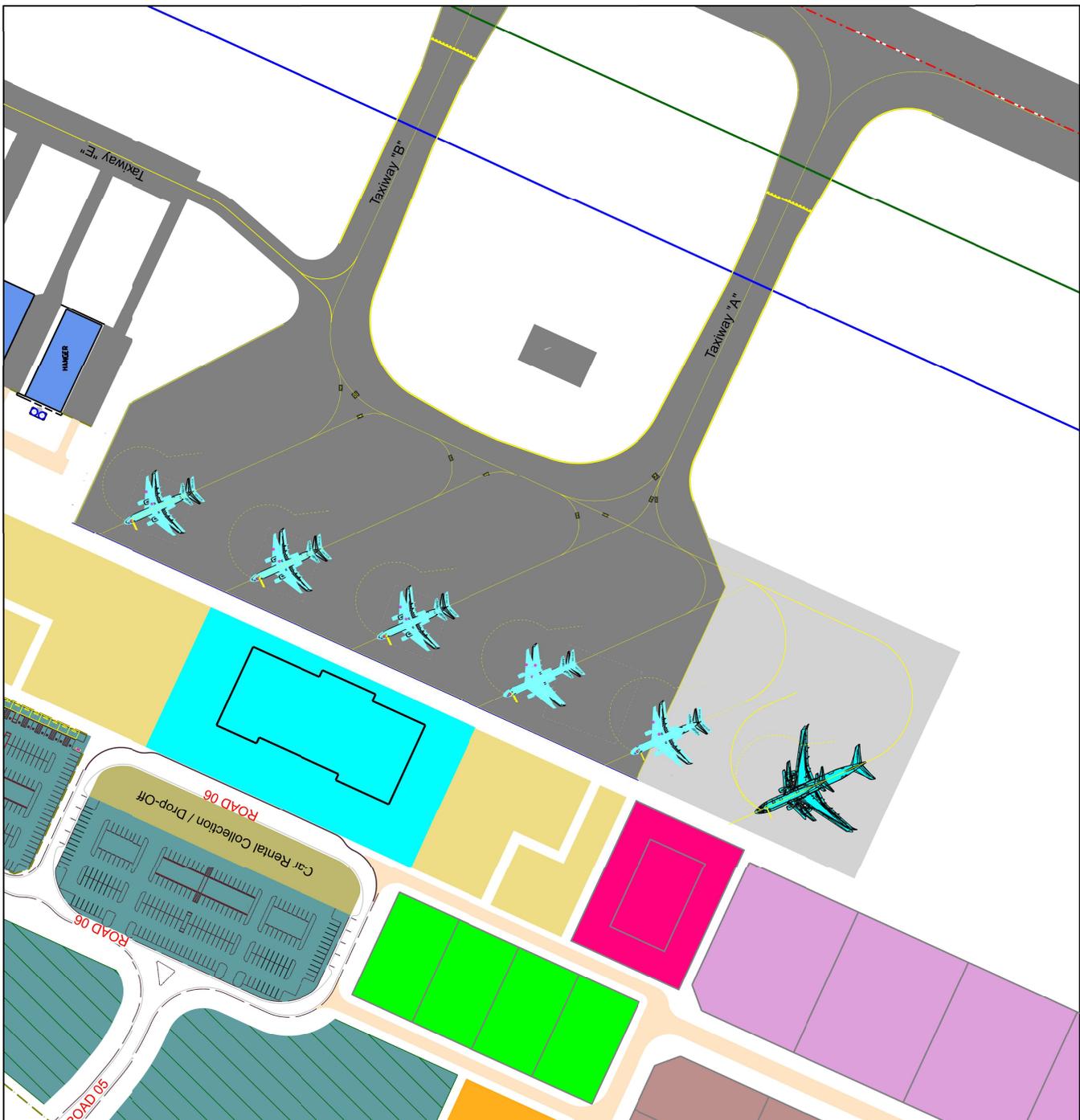
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**Legend**

- Proposed Terminal (11,718m<sup>2</sup>)
- Future Apron Expansion
- Future Fuel Farm
- Future Airside Commercial Development
- Future Airport and Airline Support
- GSE Maintenance / Storage / Staging
- Future Retail Commercial
- Future Public Car Park
- Future Long Term Public Car Park Reserve
- Future Rental Car Parking
- Future Cargo / Freight

**Busselton Margaret River Airport  
 Master Plan**

**Exhibit 8-3  
 South Apron Expansion  
 (6 Aircraft Stands)**





**BUSSETON  
MARGARET RIVER  
Airport**



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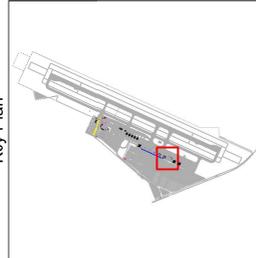


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**Legend**

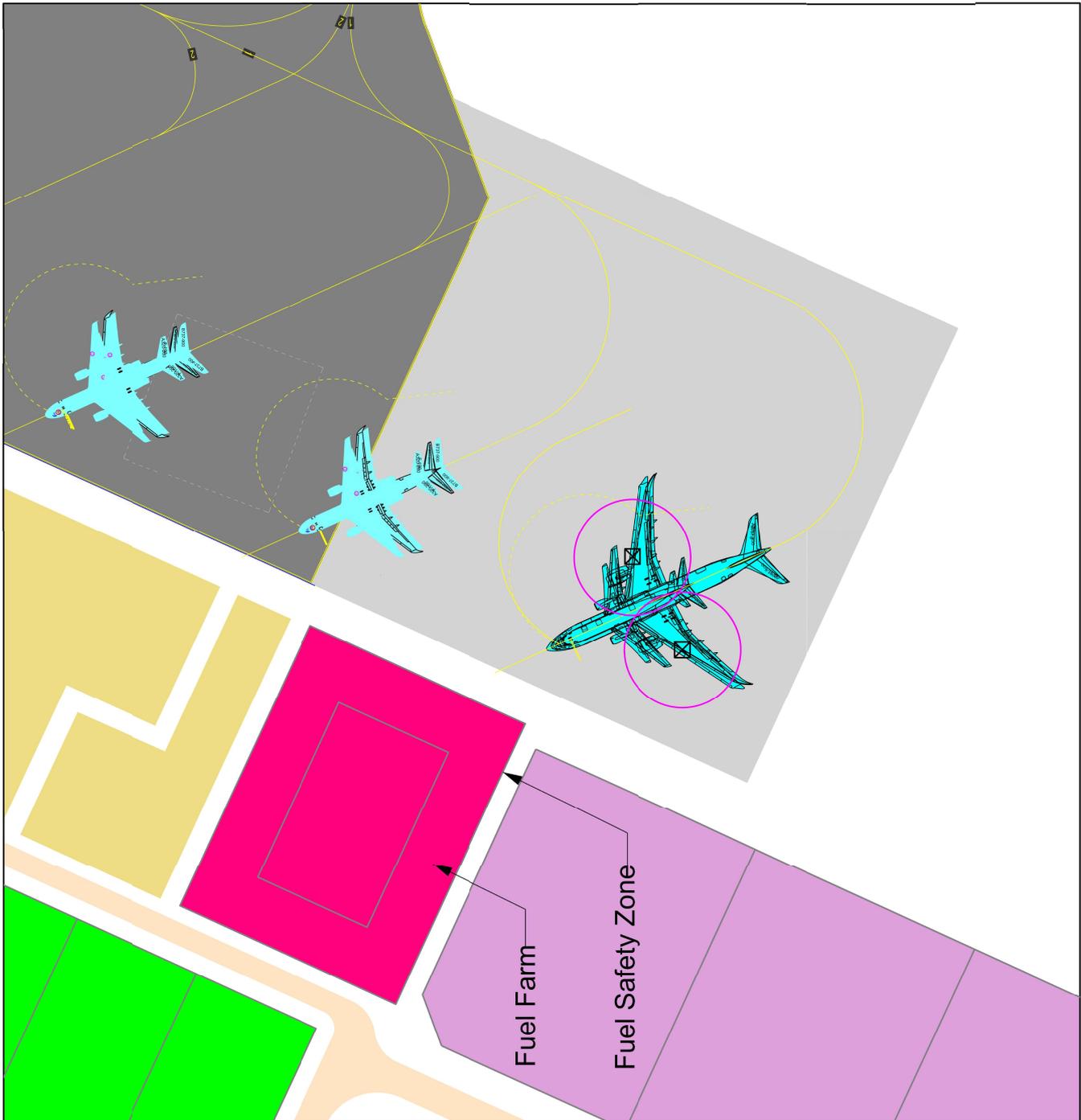
- Future Apron Expansion
- Future Fuel Farm
- Future Airport and Airline Support
- GSE Maintenance / Storage / Staging
- Future Cargo / Freight
- Fuel Hydrant Reach
- Fuel Hydrant

Key Plan



**Busseton Margaret River Airport  
Master Plan**

**Exhibit 8-4  
Fuel Hydrant and  
New Fuel Storage**





**BUSSETON  
MARGARET RIVER  
Airport**



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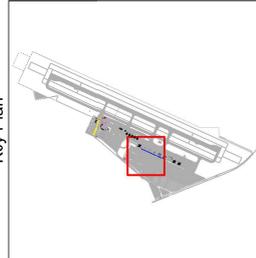


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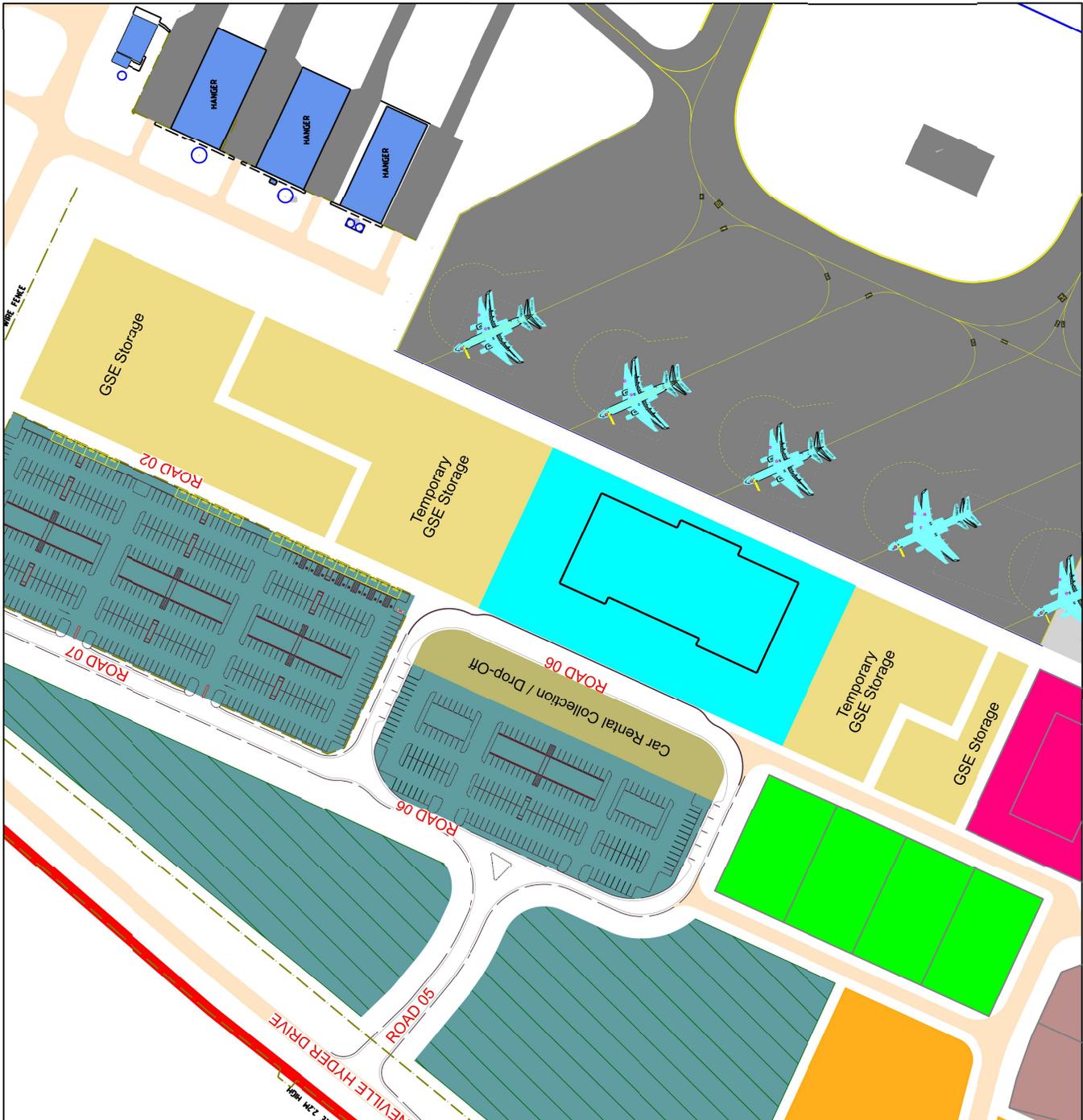
- Proposed Terminal (11,718m<sup>2</sup>)
- Future Fuel Farm
- Future Airside Commercial Development
- Future Airport and Airline Support
- GSE Maintenance / Storage / Staging
- Future Retail Commercial
- Future Public Car Park
- Future Long Term Public Car Park Reserve
- Future Rental Car Parking

Key Plan



**Busseton Margaret River Airport  
Master Plan**

**Exhibit 8-6  
GSE Storage**



**BUSSELTON MARGARET RIVER Airport**

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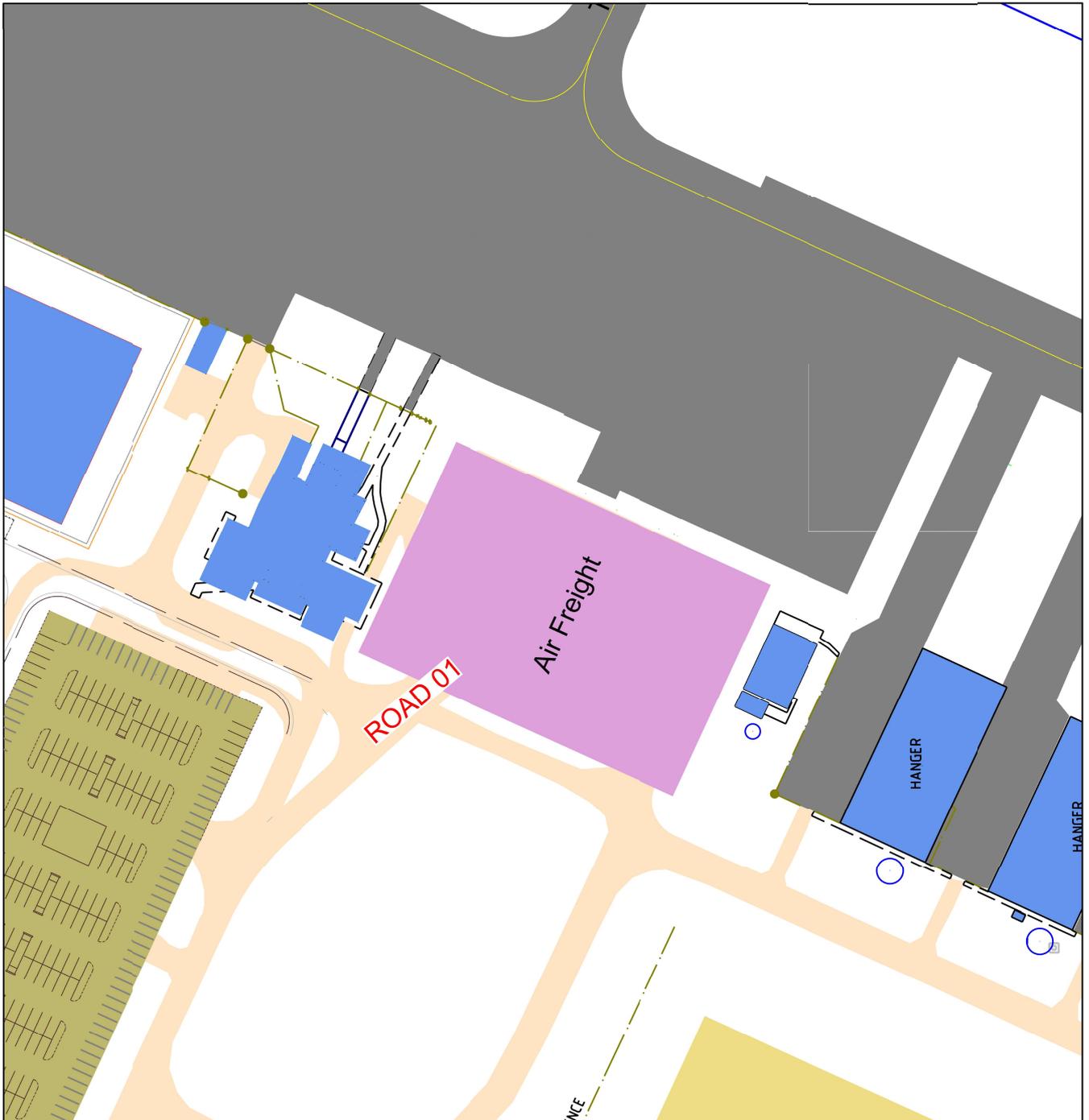
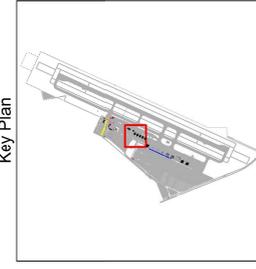
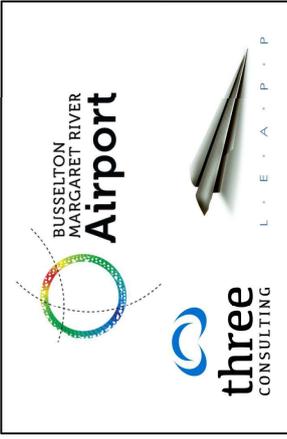
**Legend**

- Future Cargo / Freight
- Future Rental Car Parking

**Key Plan**

**Busselton Margaret River Airport Master Plan**

**Exhibit 8-11**  
**Air Freight - Option 1**





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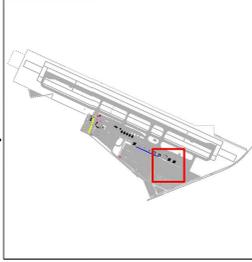
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**Legend**

- Future Apron Expansion
- Future Fuel Farm
- Future Airside Commercial Development
- Future Airport and Airline Support
- GSE Maintenance / Storage / Staging
- Future Cargo / Freight
- Future Retail Commercial
- Future Utilities
- Future Airport Commercial

Key Plan



**Busseton Margaret River Airport  
 Master Plan**

Exhibit 8-12  
 Air Freight - Option 2





**three**  
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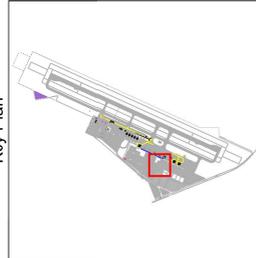
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**Legend**

- Future Passenger Terminal
- Future Apron Expansion
- Future Fuel Farm
- Future Airside Commercial Development
- Future Airport and Airline Support
- GSE Maintenance / Storage / Staging
- Future Cargo / Freight
- Future Retail Commercial
- Future Long Term Public Car Park Reserve
- Future Rental Car Parking
- Airport Commercial



Key Plan

**Busseton Margaret River Airport  
 Master Plan**

Exhibit 8-15  
 Airport and Airline Support Precinct





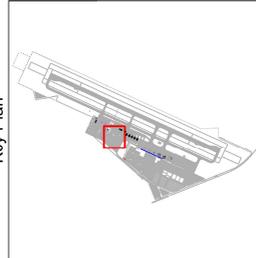


BUSSELTON MARGARET RIVER  
**Airport**

L · E · A · P · P

**Legend**

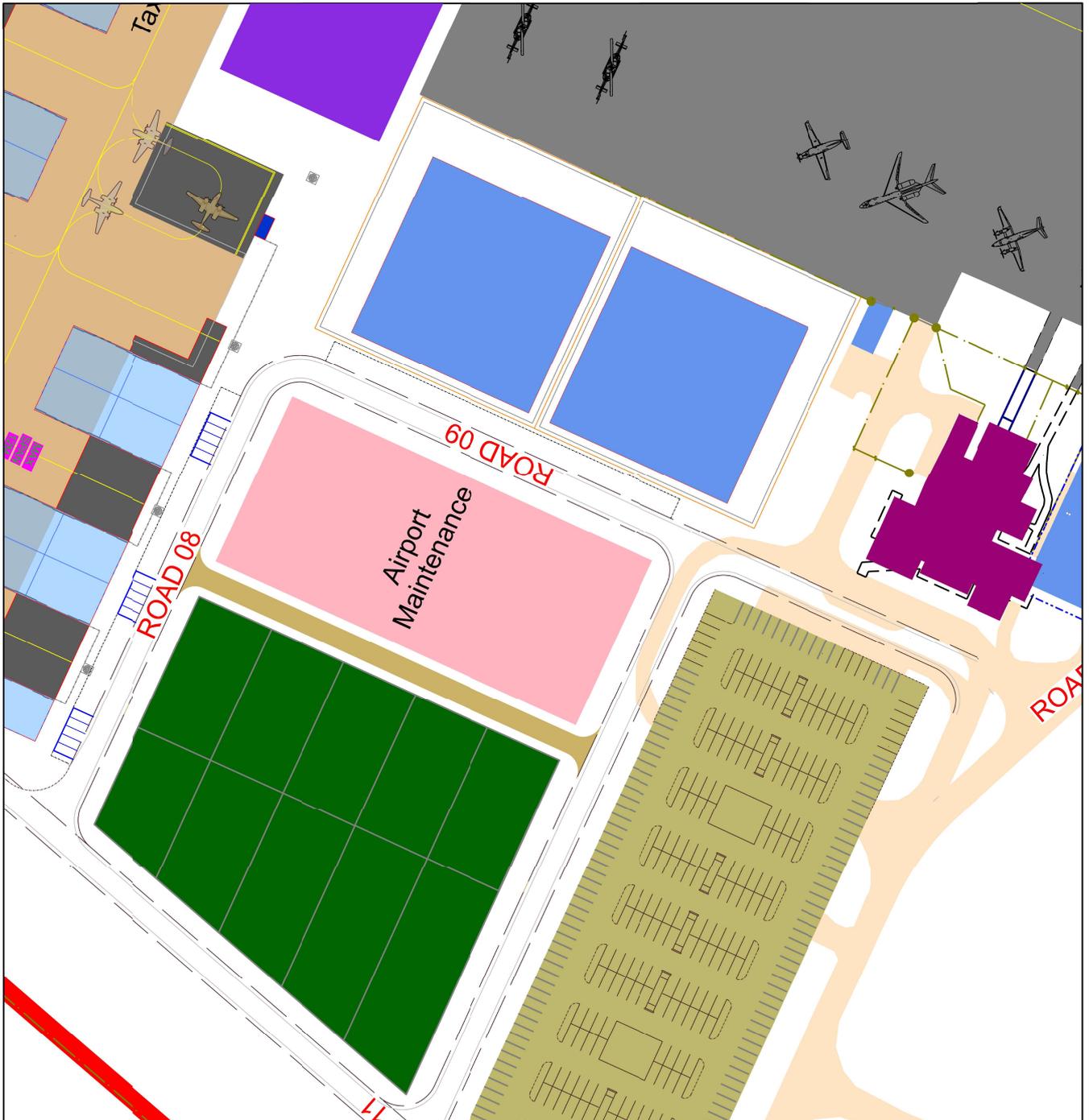
- Airport Property Boundary
- ARFFS
- Future Rental Car Parking
- Future Northern Commercial Development
- Future Airport Maintenance



Key Plan

**Busseton Margaret River Airport Master Plan**

**Exhibit 8-18**  
**Airport Maintenance**





**BUSSELTON  
 MARGARET RIVER  
 Airport**



**three  
 CONSULTING**



L E E A P P

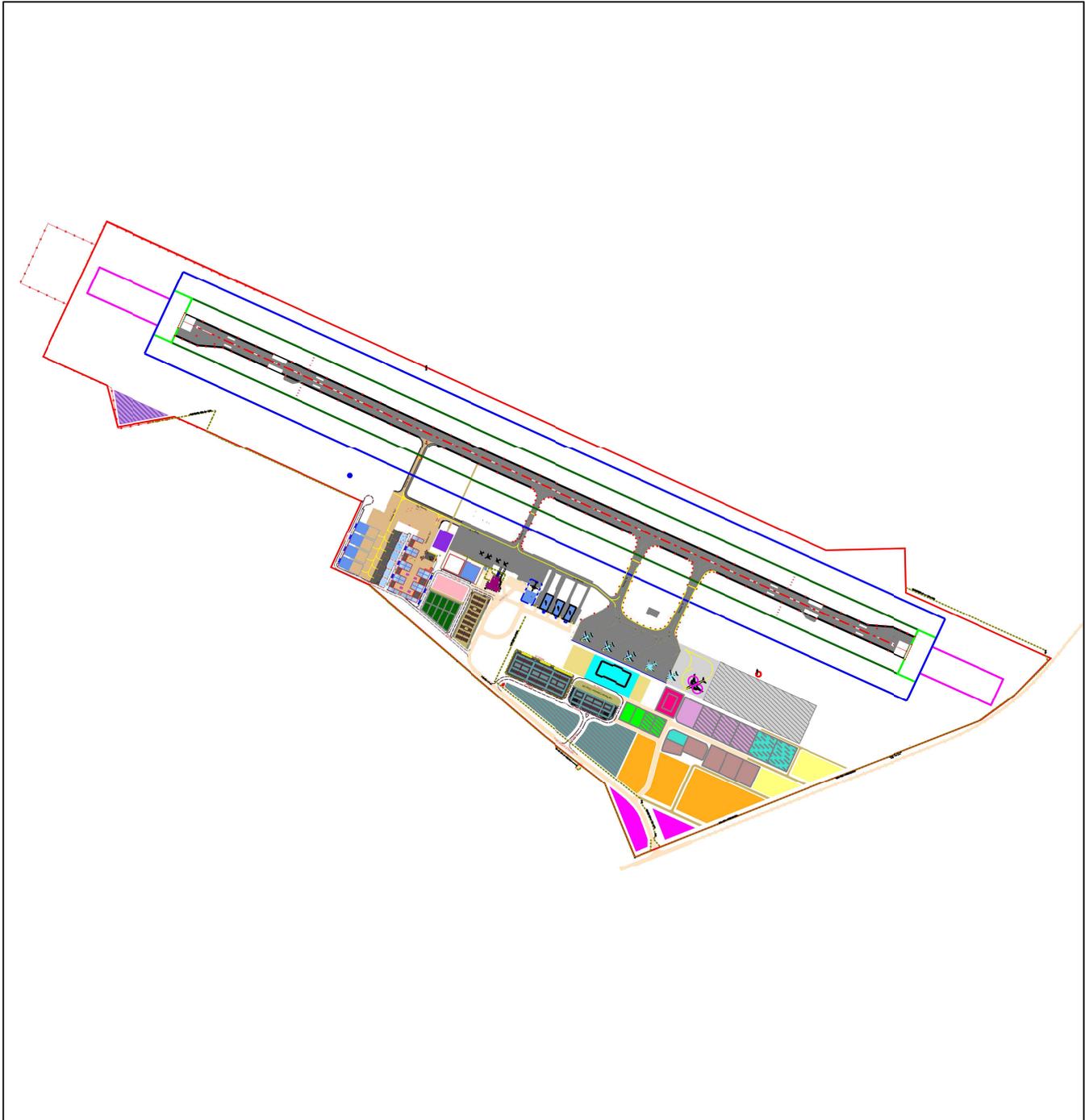
  

**Legend**

- Airport Property Boundary
- Runway Strip
- Clearway
- RESA
- Future Passenger Terminal
- Future Apron Expansion
- Future Long Term Apron Reserve
- Future Fuel Farm
- ARFFS
- Future Airside Commercial Development
- Future Airside Commercial Development Rsv
- Future Airport and Airline Support
- Future Airport and Airline Support Reserve
- GSE Maintenance / Storage / Staging
- Future Cargo / Freight
- Future Cargo / Freight Reserve
- Future Retail Commercial Reserve
- Future Accommodation Reserve
- Future Public Car Park
- Future Long Term Public Car Park Reserve
- Future Rental Car Parking
- Future Northern Commercial Development Rsv
- Future Airport Maintenance Rsv
- Future Utilities Reserve
- Airport Commercial Reserve
- ARFFS Training Area
- FBO

**Busselton Margaret River Airport  
 Master Plan**

Exhibit 9-1  
 Airport Master Plan



**BUSSELTON MARGARET RIVER Airport**



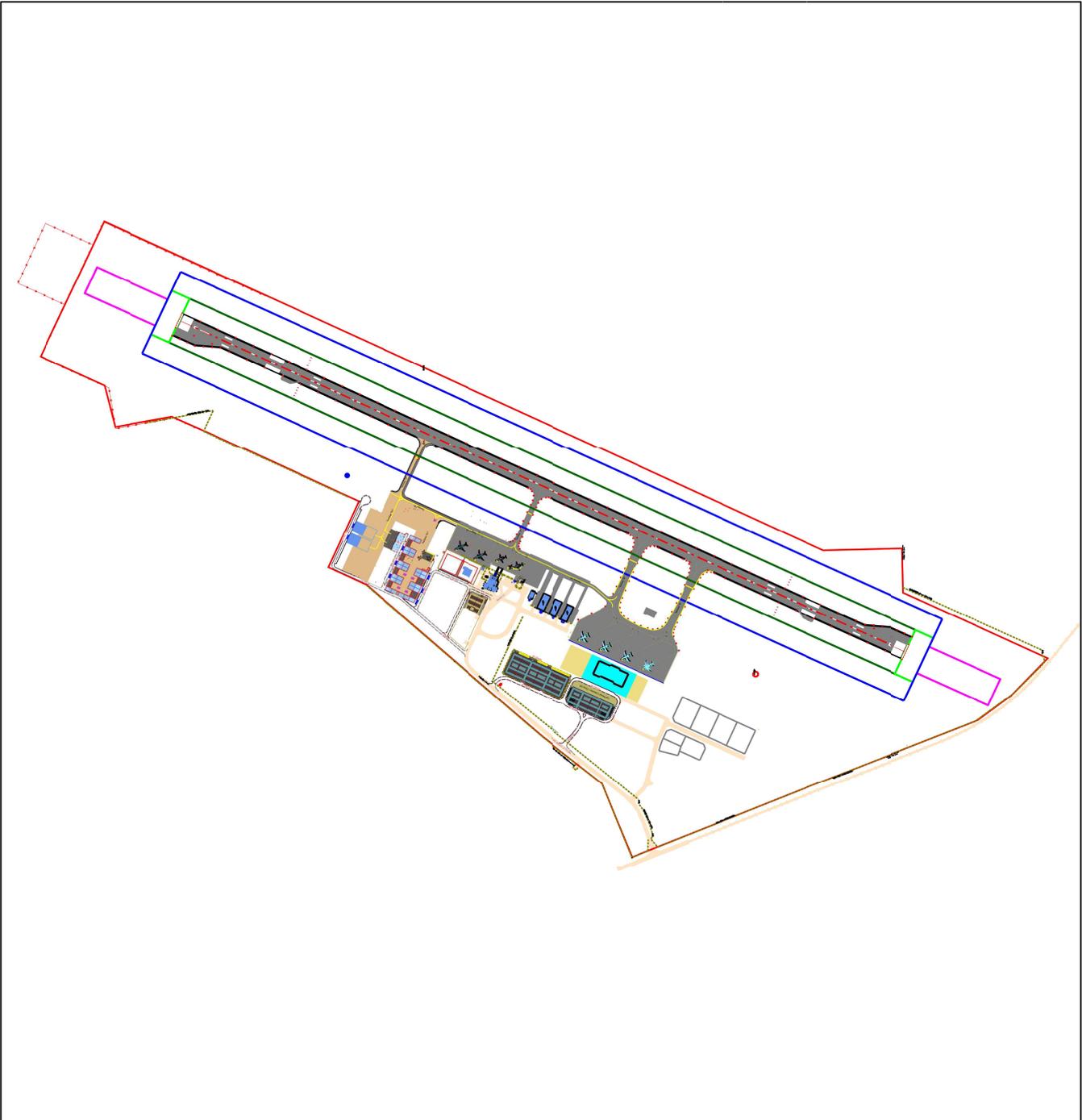
three  
CONSULTING

Legend

- Airport Property Boundary
- Runway Strip
- Cleanway
- RESA
- Future Passenger Terminal
- GSE Maintenance / Storage / Staging
- Future Public Car Park

**Busselton Margaret River Airport Master Plan**

Exhibit 9-2  
Airport Master Plan - Phase 1  
(2023 to 2028)

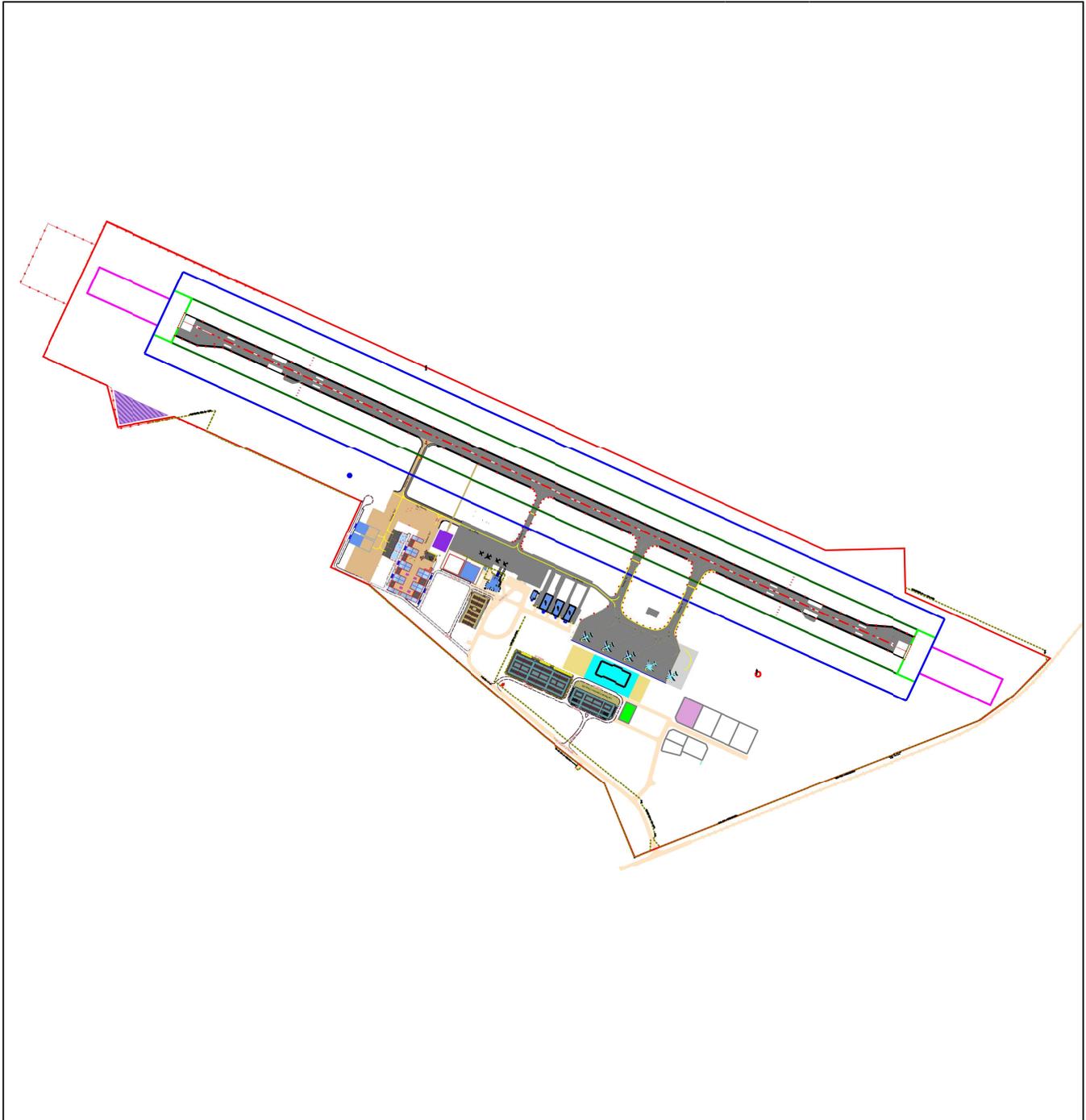




- Legend
- Airport Property Boundary
  - Runway Strip
  - Clearway
  - RESA
  - Future Passenger Terminal
  - Future Apron Expansion
  - ARFFS
  - Future Airport and Airline Support
  - GSE Maintenance / Storage / Staging
  - Future Cargo / Freight
  - Future Public Car Park
  - Future Rental Car Parking
  - ARFFS Training Area
  - FBO

**Busselton Margaret River Airport  
 Master Plan**

Exhibit 9-3  
 Airport Master Plan - Phase 2  
 (2028 to 2033)



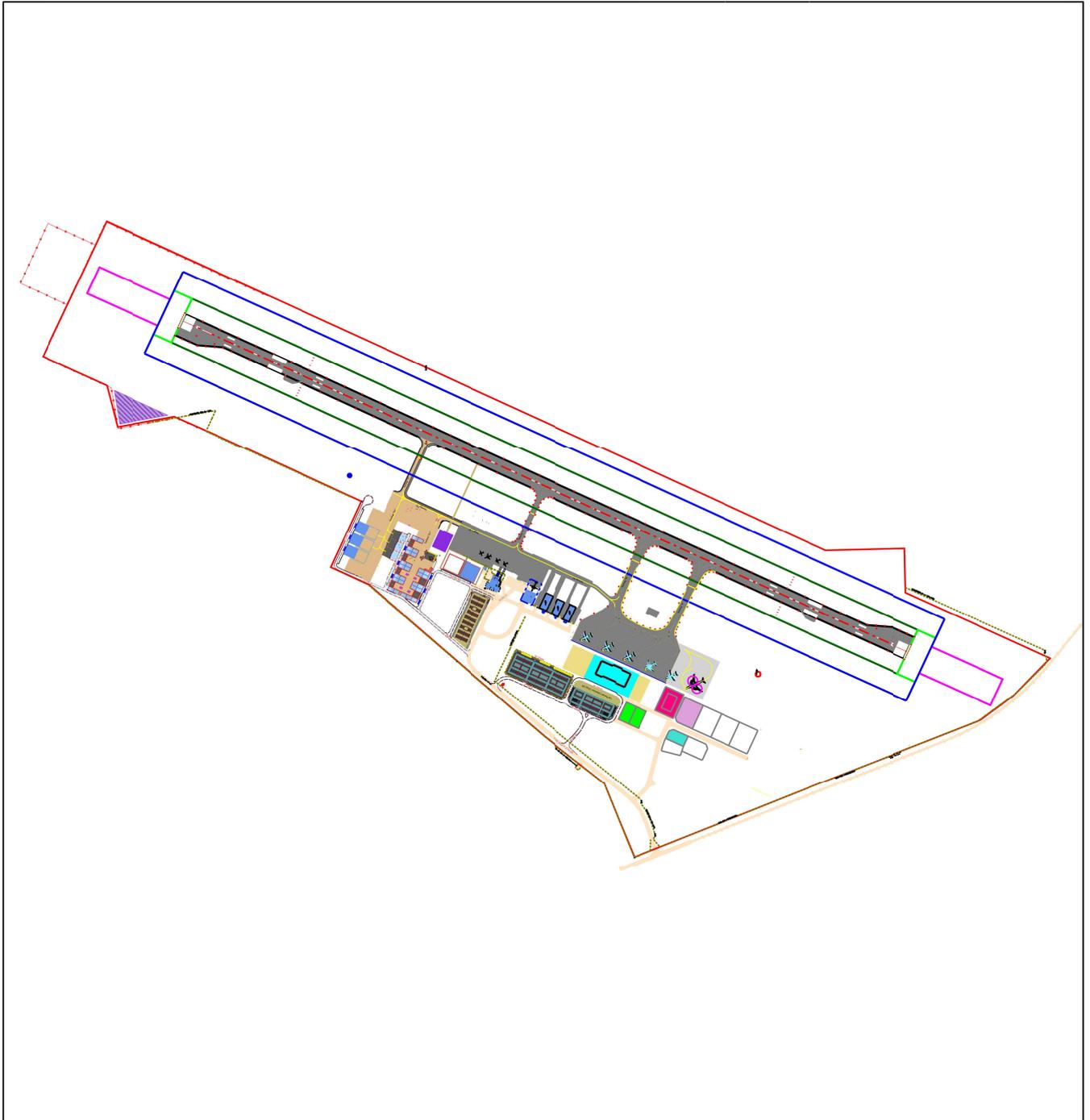


Legend

- Airport Property Boundary
- Runway Strip
- Clearway
- RESA
- Future Passenger Terminal
- Future Apron Expansion
- Future Fuel Farm
- ARFFS
- Future Airside Commercial Development
- Future Airport and Airline Support
- GSE Maintenance / Storage / Staging
- Future Cargo / Freight
- Future Public Car Park
- Future Rental Car Parking
- ARFFS Training Area
- FBO

**Busselton Margaret River Airport  
 Master Plan**

Exhibit 9-4  
 Airport Master Plan - Phase 3  
 (2033 to 2043)





**BUSSELTON  
 MARGARET RIVER  
 Airport**



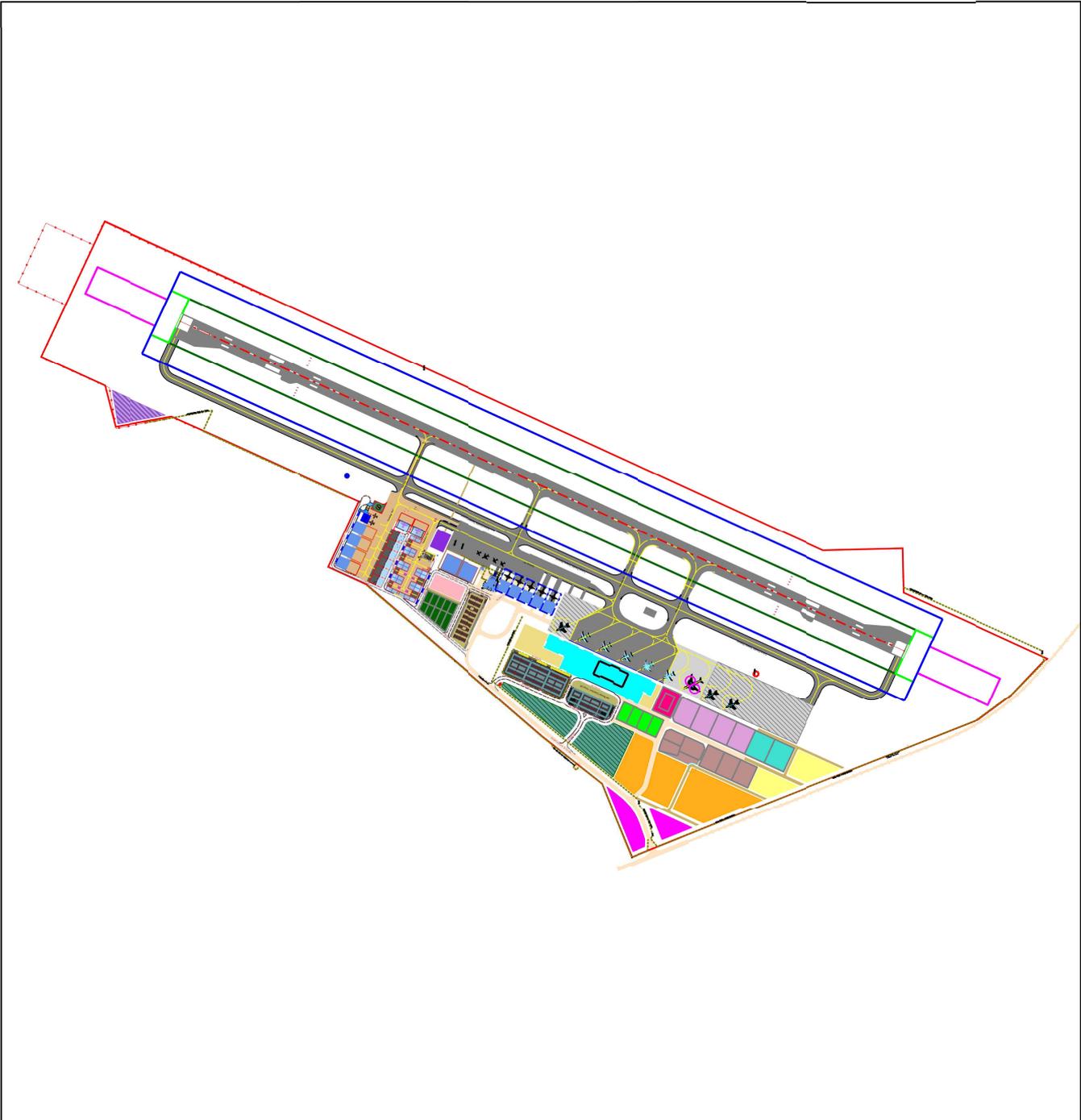
**three  
 CONSULTING**  
 L · E · A · P · P

**Legend**

- Airport Property Boundary
- Runway Strip
- Clearway
- RESA
- Future Passenger Terminal
- Future Apron Expansion
- Future Long Term Apron Reserve
- Future Fuel Farm
- ARFFS
- RFDS
- Future Airside Commercial Development
- Future Airport and Airline Support
- GSE Maintenance / Storage / Staging
- Future Cargo / Freight
- Future Retail Commercial
- Future Accommodation
- Future Public Car Park
- Future Long Term Public Car Park Reserve
- Future Rental Car Parking
- Future Northern Commercial Development
- Future Airport Maintenance
- Future Utilities
- Airport Commercial
- ARFFS Training Area
- FBO

**Busselton Margaret River Airport  
 Master Plan**

Exhibit 10-1  
 Ultimate Airport Development



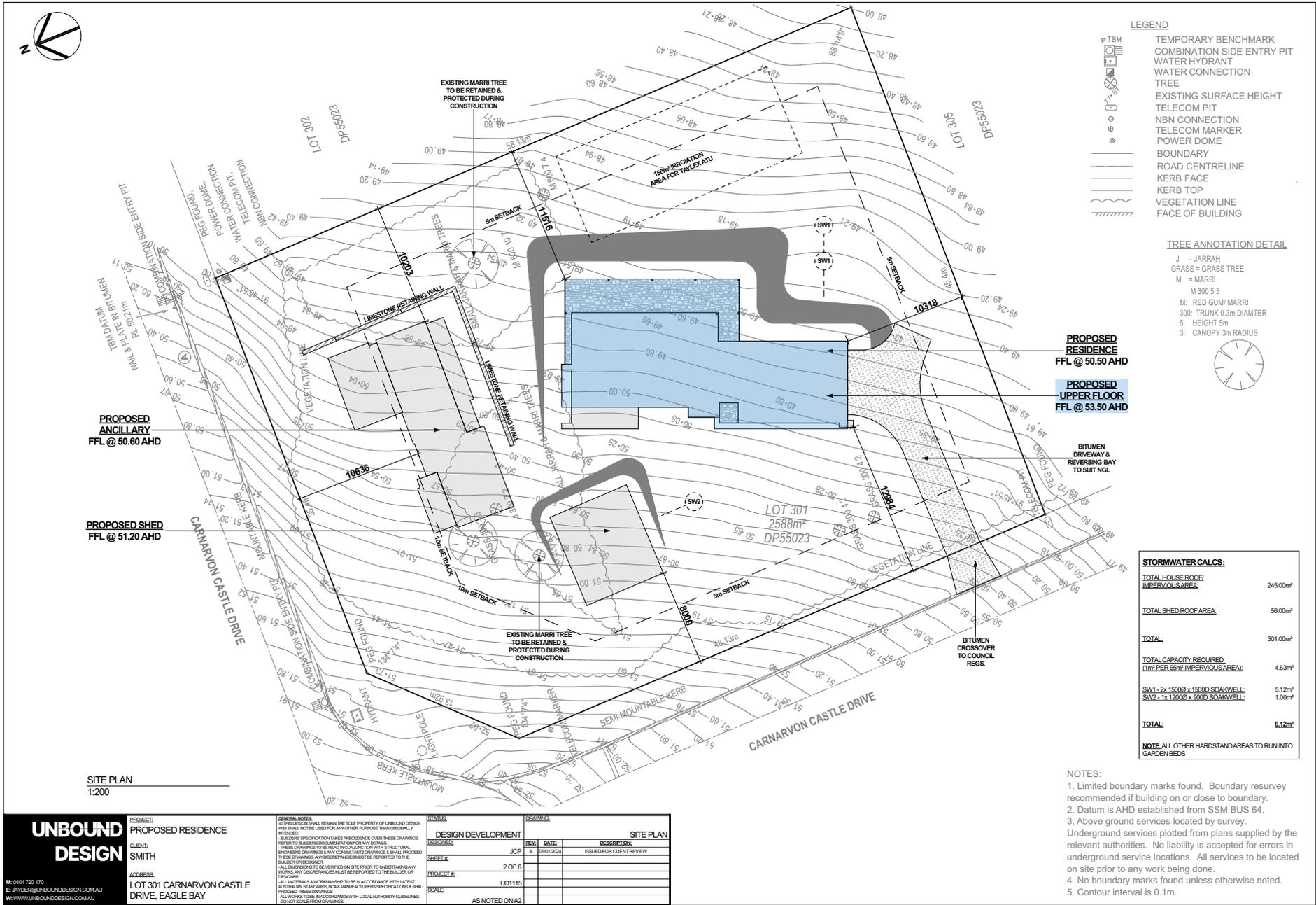


Disclaimer: Every effort has been made to make the information displayed here as accurate as possible. This process is ongoing and the information is therefore ever changing and cannot be disseminated as accurate. Care must be taken not to use this information as correct or legally binding. To verify information contact the City of Busselton office.

Attachment A - Location Plan  
 DA22/0817 Bed and  
 Breakfast (Change of Use and  
 New Development)

05/09/2023  
 1:2219 @ A4L





SITE PLAN  
1:200

**UNBOUND DESIGN**

PROJECT: PROPOSED RESIDENCE  
CLIENT: SMITH  
ADDRESS: LOT 301 CARNARVON CASTLE DRIVE, EAGLE BAY

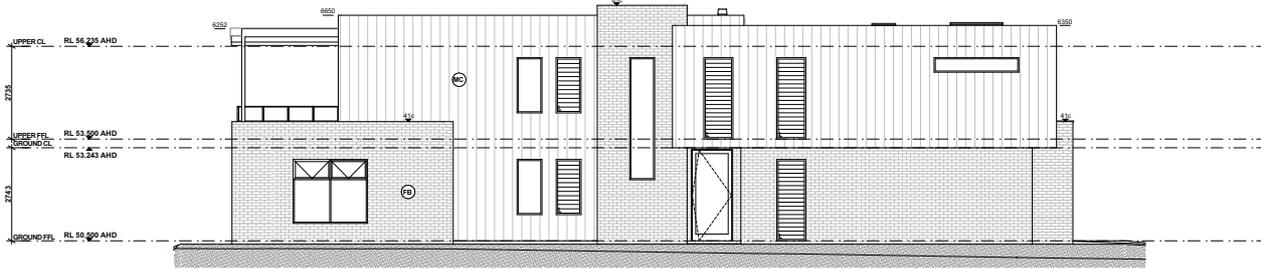
REVISIONS	NO.	DATE	DESCRIPTION
DESIGN DEVELOPMENT			
NO. 1	A	06/01/2024	ISSUED FOR CLIENT REVIEW
NO. 2			
NO. 3			
NO. 4			
NO. 5			
NO. 6			
NO. 7			
NO. 8			
NO. 9			
NO. 10			
NO. 11			
NO. 12			
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NO. 92			
NO. 93			
NO. 94			
NO. 95			
NO. 96			
NO. 97			
NO. 98			
NO. 99			
NO. 100			

M: 0434 720 170  
E: JAY@UNBOUNDDESIGN.COM.AU  
W: WWW.UNBOUNDDESIGN.COM.AU



**WALL FINISHES LEGEND**

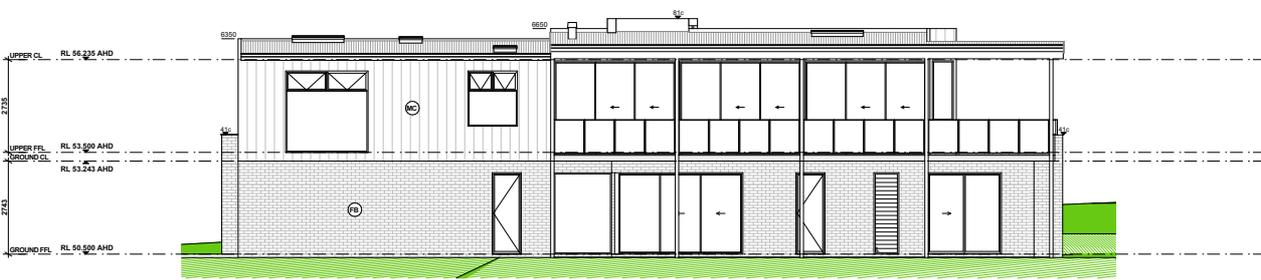
FB	1c FACE BRICK AS SELECTED
MC	LYSAGHT SNAPSEAM 260mm AS PER MANUFACTURERS DETAILS



**E1 WEST ELEVATION**  
1:100



**E2 NORTH ELEVATION**  
1:100



**E3 EAST ELEVATION**  
1:100



**E4 SOUTH ELEVATION**  
1:100

**UNBOUND  
DESIGN**

PROJECT: PROPOSED RESIDENCE  
CLIENT: SMITH  
ADDRESS: LOT 301 CARNARVON CASTLE DRIVE, EAGLE BAY

**GENERAL NOTES**  
1. THIS DESIGN SHALL REMAIN THE SOLE PROPERTY OF UNBOUND DESIGN AND SHALL NOT BE USED FOR ANY OTHER PURPOSE THAN ORIGINALLY INTENDED.  
2. BUILDING SPECIFICATION TAKES PRECEDENCE OVER THESE DRAWINGS.  
3. REVISIONS TO BE MADE IN CONJUNCTION WITH STRUCTURAL ENGINEER DRAWINGS AND ANY CONSULTANTS/ENGINEERS SHALL PROCEED THESE DRAWINGS ANY DISCREPANCIES MUST BE REPORTED TO THE DESIGNER.  
4. ALL DIMENSIONS TO BE CHECKED ON SITE PRIOR TO UNDERSTANDING ANY WORKS. ANY DISCREPANCIES MUST BE REPORTED TO THE DESIGNER OR ARCHITECT.  
5. ALL MATERIALS & WORKMANSHIP TO BE IN ACCORDANCE WITH LATEST AUSTRALIAN STANDARDS FOR MANUFACTURERS SPECIFICATIONS & SHALL PROCEED THESE DRAWINGS.  
6. ALL WORKS TO BE IN ACCORDANCE WITH LOCAL AUTHORITY GUIDELINES.  
7. DO NOT SCALE FROM DRAWINGS.

**STATUS**  
DESIGN DEVELOPMENT  
SCALE: AS NOTED ON A2

REV	DATE	DESCRIPTION
A	06/01/2024	ISSUED FOR CLIENT REVIEW
B		
C		
D		
E		
F		
G		
H		
I		
J		
K		
L		
M		
N		
O		
P		
Q		
R		
S		
T		
U		
V		
W		
X		
Y		
Z		

M: 0454 720 170  
E: JAVON@UNBOUNDDESIGN.COM.AU  
W: WWW.UNBOUNDDESIGN.COM.AU

- NOTE:**
- TOP OF FOOTINGS FOR ALL EXTERNAL PIERS/POSTS ARE TO BE 2c BELOW FINISHED PAVING LEVEL.
  - LOCATION OF DOWN PIPES IS INDICATIVE ONLY & MAY BE CHANGED AT BUILDERS DISCRETION AND MUST COMPLY WITH PART 3.5.2 VOLUME 2 OF THE BCA.
  - PROVIDE HOOP IRON HOLDING DOWN STRAPS TO AS 4055.
  - R4.1 INSULATION TO FLAT & RAKING CEILING AREAS OF HOUSE.
  - R2 INSULATION TO EXTERNAL REVERSED BRICK VENEER WALLS.
  - METAL CORNER BEADS TO ALL EXTERNAL PLASTERED CORNERS ON INTERNAL WALLS.
  - FLYSCREENS & KEYLOCKS TO ALL ALUMINIUM WINDOWS AND SLIDING DOORS.
  - 31c + PLATE CEILINGS THROUGHOUT UNLESS OTHERWISE NOTED.
  - ANTICON ROOF BLANKET TO ENTIRE ROOF U.N.O.

Roof Area Calculation - All Floors			
Floor	Pitch	Area (flat)	Area (pitched)
<b>GROUND FLOOR</b>			
	5° 0' 0"	113.39	113.82
		<b>113.39 m<sup>2</sup></b>	<b>113.82 m<sup>2</sup></b>

Floor Areas - All Stories			
Floor	Location	Area	Perimeter
<b>GROUND FLOOR</b>			
	RESIDENCE	82.70	46.32
	PERGOLA	22.80	19.40
	VERANDAH 1	14.81	22.42
	VERANDAH 2	10.03	14.40
		<b>130.34 m<sup>2</sup></b>	

**Special**

ADVANTAGE SPECIFICATION	MODEL N°
© COPYRIGHT	6483

**COUNTRY BUILDERS SOUTHWEST** PTY LTD

Unit 7 Homemaker Centre  
Cnr Blair & Strickland Streets, Bunbury, W.A. 6230.  
Telephone: (08) 9792 0100. Facsimile: (08) 9792 0101.  
P.O. Box 451, Bunbury, W.A. 6231.

13 Bussell Highway,  
West Busseton WA 6280.  
Telephone: (08) 9754 9000. Facsimile: (08) 9754 9001.  
P.O. Box 1404, Busseton, W.A. 6280.

Rev. Builder N°: 11422. A.B.N. 94 105 492 140. A Division of JWH GROUP Pty Ltd.

REV	VO #	DRN	DATE	CHK
1	Clarification Items	BSA	11/03/2022	BSA
2	Client Amends	BSA	17/03/2022	BSA
3	Engineering	BSA	06/05/2022	BSA
4	PCV 5	JKS	30/05/2022	

Sub-contractors to verify all dimensions on site.

THIS IS ONE OF THE DRAWINGS REFERRED TO IN THE CONTRACT.

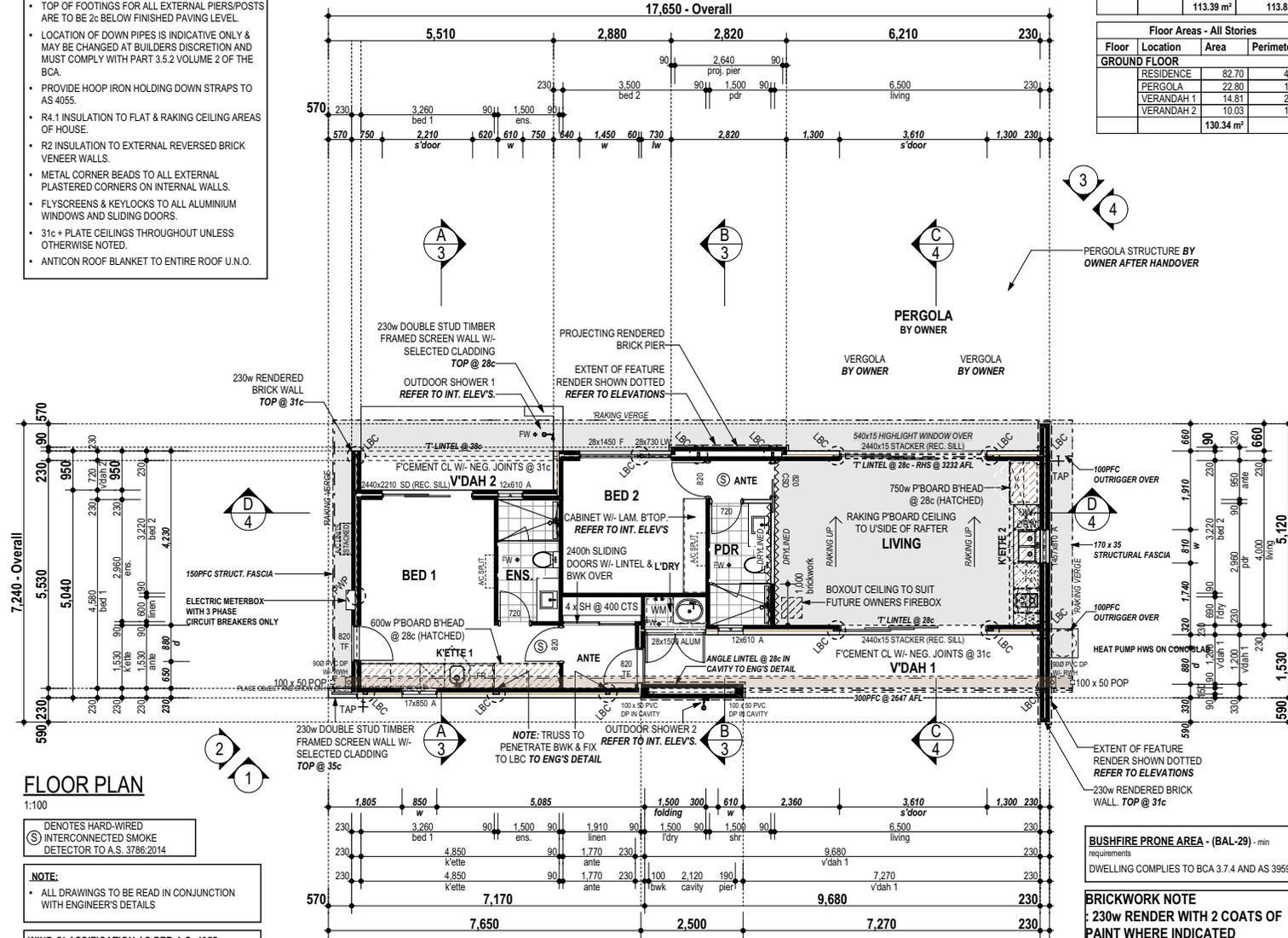
DATED:...../...../.....

OWNER	WITNESS
OWNER	WITNESS
BUILDER	WITNESS

CLIENT: **Teena L Smith**

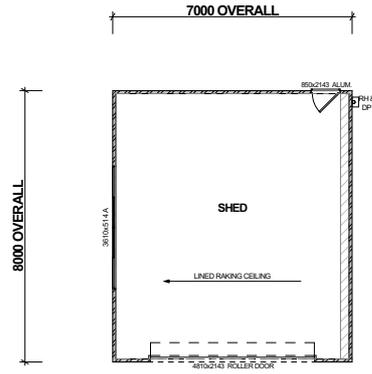
ADDRESS:  
**Lot 301  
Carnarvon Castle Drive  
EAGLE BAY**

SHEET N°	1 OF 8	
JOB N°	302553	
REVISION	DATE	
4	30/05/2022	

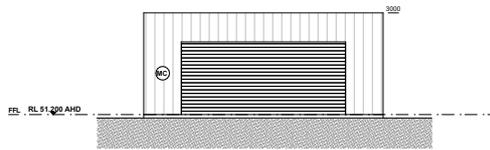




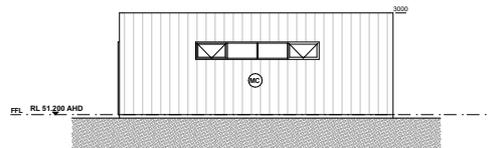
WALL FINISHES LEGEND	
MC	LYSAGHT SNAPSEAM 200mm AS PER MANUFACTURERS DETAILS



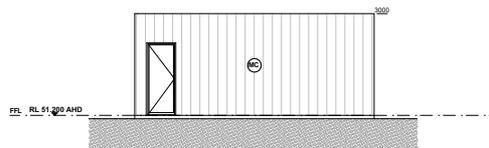
SHED FLOOR PLAN  
1:100



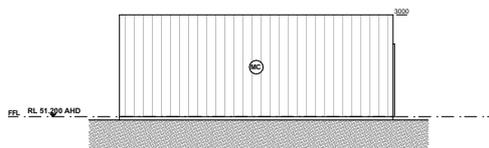
E1 WEST ELEVATION  
1:100



E2 NORTH ELEVATION  
1:100

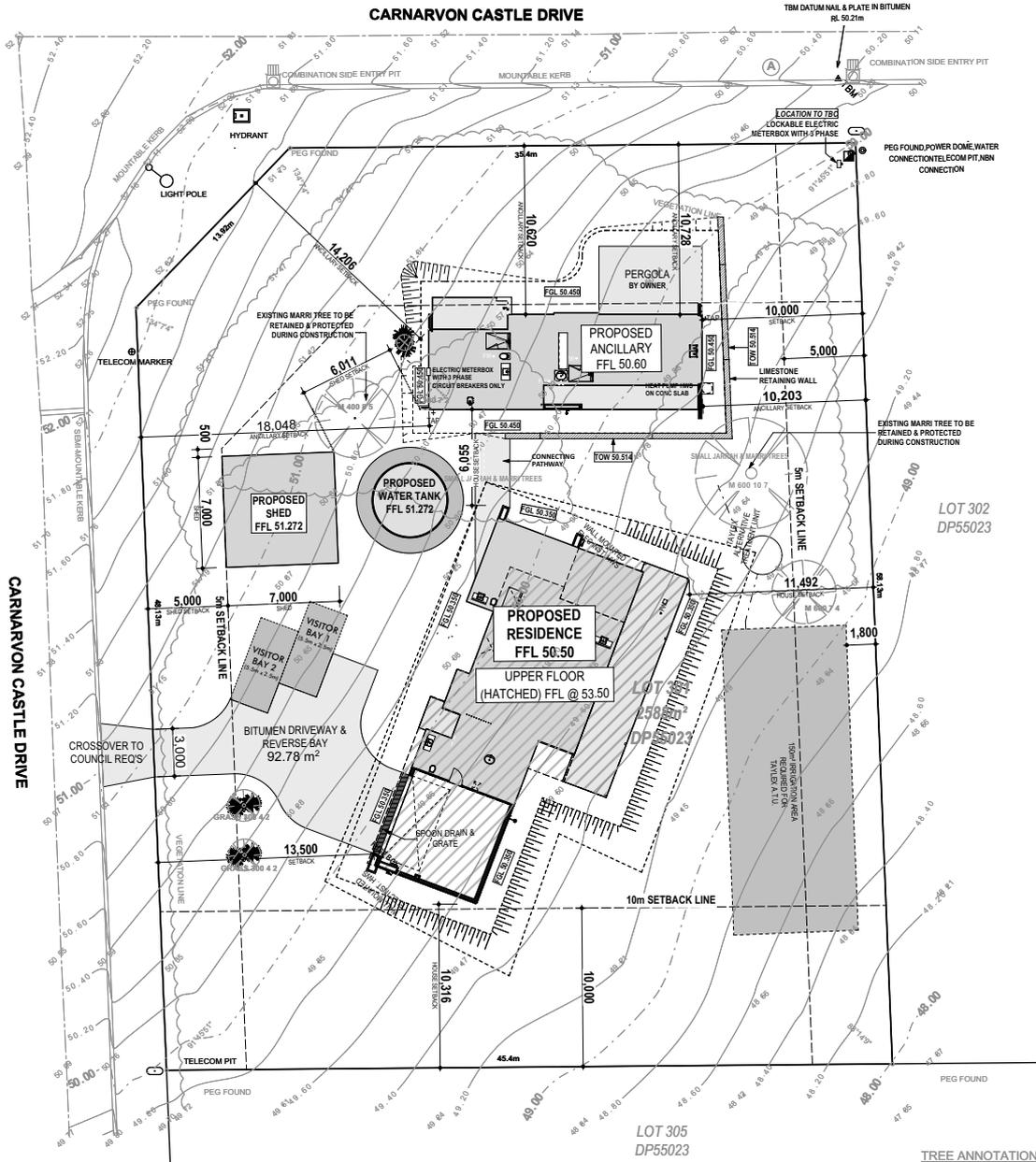


E3 EAST ELEVATION  
1:100



E4 SOUTH ELEVATION  
1:100

<b>UNBOUND DESIGN</b> M: 0454 720 170 E: JAYDEN@UNBOUNDDESIGN.COM.AU W: WWW.UNBOUNDDESIGN.COM.AU	PROJECT: PROPOSED RESIDENCE	GENERAL NOTES: 1. THIS DESIGN SHALL REMAIN THE SOLE PROPERTY OF UNBOUND DESIGN AND SHALL NOT BE USED FOR ANY OTHER PURPOSE THAN ORIGINALLY INTENDED. 2. BUILDING SPECIFICATION TAKES PRECEDENCE OVER THESE DRAWINGS. 3. REVIEW YOUR DESIGN DOCUMENTATION FOR ANY DETAILS. 4. THESE DRAWINGS TO BE READ IN CONJUNCTION WITH STRUCTURAL ENGINEER DRAWINGS & ANY CONSULTANTS DRAWINGS & SHALL PROCEED THESE DRAWINGS ANY DISCREPANCIES MUST BE REPORTED TO THE DESIGNER. 5. ALL DIMENSIONS TO BE CHECKED ON SITE PRIOR TO COMMENCEMENT OF WORKS ANY DISCREPANCIES MUST BE REPORTED TO THE BUILDER OR ARCHITECT. 6. ALL MATERIALS & WORKMANSHIP TO BE IN ACCORDANCE WITH LATEST AUSTRALIAN STANDARDS FOR MANUFACTURERS SPECIFICATIONS & SHALL PROCEED THESE DRAWINGS. 7. ALL WORKS TO BE IN ACCORDANCE WITH LOCAL AUTHORITY GUIDELINES. 8. DO NOT SCALE FROM DRAWINGS.	STATUS: DESIGN DEVELOPMENT	DRAWING: SHED FINISHES
	CLIENT: SMITH ADDRESS: LOT 301 CARNARVON CASTLE DRIVE, EAGLE BAY		APPROVED BY: JCP DATE: 06/01/2024 DESCRIPTION: ISSUED FOR CLIENT REVIEW	SHEET: 6 OF 6 SCALE: LD1115 AS NOTED ON A2



**PROPOSED SITE PLAN**  
SCALE 1:200 (A2 SHEET)

LEGEND		POWER DOME	
	TEMPORARY BENCHMARK		BOUNDARY
	COMBINATION SIDE ENTRY PIT		ROAD CENTRELINE
	WATER HYDRANT		KERB FACE
	WATER CONNECTION		KERB TOP
	TREE		VEGETATION LINE
	EXISTING SURFACE HEIGHT		FACE OF BUILDING
	TELECOM PIT		
	NBN CONNECTION		
	TELECOM MARKER		

- NOTES:
- Limited boundary marks found. Boundary resurvey recommended if building on or close to boundary.
  - Datum is AHD established from SSM BUS 64.
  - Above ground services located by survey. Underground services plotted from plans supplied by the relevant authorities. No liability is accepted for errors in underground service locations. All services to be located on site prior to any work being done.
  - No boundary marks found unless otherwise noted.
  - Contour interval is 0.1m.

**TREE ANNOTATION DETAIL**

- J = JARRAH
  - GRASS = GRASS TREE
  - M = MARRI
  - M 300 5.3
  - M: RED GUM/ MARRI
  - 300: TRUNK 0.3m DIAMETER
  - 5: HEIGHT 5m
  - 3: CANOPY 3m RADIUS
- 
- CANOPY NOT TO SCALE

Rev.	Initial Issue	Description	Drawn	Date	Checked



Surveyor: JW  
Survey date: 14/12/2020  
Pract/Cad: N/A

The contents of this plan are date stated within the revision panel. All consultants and persons wishing to utilise this data should satisfy themselves of this plans currency by contacting Survcvn.

The boundaries depicted on this plan were not re-established as part of this survey, therefore this plan does not guarantee their accuracy. Re-establishment of the cadastral boundaries is recommended for any proposed works on or near existing boundaries.



**FEATURE AND CONTOUR SURVEY**  
**LOT 301 ON DEPOSITED PLAN 55023,**  
**CARNARVON CASTLE DRIVE, EAGLE BAY.**

Client: **TEENA & LEE SMITH**

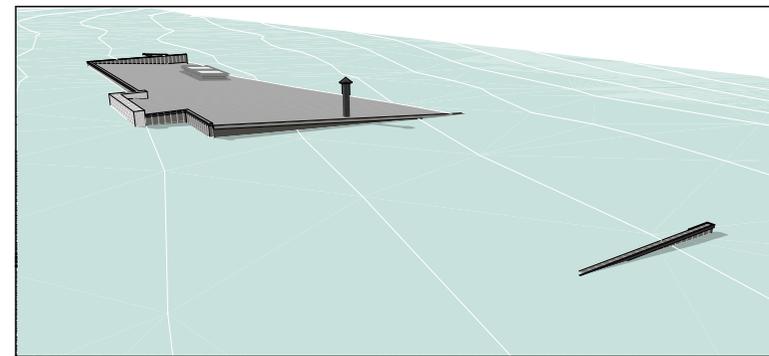
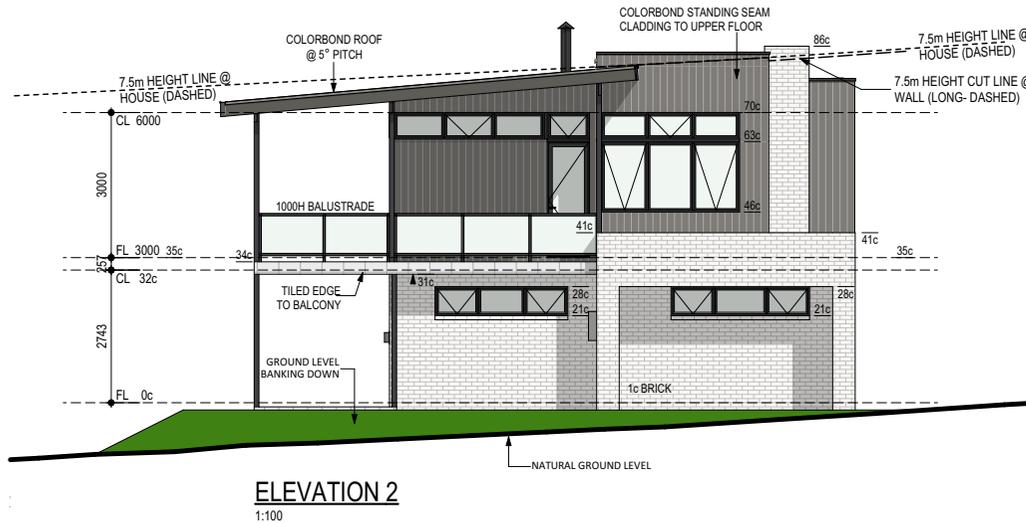
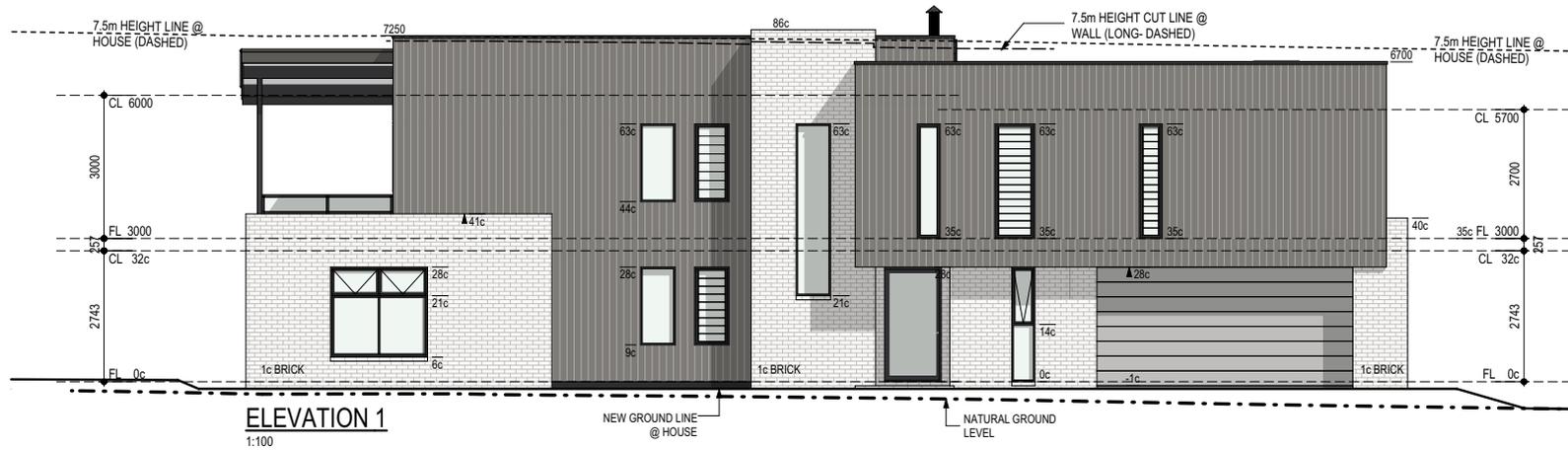
PO Box 365  
Dunsborough  
WA 6261  
Australia

Tel: (08) 9756 7719  
Fax: (08) 9756 8000  
mail@survcvn.com.au  
www.survcvn.com.au

PROJ: Michael  
MARRI - Ridge  
DATUM: AHD  
8366\_SS - A





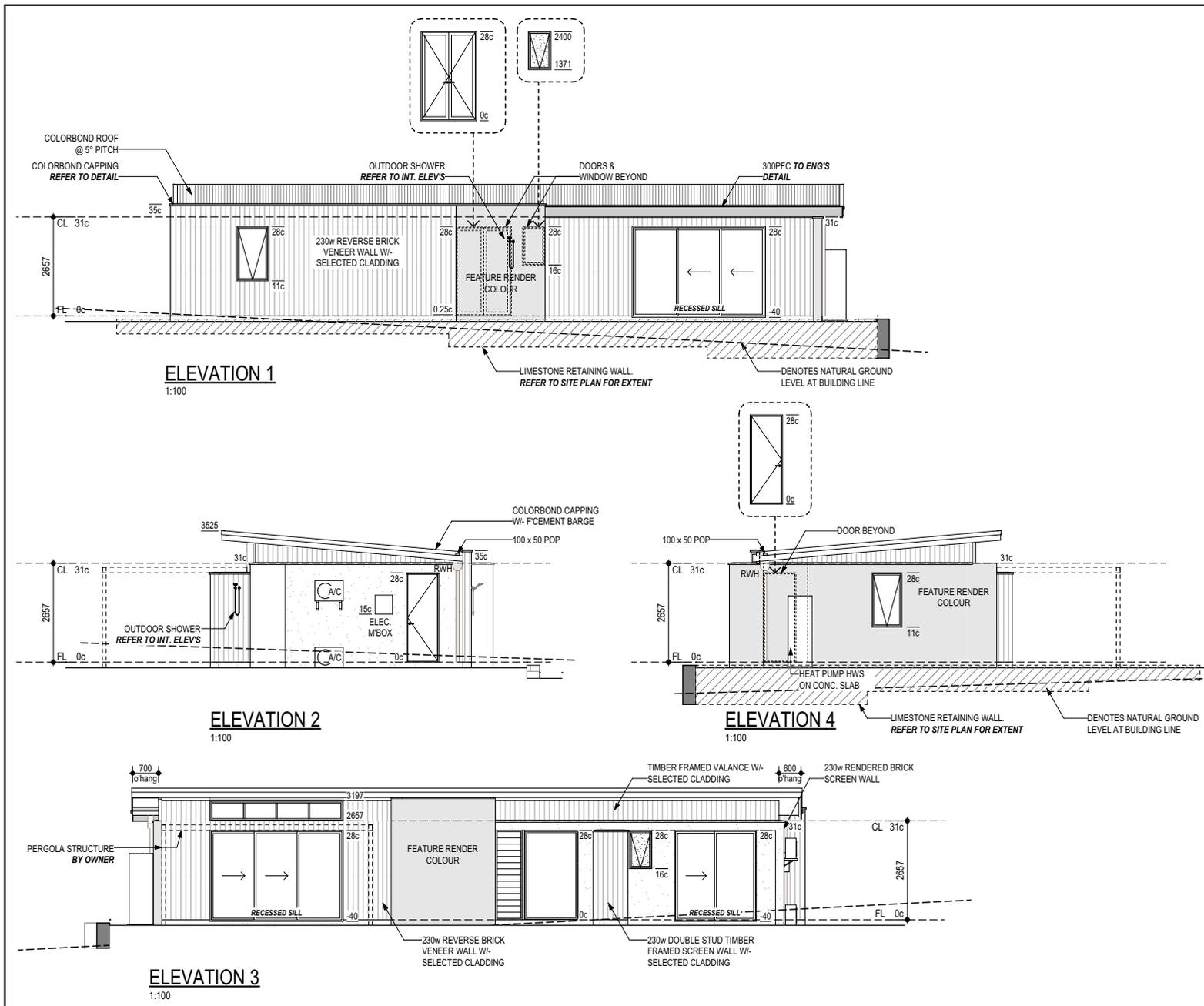


EXTENT OF PARAPET WALLS & ROOF BEYOND 7.5m HEIGHT PLANE

**SMITH RESIDENCE**  
 LOT 301, CARNARVON CASTLE DRIVE, EAGLE BAY  
 Consultant :Gaynor Designer :Franco Date :16/06/2023  
W:\JOB5 MASTER.300000 - 399999 (Framework)\302128 Smith\Working Drawings\01 Sketch\07 Franco 2022\Drawings\SMITH\_302128\_Franco Rev 1.3.pln







Special

ADVANTAGE SPECIFICATION	MODEL N°
© COPYRIGHT	6483

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W.A.

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Cnr Blair & Strickland Streets, Bunbury, W.A. 6230.  
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Rev. Builder N°: 11422. A.B.N. 64 105 492 140. A Division of JWH GROUP Pty Ltd

REV	VO #	DRN	DATE	CHK
1	Clarification Items	BSA	11/03/2022	BSA
2	Client Amends	BSA	17/03/2022	BSA
3	Engineering	BSA	06/05/2022	BSA
4	PCV 5	JKS	30/05/2022	

Sub-contractors to verify all dimensions on site.

THIS IS ONE OF THE DRAWINGS REFERRED TO IN THE CONTRACT.

DATED: ...../...../.....

OWNER	WITNESS
OWNER	WITNESS
BUILDER	WITNESS

CLIENT:

Teena L Smith

ADDRESS:

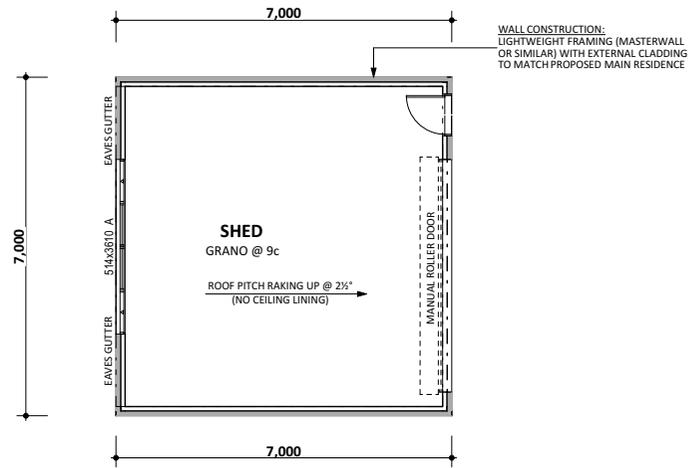
Lot 301  
Carnarvon Castle Drive  
EAGLE BAY

SHEET N° 2 OF 8  
+ 4 ANG

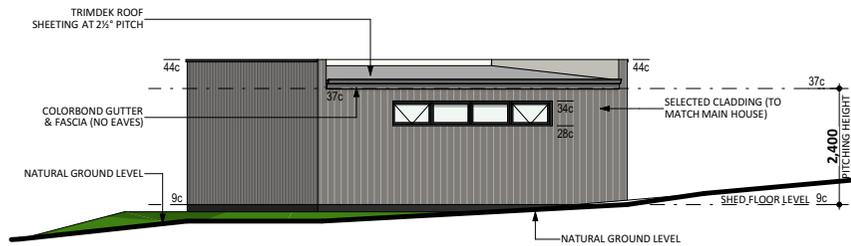
JOB N° 302553

REVISION 4 DATE 30/05/2022

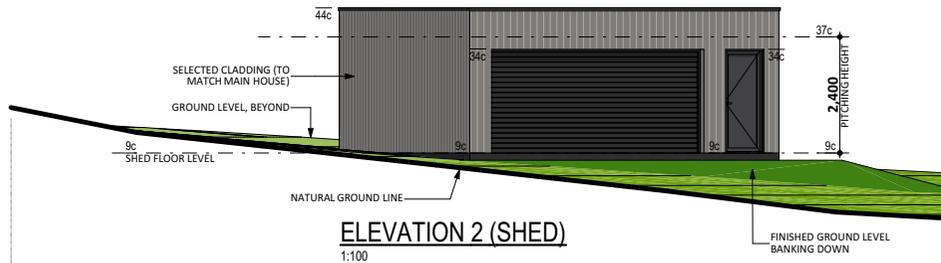




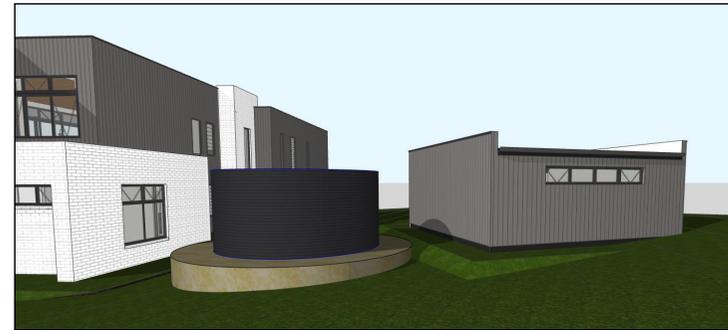
**SHED FLOOR PLAN**  
1:100



**ELEVATION 1 (SHED)**  
1:100



**ELEVATION 2 (SHED)**  
1:100



**SMITH RESIDENCE**  
LOT 301, CARNARVON CASTLE DRIVE, EAGLE BAY  
Consultant :Gaynor Designer :Franco Date :16/06/2023

W:\085 MASTER.300000 - 399999 (Framework).302128 Smith\Working Drawings\01 Sketch\07 Franco 2022\Drawings\SMITH\_302128\_Franco Rev 1.3.pln

Window Schedule														
Home Story Name	Ground Floor													
ID	SW-01	W-01	W-02	W-03	W-04	W-05	W-06	W-07	W-08	W-09	W-10	W-11	W-12	Upper Floor
From Room	SHED	PDR	ENTRY	RUMPUS	RUMPUS	GUEST 1 ENS	GUEST 1	GUEST 1	GUEST 2	GUEST 2	GUEST 2 ENS	LIFT	STUDY	W-13
Elevation View (from exterior)														
Frame type	Awning window	Awning window	Fixed window	Fixed window	Sliding stacker door	Special window	Sliding glass door	Awning window	Awning window	Awning window	Awning window	Special window	Special window	Fixed window
Frame Height	0.51	2.40	2.40	2.40	2.80	2.40	2.14	0.60	0.60	1.89	1.20	1.63	1.63	3.60
size Width	3.61	0.73	1.69	1.15	3.55	0.85	2.17	2.89	2.89	2.17	0.61	0.73	0.73	0.73
Glazing type	Clear glazing	Reeded glazing	Clear glazing	Clear glazing	Clear glazing	Clear glazing	Clear glazing	Clear glazing	Clear glazing	Clear glazing	Clear glazing	Clear glazing	Clear glazing	Clear glazing
Notes			Narrow frame / frameless style	Corner window	Corner stacker									

Window Schedule													
Home Story Name	Upper Floor												
ID	W-14	W-15	W-16	W-17	W-18	W-19	W-20	W-21	W-22	W-23	W-24	W-25	W-26
From Room	PORCH	PORCH	SITTING	ENSUITE	ENSUITE	MASTER SUITE	MASTER SUITE	TEENA	LIVING	LIVING	LIVING	LIVING	DINING
Elevation View (from exterior)													
Frame type	Fixed window	Fixed window	Louved window	Fixed window	Louved window	Awning window	Awning window	Awning window	Fixed window	Awning window	Sliding stacker door	Fixed window	Fixed window
Frame Height	1.20	2.40	2.40	0.43	2.40	2.40	2.40	1.63	2.40	0.55	3.00	0.55	2.40
size Width	0.90	0.85	0.85	2.48	0.85	1.93	2.41	1.45	1.21	1.21	3.58	3.60	3.58
Glazing type	Clear glazing	Clear glazing	Clear glazing										
Notes													

Window Schedule											
Home Story Name	Upper Floor										
ID	W-27	W-28	W-29	W-30	W-31	W-32	W-33	W-34	W-35	W-36	W-37
From Room	DINING	DINING	DINING	DINING	KITCHEN	SCULLERY	SCULLERY	SCULLERY	SCULLERY	LIFT	PDR 2
Elevation View (from exterior)											
Frame type	Fixed window	Sliding stacker door	Fixed window	Awning window	Awning window	Fixed window	Fixed window	Awning window	Awning window	Fixed window	Louved window
Frame Height	0.55	3.00	0.55	0.55	0.55	1.46	0.55	1.46	0.55	1.63	1.63
size Width	3.60	3.58	3.60	3.14	0.88	0.61	0.61	2.40	2.39	0.73	0.73
Glazing type	Clear glazing	Clear glazing	Clear glazing	Clear glazing	Clear glazing	Clear glazing	Clear glazing	Clear glazing	Clear glazing	Clear glazing	Clear glazing
Notes				MOTORISED (LINKED WITH W-31 & W-36)	MOTORISED			SERVERY WINDOW	MOTORISED		

REV	VO #	DRN	DATE	CHK
1	WORK'G DRAW'G	TW	30/07/2023	

Sub-contractors to verify all dimensions on site.

**ANCILLARY DRAWINGS**  
THESE DRAWINGS DO NOT FORM PART OF THE CONTRACT DOCUMENTATION  
TO BE USED FOR REFERENCE PURPOSES ONLY

CLIENT:  
**SMITH**

ADDRESS:  
Lot 301  
Carnarvon Castle Drive  
Eagle Bay

ANC SHEET #	A6 OF 7
JOB #	
REVISION	DATE
1	30/07/2023

Door Schedule - Ground Floor														
ID	D-01	D-02	D-03	D-04	D-05	D-06	D-07	D-08	D-09	D-10	D-11	D-12	D-13	D-14
From Room	PORCH	DRIVEWAY	L'DRY	RUMPUS	WINE	GARAGE	GARAGE PASSAGE	GARAGE PASSAGE	RUMPUS	STUDY	BED PASSAGE	BED PASSAGE	BED PASSAGE	GUEST 2
To Room	ENTRY	GARAGE	EXTERNAL	TERRACE	RUMPUS	GARAGE PASSAGE	PDR	L'DRY	BED PASSAGE	BED PASSAGE	WIL	GUEST 2	GUEST 1	GUEST 2 ENS
Plan View														
Elevation View (from door side)														
Frame type	Aluminium door	Sectional garage door	Aluminium door	Aluminium door	Aluminium door	Internal hinged door - metal frame	Internal hinged door - metal frame	Internal hinged door - metal frame	Cavity sliding door	Cavity sliding door	Internal hinged door - metal frame			
Frame Height	2.40	2.49	2.40	2.40	2.40	2.40	2.40	2.40	2.34	2.34	2.40	2.40	2.40	2.40
Frame Width	1.21	5.41	0.85	0.85	1.60	0.93	0.93	0.93	0.82	0.82	0.83	0.93	0.93	0.83
Glazing type	Clear glazing	Not applicable	Clear glazing	Clear glazing	Double glazed	Not applicable	Not applicable	Not applicable	Not applicable	Not applicable	Not applicable	Not applicable	Not applicable	Not applicable
Door hand	Right	N/A	Right	Right	Left	Right	Left	Left	Right	Right	Left	Left	Right	Left
Notes														

Door Schedule - Ground Floor						
ID	D-15	D-16	D-17	D-18	SD-01	SD-02
From Room	GUEST 1	GUEST 1 ROBE	GUEST 2 ROBE	L'DRY	EXTERNAL	EXTERNAL
To Room	GUEST 1 ENS	GUEST 1	GUEST 2	L'DRY	SHED	SHED
Plan View						
Elevation View (from door side)						
Frame type	Internal hinged door - metal frame	Sliding robe doors	Sliding robe doors	Sliding robe doors	Roller door	Aluminium door
Frame Height	2.40	2.14	2.14	2.40	2.14	2.14
Frame Width	0.83	1.71	1.71	3.00	4.81	0.85
Glazing type	Not applicable	Not applicable	Not applicable	Not applicable	Not applicable	Clear glazing
Door hand	Right	N/A	N/A	N/A	N/A	Right
Notes						

Door Schedule - Upper Floor										
ID	D-19	D-20	D-21	D-22	D-23	D-24	D-25	D-26	D-27	D-28
From Room	KITCHEN	ENSUITE	DRESSING	ENSUITE	MASTER SUITE	SITTING	SITTING	LIVING	LIFT	LIFT
To Room	UPPER TERRACE	WC	ENSUITE	MASTER SUITE	DRESSING	MASTER SUITE	TEENA	SITTING	KITCHEN	PDR 2
Plan View										
Elevation View (from door side)										
Frame type	Aluminium door	Internal Aluminium - LOH	Cavity sliding door	Cavity sliding door	Cavity sliding door	Internal Aluminium	Internal Aluminium	Internal Aluminium	Cavity sliding door	Cavity sliding door
Frame Height	2.40	2.70	2.70	2.70	2.70	2.70	2.70	2.70	2.70	2.70
Frame Width	0.88	0.78	0.72	0.82	0.82	0.88	0.88	1.70	0.82	0.72
Glazing type	Clear glazing	Not applicable	Not applicable	Not applicable	Not applicable	Not applicable	Not applicable	Not applicable	Not applicable	Not applicable
Door hand	Right	Right	Right	Right	Right	Left	Right	Both	Left	Left
Notes	External door		Platinum 9000	Platinum 9000	Platinum 9000				Platinum 9000	Platinum 9000

REV	VO #	DRN	DATE	CHK
1	WORK'G DRAWG	TW	30/07/2023	

Sub-contractors to verify all dimensions on site.

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TO BE USED FOR REFERENCE PURPOSES ONLY

CLIENT:  
SMITH  
  
ADDRESS:  
Lot 301  
Carnarvon Castle Drive  
Eagle Bay

ANC SHEET N°	A7 OF 7
JOB N°	
REVISION	DATE
1	30/07/2023

## Bushfire management plan/Statement addressing the Bushfire Protection Criteria coversheet

Site address:

Site visit: Yes  No

Date of site visit (if applicable): Day  Month  Year

Report author or reviewer:

WA BPAD accreditation level (please circle):

Not accredited  Level 1 BAL assessor  Level 2 practitioner  Level 3 practitioner

If accredited please provide the following.

BPAD accreditation number:  Accreditation expiry: Month  Year

Bushfire management plan version number:

Bushfire management plan date: Day  Month  Year

Client/business name:

	Yes	No
Has the BAL been calculated by a method other than method 1 as outlined in AS3959 (tick no if AS3959 method 1 has been used to calculate the BAL)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Have any of the bushfire protection criteria elements been addressed through the use of a performance principle (tick no if only acceptable solutions have been used to address all of the bushfire protection criteria elements)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Is the proposal any of the following (see [SPP 3.7 for definitions](#))?

	Yes	No
Unavoidable development (in BAL-40 or BAL-FZ)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Strategic planning proposal (including rezoning applications)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
High risk land-use	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Vulnerable land-use	<input type="checkbox"/>	<input checked="" type="checkbox"/>

None of the above

**Note:** Only if one (or more) of the above answers in the tables is yes should the decision maker (e.g. local government or the WAPC) refer the proposal to DFES for comment.

Why has it been given one of the above listed classifications (E.g. Considered vulnerable land-use as the development is for accommodation of the elderly, etc.)?

The information provided within this bushfire management plan to the best of my knowledge is true and correct:

Signature of report author or reviewer



Date

# Bushfire Management Plan

29 Carnarvon Castle Drive Eagle Bay

November 2022





## LIMITATIONS STATEMENT

This Bushfire Management Plan ('BMP') has been prepared for the assessment (*Planning and Development Act 2005*) to construct a single dwelling at 29 Carnarvon Castle Drive Eagle Bay, with State Planning Policy 3.7 (Bushfire). The site is within the City of Busselton which is the decision maker.

### Envision Bushfire Protection

ABN: 90958370365

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Email: admin@envisionbp.com.au

### Version Control

29 Carnarvon Castle Drive Eagle Bay			
Version	Date	Author	
V1	04 November 2022	Anthony Rowe	submission

### Copyright

Unless otherwise agreed in writing, this report is the intellectual property of Envision Bushfire Protection. The report is designed to be used exclusively by the person who commissioned it. Permission must be sought prior to the reproduction of any portion of this document, and every effort is made to ensure proper referencing of this document.

### Disclaimer

In undertaking this work, the authors have made every effort to accurately apply the available information at the time of writing following the instructions of the regulatory authorities and applying best practice as described by the Fire Protection Association Australia. Any conclusions drawn or recommendations made in the report are made in good faith, and the consultants take no responsibility for how this information and the report are subsequently used.

Envision Bushfire Protection accepts no liability for a third party's use of, or reliance upon, this specific report.

Envision Bushfire Protection accepts no liability for the inaction of the owner to provide or maintain the bushfire protection measures identified in this report. Vegetation is dynamic, building materials may distort, and the accumulation and the location of flammable materials near the building may affect the potential for damage or loss of a building to occur.

Failure to maintain the property and/or building to these standards may compromise an insurance policy if currently covering any of your assets or those of any third party that may be consequentially affected due such failure. If not insured, and if you are seeking insurance, this report may not influence the decision of any insurer not to offer cover.

Importantly the measures contained in this report cannot guarantee human safety or an absence of harm or that the building will not be damaged or would survive a bushfire event on every occasion. This is due to the unpredictable nature of fire behaviour (knowledge in this field continues to develop) and the unpredictable nature of extreme weather conditions.



### Scope of this report

Envision Bushfire Protection has been engaged to provide expert bushfire safety and planning advice.

The scope of the advice has been to assess the proposal for compliance with the policy measures described in State Planning Policy 3.7.

The investigations and mitigation measures identified in the BMP, has, in turn, formed the basis for the preparation of a Bushfire Emergency Evacuation Plan.

### Client relationship

I was engaged to provide expert bushfire safety and planning advice. My relationship with the client is a standard commercial contract, and no private, personal, or other matter has influenced the content of the BMP or my findings.

STATEMENT OF CONFORMITY – PLANNING AND DEVELOPMENT ACT 2005

**Anthony Rowe Level 3 - BPAD36690**

Principal



The signatory declares that this Bushfire Management Plan meets the requirements of State Planning Policy 3.7 and the Guidelines for Planning in Bushfire Prone Areas v1.4



## EXECUTIVE SUMMARY

### Preface

The proposal is to construct a single dwelling at 29 Carnarvon Castle Drive Eagle Bay

The site is within a declared bushfire prone area. Accordingly, the proposal is development that is required to be assessed for its compliance with State Planning Policy 3.7 *Planning in Bushfire Prone Areas* ('SPP 3.7') and the bushfire protection criteria described in the Guidelines V1.4.

The intent of the policy is: "**to preserve life and reduce the impact of bushfire on property and infrastructure**".

This BMP has been prepared in accordance with SPP 3.7 and Appendix Five in the Guidelines V1.4 and the Department of Planning Lands and Heritage (DPLH) *BMP Template for a complex development application*; the following has been prepared to comply with the DPLH template.

#### 1. Proposal details (addressed in Section 1)

The proposal comprises a single dwelling, shed (outbuilding) and paved driveway.

The site is a 2588 m<sup>2</sup> lot and located within a residential area in Eagle Bay.

The site has a consistent downward slope of 1.0° downward to the east and south.

The site is presently occupied by an Ancillary dwelling and vegetated with scrub (trees up to 6 m) and shrubland (shrubs < 2m).

#### 2. Environmental considerations (addressed in Section 2)

The site is located within a 'built-out residential area', in the township of Eagle Bay within the City of Busselton.

The site is identified to be within an area supporting *Threatened Ecological Communities* (DBCA-038) – Western Ring Tail Possum.

The placement of the dwelling, paved driveway and outbuilding will displace vegetation on site. The development of the land to the south is compliant with the land being low threat. The lot immediately east boundary is currently classed as Shrubland; it is zoned residential and can be expected to be developed with a house and gardens and following development will become a low threat.

The clearing of vegetation to facilitate the construction of a dwelling at an established residential lot is exempt from the requirements of State Environment legislation. The advice of the Department of Water, Environment Regulation should be sought prior to any clearing of vegetation. The Department provides services to the Environmental Protection Authority and the administration of the clearing requirements under the Environment Protection Act 1986.

#### 3. Bushfire assessment results (addressed in Section 3)

A Bushfire Attack Level assessment following **Method 1** AS 3959:2018, and the DPLH *Visual guide for bushfire risk assessment in Western Australia*, and using an FFDI of 80, has been undertaken.



Buildings and driveways will displace most of the vegetation on the site the remainder will be developed to an Asset Protection Zone Standard.

The land immediate to the south of the site is under development of single dwellings; whilst that development is not yet evident in aerial photos, the area is classified as low threat. The vacant lot immediately east of the site is currently classified as Shrubland but will become low threat following its development for the intended residential purpose.

The Indicative BAL (post development APZ) is attached at Figure 5.

The indicative BAL at the dwelling has been assessed as BAL-29 utilising Method 1.

#### **4. Identification of bushfire hazard issues (addressed in Section 4)**

The site is located within an area comprising low density residential lots. The lot to the south is now under construction with a single dwelling and a landscaped site. Immediately east is Shrubland, but that threat will be displaced when development of the residential lot occurs.

The site borders Carnarvon Castle Drive at both the west and south boundaries.

An area of contiguous Scrub is located to the west of the site but is separated from the site by Carnarvon Castle Drive. Fragmented Scrub is also located north of the site but is also separated from the site by Caravan Castle Drive.

The proposed dwelling will be sited to achieve BAL 29. The dwelling will not be subject to direct flame contact, other than the possibility resulting from the ignition of flammable materials if located near to the building.

Aside from radiant heat, the site may experience localised ember attack, from a fire in the scrub area to the north and west of the site. Ember attack from large forest areas within 5 km can also occur, but the density reduces as the distance from the firefront increases.

The construction standard (BAL 29) addresses measures to resist radiant heat and ember attack but the ignition of flammable materials if left near the building may exceed the construction standard. Ongoing site management and vigilance are required throughout the fire season to avoid the ignition of flammable materials from ember attack.

Carnarvon Castle Drive provides access to the site from the township of Eagle Bay, BAL Low, and from along the coast. It is a long no through road, but a community emergency access way has been established, as an alternate route, for the township to evacuate through to Cape Naturaliste Road.

The area is provided with a private reticulated water supply and a hydrant to Water Corporation standards is located immediate to the site. The Eagle Bay township has a volunteer fire brigade. Whilst attendance cannot be guaranteed, facilities to support fire suppression, water and the brigade, are available.

#### **5. Assessment against the bushfire protection criteria (addressed in Section 5)**

The proposal was compared with the four Bushfire Protection Criteria Elements: Location, Siting and Design, Access, and Water.



**Note:** The Guidelines for Planning in Bushfire Prone Area V1.4, no longer mandate compliance with the Element 3 Vehicle Access requirements that are external to the site, if the lot exists and the proposal is for a single dwelling, ancillary dwelling or minor development.

### **5.1 Bushfire Protection Criteria**

Element 1 Location - requires development avoid areas of extreme Bushfire Hazard level or achieve BAL-29 upon completion.

The proposed dwelling is BAL-29.

Element 2 Siting and Design – requires that a planning application should demonstrate the habitable buildings will not be exposed to a BAL greater than BAL-29.

Figure 5 illustrates the proposal upon completion can achieve BAL-29.

Element 3 Vehicle Access – requires the site has access to a through road to provide alternate routes of escape and access for fire fighters.

The proposal is for a single dwelling on a residential lot within a residential built out area. The public road network servicing the site has been established.

Only the technical requirements for private driveways apply to a proposal for a single dwelling, if the driveway is longer than 70 m.

The proposed dwelling is less than 70 m from a public road. The technical requirements, for a private driveway do not apply.

Element 4 Water – requires the availability of water for firefighting purposes. The site has access to a reticulated water supply and hydrants are located in Carnarvon Castle Drive immediate to the site.

### **5.2 Additional Bushfire Management Strategies** (addressed in section 5.2)

No additional bushfire management strategies have been identified.

### **5.3 Spatial representation of the bushfire management strategies** (Figure EX 1)

The key features demonstrating compliance with the bushfire protection measures are identified on the *Spatial representation of the bushfire management strategies*.

These actions are reflected in the following *Responsibilities for implementation and management of the bushfire measures*.

## **6. Responsibilities for implementation and management of the bushfire measures**

*The Owner responsibilities (Guidelines 4.6.3) identify the bushfire management measures necessary to achieve compliance with the bushfire protection criteria.*

*The Responsibilities for Implementation and Management of the Bushfire Measures, addressed in section 6 of the BMP and identified at Figure EX1 supersede or are additional to the details included within the development application.*

Figure Ex1: Spatial representation of the bushfire management strategies



**Responsibilities for implementation and management of the bushfire measures**

1. The site (29 Carnarvon Castle Drive Eagle Bay) is to be maintained in compliance with the Asset Protection Zone standard describe in Element 2, Schedule 1 Standards for Asset Protection Zones (Guidelines Planning in Bushfire Prone Areas v1.4).
2. The dwelling to be constructed to BAL-29, commensurate to the BAL rating (following the maintenance of the site in APZ standard). The BAL-29 construction requirements are described in s. 3 and 7 in AS 3959:2018.
3. Fire hoses are provided at the dwelling capable of applying water to all external surfaces of the dwelling and for a distance of 10 m around the dwelling. The hoses are to comply with AS/NZS1221 and be shielded from radiant heat.

**LEGEND**

- Site Boundary
- Asset Protection Zone
- Access
- H Hydrant

**Property Assessment Details**

29 Carnarvon Castle Drive Eagle Bay

Prepared by: Anthony Rowe

Accreditation Level: Level 3

Accreditation Number: 36690

Date: 4 November 2022



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## 1. PROPOSAL DETAILS

### 1.1 Introduction

Envision Bushfire Protection has been commissioned to prepare a Bushfire Management Plan, in support of an application for construction of a single dwelling at Lot 301 Carnarvon Castle Drive Eagle Bay.

The subject land is within a declared bushfire prone area (Plate 2).

In accordance with SPP 3.7 the planning authority in determining an application in a declared bushfire prone area must be satisfied the proposal is consistent with the Policy intent, ***to preserve life and reduce the impact of bushfire on property and infrastructure.***

#### **Purpose of this Plan**

The purpose of this BMP is to assess the suitability of the proposal and its location for the intended use and identify the measures to avoid an increase in the threat of bushfire and reduce the vulnerability of people and property (to the degree necessary) from a 'significant adverse bushfire impact'.

SPP 3.7 is a risk-based framework that utilises AS 3959:2018 to determine the bushfire risk. Acceptable Solutions, or compliance through performance principle described under each Element in the Bushfire Protection Criteria, represent acceptable treatments, determined as an acceptable risk.

#### **Site and Proposal Description**

The proposal is to utilise the single dwelling.

An inspection of the locality and the site was undertaken on 7 May 2022.

The land adjoining the site to the south has a single dwelling under construction and a narrow area of retained remnant vegetation (excluded) is located at the south boundary. The residential lot to the east of the site is yet to be developed and is characterised by shrubs up to 2 m (shrubland).

Vegetation in the locality and on the site was recorded and witnessed with GPS reference photographs, and slopes recorded with a Nikon forestry Pro.

The road access routes, were traversed as part of the inspection and determined to be suitable for two wheel drive vehicles.

Hydrants were identified to be in proximity to the site, along Carnarvon Castle Drive.



**Proposal details**

The proposal and its context comprise:

<b>Address</b>	<b>29 Carnarvon Castle Drive</b>			
<b>Local Government Area</b>	City of Busselton			
<b>Local Planning Scheme Zone</b>	The land is zoned Residential in the City of Busselton Local Planning Scheme 21.			
<b>Bushfire Season</b>	November – May (precise dates vary annually)			
<b>Development proposal</b>	Single dwelling			
<b>Building class</b>	Class 1			
<b>Lot size</b>	2588 m <sup>2</sup>			
<b>Land description site</b>	The site is currently vacant of any building			
<b>Adjoining Land uses</b>	North	East	South	West
	Contiguous Scrub extending north.	Residential, remnant vegetation until developed.	Residential, followed by remnant vegetation.	Residential Fragmented scrub extending west.
<b>Road Access</b>	The site is accessed by Carnarvon Castle Drive, it connects to Fern Road-Eagle Bay Road to offer travel into Dunsborough town centre 9 km to the south. An alternate route to Dunsborough is provided through an Emergency Access Way to Cape Naturaliste Road from Carnarvon Castle Drive (west from the site) or from Annamaria Rise by road south of the site.			
<b>Nearest town centre</b>	Dunsborough			
<b>Water supply</b>	The site has access to a reticulated water supply and a hydrant network.			
<b>Tele communications</b>	The site is within the Telstra 4G network			
<b>Emergency services</b>	The nearest rural fire brigade is the Eagle Bay Volunteer Bush Fire Brigade located on Fern Road 700 m east of the site.			
<b>Minor Development</b>	N/A			
<b>Unavoidable development</b>	N/A			
<b>Vulnerable Development</b>	N/A			
<b>High risk land use</b>	N/A			



Plate 1: Site in Locality

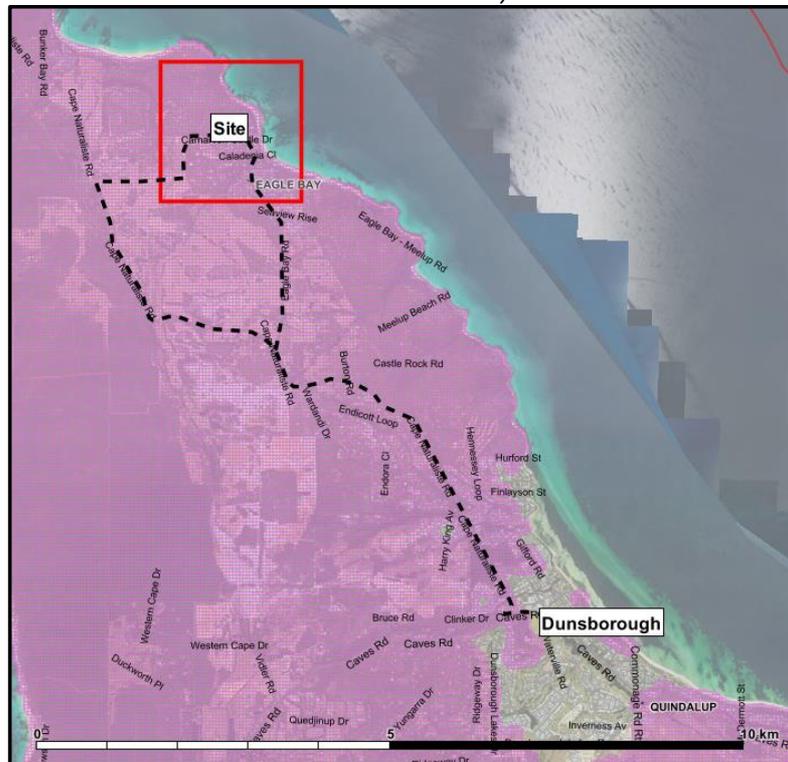


Plate 2: OBRM Bushfire Prone Area (Pink area)

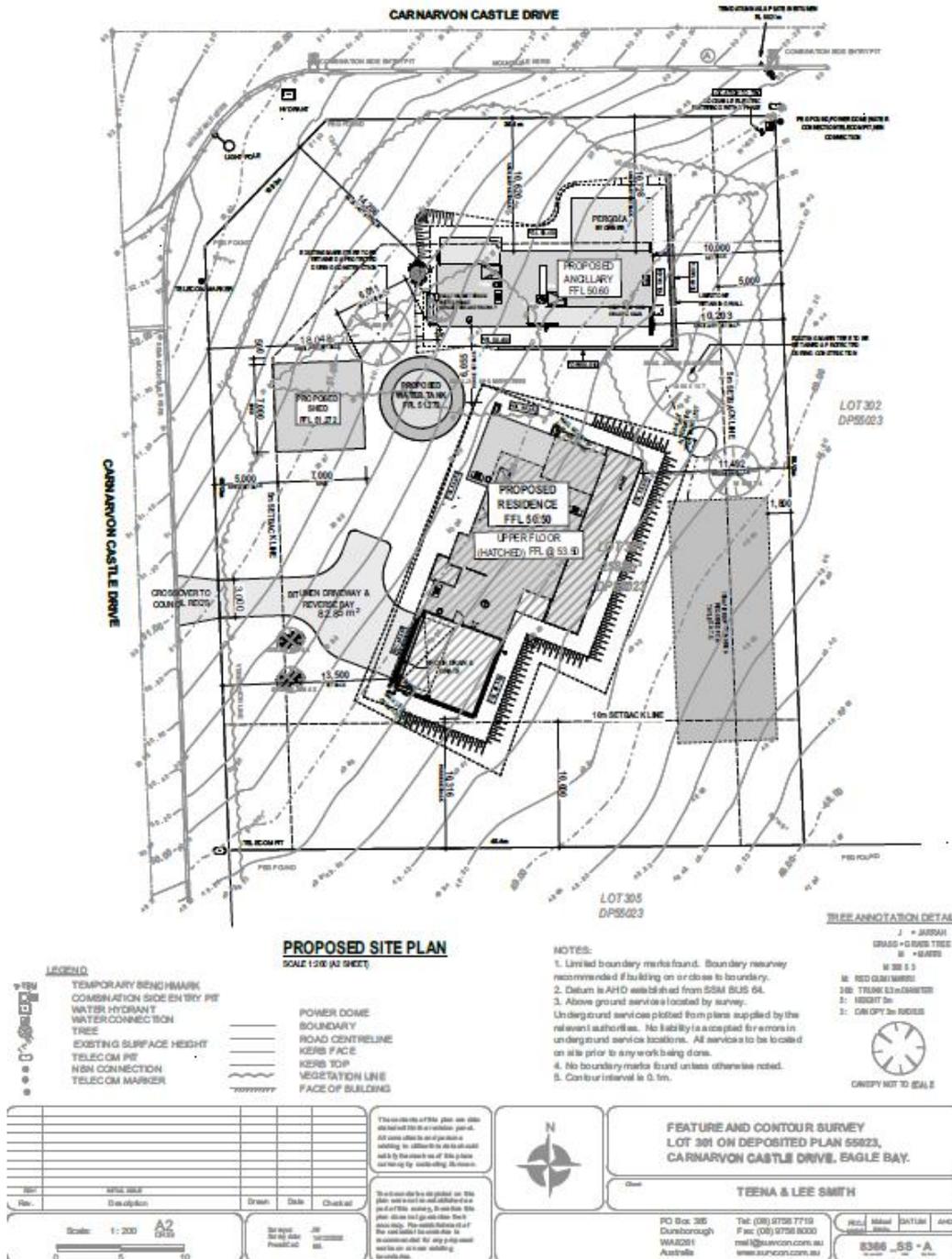


Plate 3a: Floor plan



*Plate 3b: Dwelling elevations*



## 1.2 Regulatory Compliance Requirements

### ***Planning and Development Act 2005 - SPP 3.7***

On 7 December 2015, the State Government introduced by Gazette, a state map of Bushfire Prone Areas by order under the *Fire and Emergency Services Act 1998* and introduced development controls in Bushfire Prone Areas through the *Planning and Development Act 2005*. These controls were authorised by State Planning Policy 3.7 (Planning in Bushfire Prone Areas) regulations introduced under Part 10A Schedule 2 of the *Planning and Development (Local Planning Scheme) Regulations 2015* and guided by the *Guidelines for Planning in Bushfire Prone Areas*.

The State Planning Policy, Regulations, and Guidelines now form the foundation for fire risk management planning in WA at a community and land development level. The Policy Intent of SPP 3.7 is ***to preserve life and reduce the impact of bushfire on property and infrastructure***.

Applicable clause from SPP 3.7, include:

SPP 6.2: A development application within a bushfire prone area has or will, on completion, have a moderate BHL and/or where BAL-12.5 to BAL-29 applies, may be considered for approval. *In this instance the proposal can achieve BAL 29 (refer to Figure 5).*

SPP 6.5: A development application in a bushfire protection area is to be accompanied by an assessment against the bushfire protection criteria contained within the Guidelines, demonstrating compliance within the boundary of the development. *The proposed development requires no imposition beyond its site.*

SPP 6.7: A development that will result in the introduction or intensification of development or land use in an area that has or will, on completion, have an extreme BHL and/or BAL-40 or BAL-FZ will not be supported. *The dwelling can achieve BAL 29.*

SPP 6.10: The decision-maker may impose a 'notice on title' advising that the site is located in a bushfire prone area and is subject to a Bushfire Management Plan. *This is routinely imposed as a condition of planning approval.*

### ***Building Act 2011***

The proposal is a Class 1 building (single dwelling) for the purpose of the *Building Act 2011*. The Act only applies the National Construction Code 2019 AS 3959:2018 to the construction of class 1, 2, 3, and 10a, buildings but AS 3959:2018 is used to determine the siting considerations for all habitable buildings.

Outbuildings are not required to be constructed to a BAL standard if it is located more than 6 m from a habitable building. In this instance the dwelling as the habitable building will be required to be constructed to the BAL level commensurate to its location. The Existing Ancillary Dwelling is constructed to the required BAL level. The proposed outbuilding (shed) is located more than 6 m from the habitable building and will not require a bushfire construction standard.

### ***Bush Fires Act 1954***

Section 33 of the *Bush Fires Act 1954* recognises the responsibility of all landowners to prevent the spread of bushfire. The City issues a Firebreak Notice annually. The City can issue a notice upon the landowner to act as and when specified in the notice with respect to anything which is upon the land, and which in the opinion of the local government or its duly authorised officer, is or is likely to be, conducive to the outbreak of a bushfire or the spread or extension of a bushfire.

An owner who fails to comply with a notice is guilty of an offence (Penalty: \$5,000). The local government may in addition carry out the required works of the notice and recover the costs incurred by application to a Court.



## 2. ENVIRONMENTAL CONSIDERATIONS

### 2.1 Native Vegetation – Modification and Clearing

A fundamental consideration in the assessment of development under SPP 3.7 is to avoid instances where bushfire risk management measures would conflict with or be limited by other biodiversity management measures.

#### ***Conservation class legislation***

The ability to implement bushfire risk management measures may be affected by a number of conservation class vegetations: described below.

#### ***Environment Protection Act 1986 and Environmental Protection (clearing native vegetation) Regulation 2004***

It is an offense to clear native vegetation without the authority of a permit or an exemption. The act of clearing native vegetation, requires a permit from either the Department of Water and Environmental Regulation (DWER) or the Department of Mines, Industry Regulation and Safety (DMIRS), unless an exemption applies.

Exemptions include:

#### ***Environment Protection Act 1986***

- Clearing required by local government Section 33 *Bush Fires Act 1954*.
- **Clearing in accordance with the terms of a subdivision approval.**
- Clearing in accordance with a permit under the *Bush Fires Act 1954* (prescribed burning) and clearing by a bushfire control officer.

*Environmental Protection (clearing native vegetation) Regulation 2004* (exemptions do not apply in Environmentally Sensitive Areas, and clearing > than 5 ha)

<https://www.der.wa.gov.au/your-environment/environmentally-sensitive-areas>

- Clearing to the extent necessary to construct an approved building.
- Clearing that is for fire hazard reduction burning.
- Clearing to maintain an area cleared in the last ten years.

#### ***(WA) Biodiversity Conservation Act 2016 and Bio-diversity Conservation Regulations 2018***

The *Biodiversity Conservation Act, 2016*, replaces the *Wildlife Conservation Act, 1950*, and the *Sandalwood Act, 1929*, it became operational with the *Bio-diversity Conservation Regulations 2018*, on 1 January 2019.

The Act provides for listing species, threatened ecological communities (TECs), key threatening processes, and critical habitats. It introduces criteria for listing species 'endangered', 'critically endangered' or 'vulnerable,' to align with the Environment Conservation and Biodiversity Conservation Act 1999 (Cth).

The *Biodiversity Conservation Act 2016* recognises that activities approved under the *Environment Protection Act 1986* do not require further approval include clearing of native vegetation that is either exempt or done under the authority of a clearing permit or done in accordance with an implementation decision under Part IV of the *Environment Protection Act 1986*.



Commonwealth Environment Protection Biodiversity Conservation Act 1999

The Commonwealth Environment Protection Biodiversity Conservation Act 1999 provides for the protection of matters of national environmental significance. National environment law does not generally regulate fire prevention measures taken by state and territory governments, but no specific exemptions are provided.

In accordance with the Department of Planning Lands and Heritage template (BMP template to support a BAL Contour Assessment) a review of the listed databases has been undertaken as part of this assessment to identify whether restrictions or other specific considerations may apply that would affect the implementation of any bushfire protection initiatives that may otherwise be identified.

A fundamental consideration in the assessment of development under SPP 3.7 is to avoid instances where bushfire risk management measures would conflict with or be limited by other biodiversity management measures.

In accordance with the Department of Planning Lands and Heritage template (Bushfire Management Plan template to support a BAL Contour Assessment) a review of the listed databases (table 1) has been undertaken as part of this assessment to identify whether restrictions or other specific considerations may apply that would affect the implementation of any bushfire protection initiatives that may otherwise be identified.

**Table 1: Ecological database assessment.**

Is the land affected by:	Yes/No/NA	If yes - describe
Conservation Wetland or buffer (DBCA-019 DBCA-017)	No	
RAMSAR Wetland (DBCA-010)	No	
Threatened and Priority Flora (DBCA-036)	No	
Threatened and Priority Fauna (DBCA-037)	Yes	The site is classified as Low WRT suitability.
Threatened Ecological Communities (DBCA-038)	No	
Bush Forever (COP-071)	No	
Environmentally Sensitive Area (DWER-046)	No	
Regionally Significant Natural Areas (DWER-070)	No	
Conservation Covenant (DPIRD-023)	N/A	
<b>Does the proposal require the removal of restricted vegetation?</b>	<b>Yes</b>	<b>No</b>

No clearing of regulated vegetation is required beyond the site, but vegetation will be displaced by the building and modified to achieve the APZ within the site.

At the time of writing this report amendments to the Environment Protection Act 1986 had been proclaimed to be effective from 23 October 2021. The Department of Water Environment and



Regulation (DWER) is in the process of publishing guidance for the assessment criteria to determine whether a clearing permit is required.

The site may also be subject to other controls restricting the removal of native vegetation on the site which includes the classification of the site within the City’s Landscape Value Area, and a restrictive covenant on the land title not to clear vegetation on the land except for certain exemptions. This Bushfire Management Plan and the APZ it may identify, as part of an authorisation under the *Planning and Development Act 2005*, does not direct the considerations of other policy by the decision maker or form the basis for an exemption from the approval process described by the restrictive covenant.

## 2.2 Re-vegetation/Landscape Plans

The vegetation at the site will be largely displaced by the building and paved surfaces. Narrow areas of remnant vegetation have been retained at the development of sites to the east and south of the site. The retained amount can be considered as part of a cultivated garden and excluded as a threat. Similarly vegetation that may be retained at the site will be narrow, less than the area displaced by buildings and paving and classified as part of a cultivated garden.

## 3. BUSHFIRE ASSESSMENT

### 3.1 Bushfire Attack Level Assessment (Inputs)

The following assessment has applied the methodologies described in AS 3959:2018, the Guidelines, and has used the Fire Protection Association Australia accredited practitioner methodology for the preparation of Bushfire Attack Level (BAL) assessments.

All vegetation within 150 m (context) of the subject building has been classified following Clause 2.2.3 (AS 3959:2018) to determine the predominant vegetation affecting the behaviour at the locality. The Bushfire Attack Level is determined by the **predominant** vegetation within 100 m of the site boundary (for subdivision), or around the development site (building envelope) or the external face from a habitable building.

The classifications of vegetation used in AS 3959:2018 are based on foliage cover, measured as a percentage of a hectare and by the fuel (vegetation) height.

**Foliage cover:** *The portion of the ground that would be shaded by foliage when the sun is shining directly overhead , expressed as a percentage for each stratum or identifiable layer of vegetation*

AS 3959:2018

Layer/ Stratum	Description	Hazard
Bark	Tight/fine – course/ribbon	Spotting and ember attack potential associated with forest
Canopy	Trees taller than 6 m (forest)	Influences the flame height
Elevated fuel	Trees and Shrubs up to 6 m	Influences the flame height
Near surface	Grasses and shrubs taller than 100 mm and up to 2 m	Influences the rate of spread and canopy ignition
Surface	On ground material, leaves, twigs, bark	Influences the rate of spread

From CFA (Vic) Overall fuel assessment guide 2010

AS 3959:2018 prescribes six categories of Bushfire Attack Level (BAL): BAL-LOW, BAL-12.5, BAL-19, BAL-29, BAL-40, and BAL-FZ. In addition, BAL-FZ describes only performance solutions where the separation from classified vegetation (on completion) is less than 10 m. The BAL level is used for determining the siting of development (to be less than BAL-40) and in turn the construction standard that is equivalent to the BAL at the proposed building location.

This assessment has followed the guidance of AS 3959:2018. This includes:

- A recognition of excluded vegetation types described at cl.2.2.3.2 (e) and (f), but the underlying vegetation should still be classed e.g. an orchard may be excluded but not the grassland within it.
- A separate plot is applied if there is a variation in the slope greater than 5.0°
- For various vegetation classes a representation that is less than 10%, does not constitute the predominant class. Foliage cover referred to in AS 3959:2018 for various classes is based on the foliage cover for that class as a percentage of a ha. (shadow cast is not representative of foliage cover).
- The measurement point and the most influential vegetation class (presenting the highest BAL at the building) is used for the determination of the BAL at the building (Figure 2.2 AS 3959:2018).
- Consideration of the predominant vegetation is to consider the likelihood of regeneration.
- Orchards, and single tree rows (planted in a row less than 10 m wide) is determined by underlying the near surface fuel.

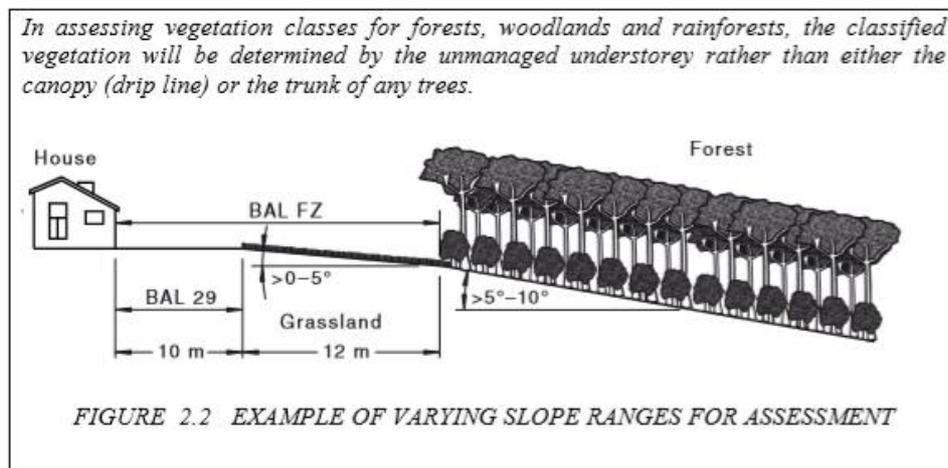


Plate 5: Effective Slope and measurement taken from AS 3959:2018

Effective slope under each vegetation plot was assessed in accordance with the methodology detailed in AS 3959:2018 Construction of buildings in bushfire prone areas (AS 3959) (Standards Australia, 2018 Bushfire Fuels). Slope data was measured on site and cross referenced with Landgate elevation data.



**Photo evidence Site Assessment & Site Plans**

The assessment of this site / development was undertaken on 7 May 2022 by a BPAD Accredited Practitioner for the purpose of determining the Bushfire Attack Level in accordance with AS 3959:2018 Simplified Procedure (**Method 1**).

**Figure 1: Location**

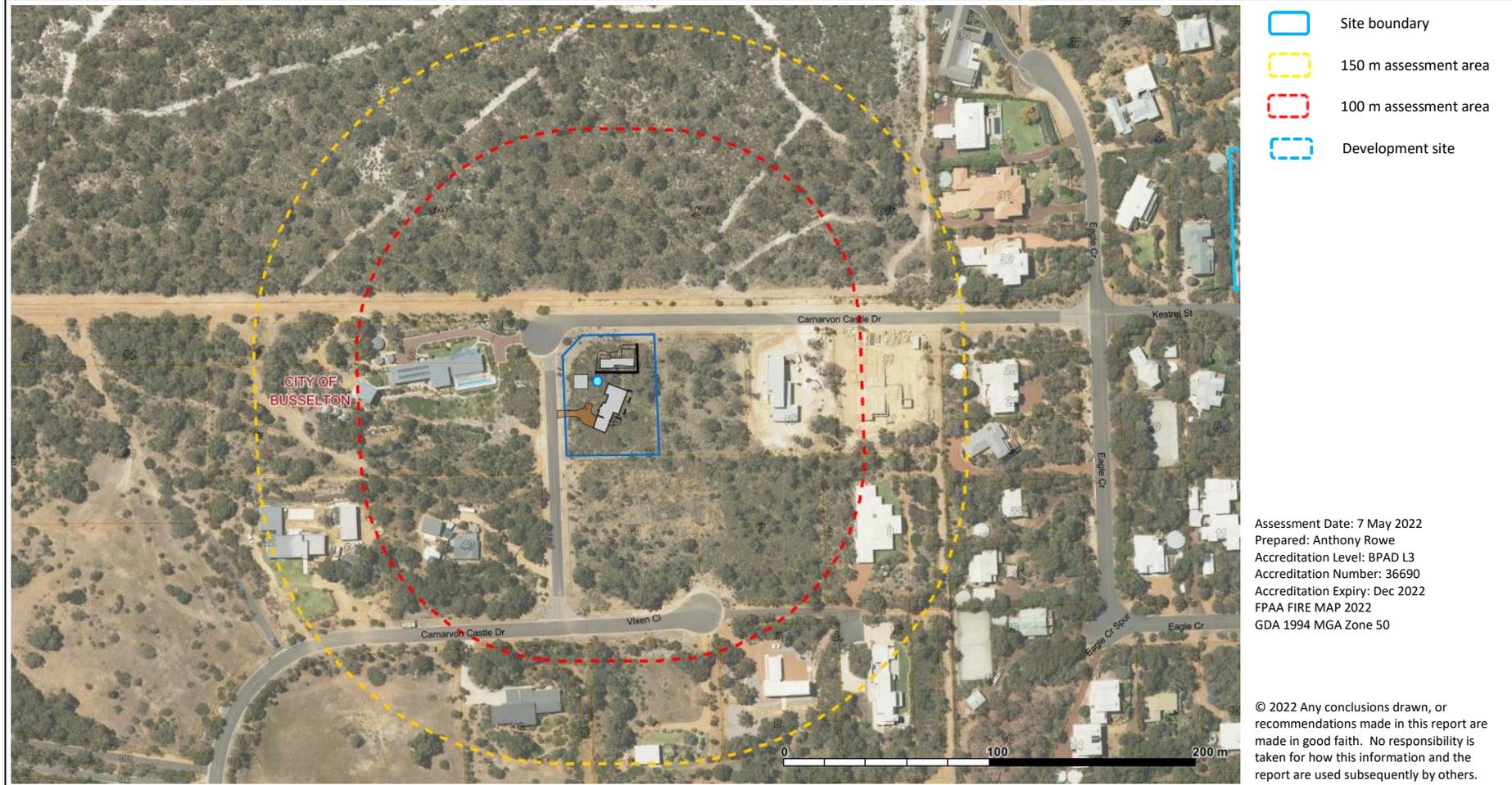


Figure 2: Topography

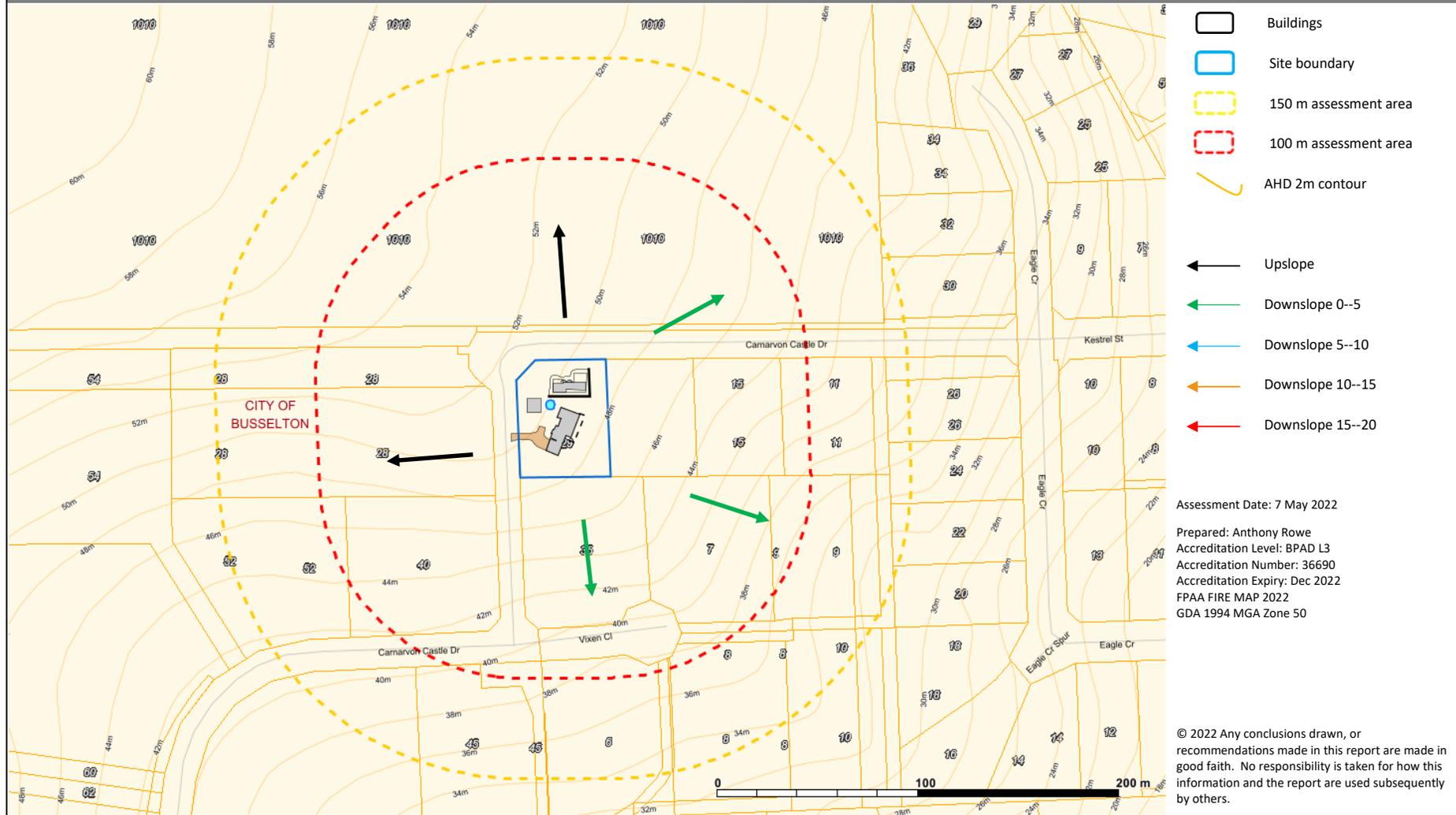
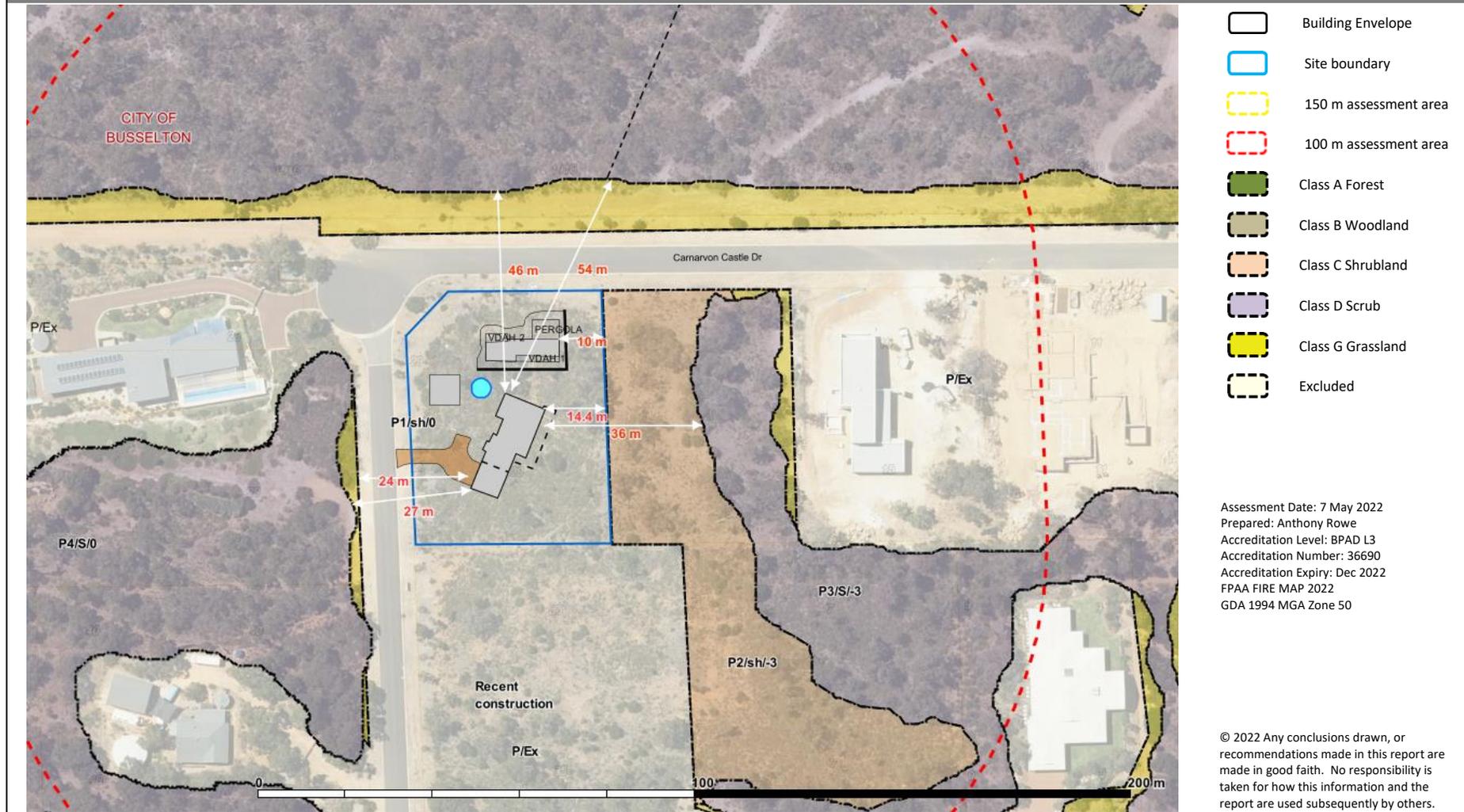


Figure 3: Vegetation Classification and photo locations – vegetation on site assumed as excluded



Figure 4: Vegetation Classification - Post Development –excluded site (enlarged)



PLOT: Ex							
Vegetation Classification			Vegetation Description (AS3959)				
Excludable - 2.2.3.2(e) Non Vegetated Areas			2.2.3.2 Exclusions – Low threat vegetation and non-vegetated areas:  Non-vegetated areas, that is, areas permanently cleared of vegetation, including waterways, exposed beaches, roads, footpaths, buildings and rocky outcrops.				
Slope							
N/A							
Observation/Justification for classification							
Fuel Hazard	Surface	Near surface				Elevated	Bark
Low	✓	✓				✓	✓
Moderate							
High							
Very High							
Extreme							
Post development							
Excludable - 2.2.3.2(e) Non Vegetated Areas							
			PHOTO ID: Ex 1				

PLOT:Ex							
Vegetation Classification			Vegetation Description (AS3959)				
Excludable - 2.2.3.2(e) Non Vegetated Areas			2.2.3.2 Exclusions – Low threat vegetation and non-vegetated areas:  Non-vegetated areas, that is, areas permanently cleared of vegetation, including waterways, exposed beaches, roads, footpaths, buildings and rocky outcrops.				
Slope							
N/A							
Observation/Justification for classification							
Fuel Hazard	Surface	Near surface				Elevated	Bark
Low	✓	✓				✓	✓
Moderate							
High							
Very High							
Extreme							
Post development							
Excludable - 2.2.3.2(e) Non Vegetated Areas							
			PHOTO ID: Ex 2				

PLOT: Ex							
Vegetation Classification			Vegetation Description (AS3959)				
Excludable - 2.2.3.2(e) Non Vegetated Areas			2.2.3.2 Exclusions – Low threat vegetation and non-vegetated areas:  Non-vegetated areas, that is, areas permanently cleared of vegetation, including waterways, exposed beaches, roads, footpaths, buildings and rocky outcrops.				
Slope							
N/A							
Observation/Justification for classification							
Fuel Hazard	Surface	Near surface				Elevated	Bark
Low	✓	✓				✓	✓
Moderate							
High							
Very High							
Extreme							
Post development							
Excludable - 2.2.3.2(e) Non Vegetated Areas							
<b>PHOTO ID: Ex 3</b>							

PLOT: 1							
Vegetation Classification			Vegetation Description (AS3959)				
Class C Shrubland - Closed heath C-10			Found in wet areas and/or affected by poor soil fertility or shallow soils. Shrubs 1 m -2 m high. Wet heaths occur in sands adjoining dunes of the littoral (shore) zone. Montane heaths occur on shallow or water-logged soils.				
Slope							
Flat							
Observation/Justification for classification							
Fuel Hazard	Surface	Near surface				Elevated	Bark
Low							✓
Moderate	✓					✓	
High							
Very High		✓					
Extreme							
Post development							
Excludable - 2.2.3.2(e) Non Vegetated Areas							
<b>PHOTO ID: 1.1</b>							
Established trees within the site, adjacent the building position.							

PLOT: 1					
Vegetation Classification			Vegetation Description (AS3959)		
Class C Shrubland - Closed heath C-10			Found in wet areas and/or affected by poor soil fertility or shallow soils. Shrubs 1 m -2 m high. Wet heaths occur in sands adjoining dunes of the littoral (shore) zone. Montane heaths occur on shallow or water-logged soils.		
Slope					
Flat					
Observation/Justification for classification					
Fuel Hazard	Surface	Near surface			
Low				✓	
Moderate	✓		✓		
High					
Very High		✓			
Extreme					
Post development					
Excludable - 2.2.3.2(e) Non Vegetated Areas					
			PHOTO ID: 1.2		

PLOT: 2					
Vegetation Classification			Vegetation Description (AS3959)		
Class C Shrubland - Closed heath C-10			Found in wet areas and/or affected by poor soil fertility or shallow soils. Shrubs 1 m -2 m high. Wet heaths occur in sands adjoining dunes of the littoral (shore) zone. Montane heaths occur on shallow or water-logged soils.		
Slope					
Downslope 0-5					
Observation/Justification for classification					
Fuel Hazard	Surface	Near surface			
Low				✓	
Moderate	✓		✓		
High					
Very High		✓			
Extreme					
Post development					
Class C Shrubland					
			PHOTO ID: 2.1		

PLOT: 2							
Vegetation Classification			Vegetation Description (AS3959)				
Class C Shrubland - Closed heath C-10			<p>Found in wet areas and/or affected by poor soil fertility or shallow soils. Shrubs 1 m -2 m high. Wet heaths occur in sands adjoining dunes of the littoral (shore) zone. Montane heaths occur on shallow or water-logged soils.</p>  <p>PHOTO ID: 2.2</p>				
Slope							
Downslope 0-5							
Observation/Justification for classification							
Fuel Hazard	Surface	Near surface				Elevated	Bark
Low							✓
Moderate	✓					✓	
High							
Very High		✓					
Extreme							
Post development							
Class C Shrubland							

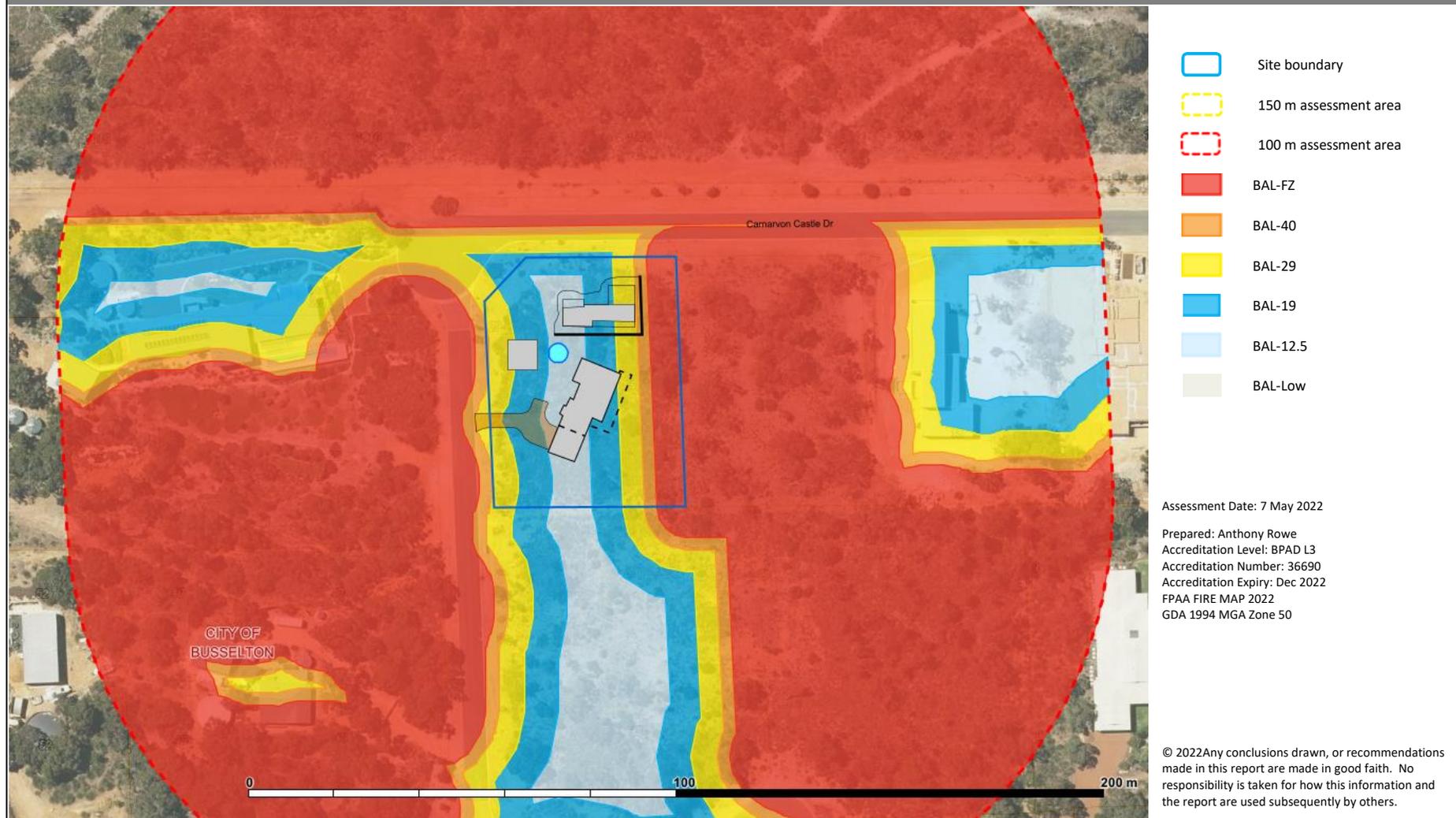
PLOT: 3							
Vegetation Classification			Vegetation Description (AS3959)				
Class D Scrub - Closed scrub D-13			<p>Found in wet areas and/or areas affected by poor soil fertility or shallow soils; &gt;30% foliage cover. Dry heaths occur in rocky or sandy areas. Shrubs &gt;2 m high. Typical of coastal areas and tall heaths up to 6 m in height. May be dominated by Banksia, Melaleuca or Leptospermum with heights of up to 6 metres</p>  <p>PHOTO ID: 3.1</p> <p>Trees taller than 6 m are less than 10%. The Plot is mostly shrubs up to 2 m and trees less than 6 m.</p>				
Slope							
Downslope 0-5							
Observation/Justification for classification							
Fuel Hazard	Surface	Near surface				Elevated	Bark
Low							
Moderate	✓						✓
High						✓	
Very High		✓					
Extreme							
Post development							
Unchanged Scrub							

PLOT: 3							
Vegetation Classification			Vegetation Description (AS3959)				
Class D Scrub - Closed scrub D-13			Found in wet areas and/or areas affected by poor soil fertility or shallow soils; >30% foliage cover. Dry heaths occur in rocky or sandy areas. Shrubs >2 m high. Typical of coastal areas and tall heaths up to 6 m in height. May be dominated by Banksia, Melaleuca or Leptospermum with heights of up to 6 metres				
Slope							
Downslope 0-5							
Observation/Justification for classification							
Fuel Hazard	Surface	Near surface				Elevated	Bark
Low							
Moderate	✓			✓			
High			✓				
Very High		✓					
Extreme							
Post development							
Unchanged – Photo 3.2 is part of a residential lot that when developed will displace the present vegetation and will become 'excluded' as a bushfire threat.							
			<b>PHOTO ID: 3.2</b> Trees taller than 6 m are less than 10%. The Plot is mostly shrubs up to 2 m and trees less than 6 m.				

PLOT: 4							
Vegetation Classification			Vegetation Description (AS3959)				
Class D Scrub - Closed scrub D-13			Found in wet areas and/or areas affected by poor soil fertility or shallow soils; >30% foliage cover. Dry heaths occur in rocky or sandy areas. Shrubs >2 m high. Typical of coastal areas and tall heaths up to 6 m in height. May be dominated by Banksia, Melaleuca or Leptospermum with heights of up to 6 metres				
Slope							
Upslope							
Observation/Justification for classification							
Fuel Hazard	Surface	Near surface				Elevated	Bark
Low							
Moderate	✓			✓			
High			✓				
Very High		✓					
Extreme							
Post development							
Unchanged							
			<b>PHOTO ID: 4.1</b> Trees taller than 6 m are less than 10%. The Plot is mostly shrubs up to 2 m and trees less than 6 m.				

PLOT: 4							
Vegetation Classification			Vegetation Description (AS3959)				
Class D Scrub - Closed scrub D-13			Found in wet areas and/or areas affected by poor soil fertility or shallow soils; >30% foliage cover. Dry heaths occur in rocky or sandy areas. Shrubs >2 m high. Typical of coastal areas and tall heaths up to 6 m in height. May be dominated by Banksia, Melaleuca or Leptospermum with heights of up to 6 metres				
Slope							
Flat							
Observation/Justification for classification							
Fuel Hazard	Surface	Near surface				Elevated	Bark
Low	✓						
Moderate							✓
High						✓	
Very High		✓					
Extreme							
Post development							
Unchanged							
			PHOTO ID: 4.2				

Figure 5: BAL Contour - Completion APZ





### 3.2 Bushfire Attack Level (Outputs) Method 1

#### Determined BAL – Present condition

The determined Bushfire Attack Level represent the present condition at the site in relation to the proposed development without bushfire protection measures.

**Table: 2.1**

Plot no.	Vegetation Classification	Effective slope	Separation m	BAL
Ex	Excluded	NA	NA	NA
1	Shrubland	Flat	0	BAL - FZ
2	Shrubland	Downslope 0-5	14.4	BAL – 29
3	Scrub	Downslope 0-5	36	BAL – 19
5	Scrub	Flat/Upslope	26	BAL – 19

#### Indicative BAL -BAL at completion

The Indicative Bushfire Attack Level (highest BAL) for the site / proposed development is based upon the conditions and classified vegetation at the time of completion.

In this instance the building has been placed on the site to achieve BAL 29, as illustrated in Figure 5, accounting for the condition of the vegetation on adjoining land, outside of the owners control. Note the land to the east has been classified as Shrubland in the indicative indication, but will become excluded at sometime in the future when the lot is developed for its intended purpose as a dwelling.

Indicative Bushfire Attack Level	BAL
Detached Dwelling (habitable building)	BAL - 29



## 4. IDENTIFICATION OF BUSHFIRE HAZARD ISSUES

### Bushfire Behaviour

Sustainable fire combustion depends upon the availability of fuel, oxygen, and heat. Removal of any one of the three aspects will extinguish or not sustain a fire. Fuel management, the management of vegetation, is the most practical means of control.

Bushfire behaviour, as it increases in intensity and speed of travel, can exceed human control measures and when this occurs the risk increases to humans and property. Bushfire behaviour is a result of climate, topography, and the availability of bushfire fuel (vegetation).

- Climate (drought and season) & weather (temperature, humidity, wind, atmospheric instability).

#### Wind

Bushfires are influenced by the wind direction and the speed. The wind direction generally determines the direction of the fire and wind speed, along with ground slope, generally determines the speed a fire will travel over ground. As wind strength increases it increases the availability of oxygen allowing the fire intensity to increase.

Atmospheric conditions determine the potential for the uplift of embers and particles that can be distributed by the prevailing wind direction well ahead of the fire, up to 9 km, to create spot fires that can advance the location of the fire front.

#### Fire Danger Index FDI

FDI is an indicator of potential fire intensity and behaviour based upon weather conditions; temperature, humidity, and wind speed, together with climate measures, drought factor representing the dryness of the ground fuels.

The FDI is an indicator of the potential for house loss and fatalities.

The FDI is used as a basis for determining the required design performance of a building.

- Topography (slope of the ground, aspect) – fire travels faster uphill, and in some conditions may determine the direction of the fire. The landform can also channel and increase the windspeed at a locality and create turbulence. It is measured as 0.0° or in downslope increments of 5.0°.
- Vegetation (horizontal and vertical structure, flammability, mass, and availability). Measured as a vegetation classification, or an exclusion, in AS 3959 (Method 1). The arrangement of fuel has a greater effect upon the intensity of a fire than just its mass; its exposure to oxygen is referred to as its availability in a bushfire.

It is assumed that a bushfire will achieve a steady-state and be fully developed to maximum intensity over a 100 m (minimum) fire run. Grass fires travel faster (GFDI) than a forest canopy fire, but a forest canopy fire can eject a higher level of embers and also eject them over a greater distance, up to 5 km.

Crown fires occur when the ground fire is intense, and conversely, when ground fuels are managed, the resultant fire intensity may not be sufficient to involve the crown or sustain a fire.

Fuel reduction initiatives such as slashing and controlled burns are intended to reduce the fuel availability to a level where the intensity of the fire remains controllable.



**Climate**

The nearest weather station to the site is at Cape Naturaliste (16 km north of the site). The site is within an area described as having a Mediterranean climate of dry summers and mild, wet winters. The majority of rainfall is between May and September. The prohibited burning period is from mid-December to mid-March.

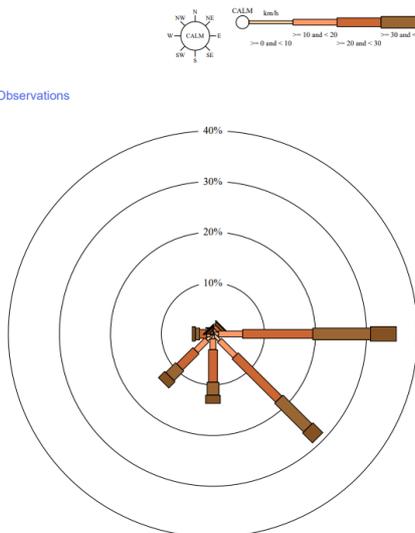
The Bushfire Danger Season has traditionally been between November and April each year, but recent climatic conditions have caused fire danger conditions to be present either side of this period.

Severe bushfire conditions FDI 50+, occur mostly between January and March. Extreme and Catastrophic conditions occur mostly in the afternoon and typically with south-south easterly winds (BoM Cape Naturaliste). A bushfire can however come from any direction.

Bushfires generally travel in the direction of the prevailing wind. The direction of the prevailing wind conditions can affect the options for evacuation and anticipated fire intensity depending upon the slope and fuel.

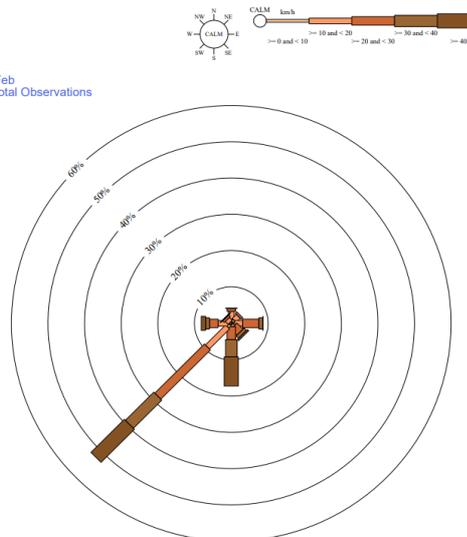
The wind roses below (Plates 5 and 6) for February (averaged) recorded at 9 am and 3 pm. illustrate the winds are strongest and most frequent from the south-east and east in the morning. Morning wind directions are predominantly from the southeast with a higher easterly component. Afternoon winds are generally stronger and of higher frequency from the south, with a higher westerly component.

9 am Feb  
 1829 Total Observations  
 Calm \*



*Plate 6: BoM weather data, prevailing wind directions as at 9 am*

3 pm Feb  
 1830 Total Observations  
 Calm \*



*Plate 7: BoM weather data, prevailing wind directions as at 3 pm*

**Site Context**



*Plate 8: Illustrates the surrounding area within 2.5 k m is predominantly pasture with Scrub Forest north of the site.*

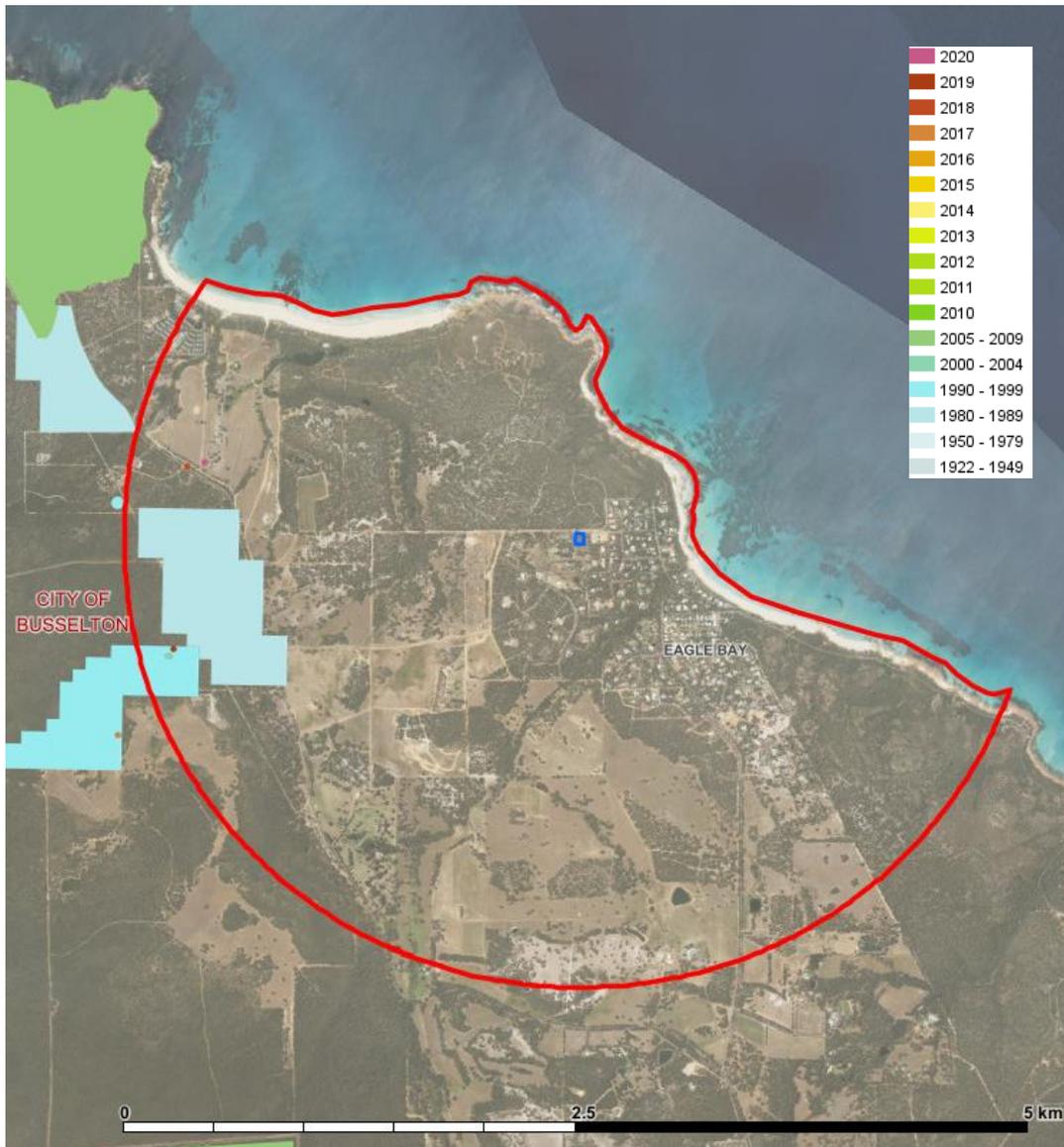


Plate 9: DBCA (indicative) landscape fire history

Plate 8; when combined with Plates 6 and 7, suggests a fire that is most likely to affect the site, will arrive from grassland, leading upslope to the western boundary (from the south west), or through fragmented forest, east and south east of the site. The extending forest could subject the site to a dispersed ember attack from a distant fire, but a concentrated ember attack can occur from the firefront at the extent of the APZ. Ember attack is responsible for the destruction of most houses, either igniting adjacent flammable materials or penetrating gaps > 2mm to reach flammable materials within a building.

The landscape fire history (Plate 9) suggests fires have not been prevalent in the area; this can be due to land management, proximity of firefighting resources, and early identification. The potential for a landscape fire however is simply a reflection of the coincidence of the availability of bushfire



fuels, climatic conditions and an ignition source. Despite the fire history in the locality the building has a likelihood to be affected that is classed as 'likely'.<sup>1</sup>

The proposed APZ, to BAL 29, will separate the dwelling from a direct flame contact and the construction to BAL 29, is commensurate to providing resistance from radiant heat and ember attack.

The dwelling will be required to be constructed to BAL-29 standard in AS 3959:2018 (at s.3 and s.7).

The construction is not a guarantee of building survival. Factors that can undermine its resistance are poor workmanship, not fully compliant with BAL 29 and leaving gaps > 2 mm at the exterior, poor maintenance and the distortion of materials resulting in gaps > 2 mm at the exterior, and a failure to maintain the APZ and the storage or attachment of flammable materials near or on the building that may expose it to a direct flame

The site does not have access to a reticulated water supply nor are hydrants available to the site. A standalone 10,000 L water supply dedicated for emergency services is to be provided as DFES is restricted from taking water from domestic tanks.

## 5. BUSHFIRE PROTECTION MEASURES

### 5.1 Guidelines for Planning in Bushfire Prone Areas Version 1.4 (the Guidelines)

The Guidelines apply to development applications located within designated bushfire prone areas. The Guidelines provide supporting information for implementation of SPP 3.7. Specifically, they provide the Bushfire Protection Criteria to be addressed for all applications.

The bushfire risk mitigation strategies detailed in (Table 2 below) are designed to comply with the Bushfire Protection Criteria detailed in Guidelines for Planning in Bushfire Prone Areas Version 1.4 (the Guidelines) Appendix 4 (WAPC, 2021).

Where discrepancy occurs between State and Local bushfire planning provisions, the higher standard of mitigation has been selected.

---

<sup>1</sup> NERAG page 39 – 'likelihood' categories



**Table 2: Bushfire Protection Criteria assessment.**

For each of the elements listed within Appendix 4 of the Guidelines for Planning in bushfire-prone areas, the ‘intent’ must be achieved either by the proposal meeting the acceptable solutions; or where these acceptable solutions cannot be fully met, then by a performance-based solution that can achieve the ‘intent.’

The acceptable solutions do not apply to all stages of development. Where these items are not applicable, they have been identified. Commentary may however be made but it is not a determinant.

✓	Acceptable solution provided	C	An Acceptable Solution to be conditioned
N/A	Not Applicable	P	Performance Principle solution see 5.2

Bushfire Protection Criteria	Method of Compliance	AS	PP	Proposed Bushfire Management Strategies
<b>Element 1: Location</b>				
<b>Intent:</b> To ensure that strategic planning proposals, subdivision and development applications are located in areas with the least possible risk of bushfire to facilitate the protection of people, property and infrastructure				
P1 The strategic planning proposal, subdivision and development application is located in an area where the bushfire hazard assessment is or will, on completion, be moderate or low, or a BAL-29 or below, and the risk can be managed. For unavoidable development in areas where BAL-40 or BAL-FZ applies, demonstrating that the risk can be managed to the satisfaction of the decision-maker	<b>A1.1 Development location</b> The strategic planning proposal, subdivision, and development application is located in an area that is or will, on completion, be subject to either a moderate or low bushfire hazard level, or BAL-29 or below.	✓		The proposal is a development application and can achieve BAL-29 at completion through the management of land within the site.



Bushfire Protection Criteria	Method of Compliance	AS	PP	Proposed Bushfire Management Strategies
<b>Element 2: Siting and Design of Development</b>				
<b>Intent:</b> To ensure that the siting and design of development minimises the level of bushfire impact				
<p><b>P2</b> The siting and design of the strategic planning proposal, subdivision or development application, including roads, paths and landscaping, is appropriate to the level of bushfire threat that applies to the site. The proposal incorporates a defensible space and significantly reduces the heat intensities at the building surface thereby minimising the bushfire risk to people, property and infrastructure, including compliance with AS 3959 if appropriate.</p>	<p><b>A2.1 Asset Protection Zone</b></p> <p>Every habitable building is surrounded by, and every proposed lot can achieve, an APZ depicted on submitted plans, which meets the following requirements:</p> <ul style="list-style-type: none"> <li>• Width: Measured from any external wall or supporting post or column of the proposed building, and of sufficient size to ensure the potential radiant heat impact of a bushfire does not exceed 29kW/m<sup>2</sup> (BAL-29) in all circumstances.</li> <li>• Location: the APZ should be contained solely within the boundaries of the lot on which the building is situated, except in instances where the neighbouring lot or lots will be managed in a low-fuel state on an ongoing basis, in perpetuity (see explanatory notes).</li> <li>• Management: the APZ is managed in accordance with the requirements of 'Standards for Asset Protection Zones.' (see Schedule 1).</li> </ul>	✓		<p>The Acceptable solution A2.1 requires that the development site can achieve on completion a BAL not exceeding BAL-29, by an APZ within the boundaries of the lot.</p> <p>The site has sufficient area to achieve an Asset Protection Zone that has sufficient dimension to achieve BAL 29 at the detached dwelling.(Figure 5 )</p>



Bushfire Protection Criteria	Method of Compliance	AS	PP	Proposed Bushfire Management Strategies
<b>Element 3: Vehicular Access</b>				
<b>Intent:</b> To ensure that the vehicular access serving a subdivision/development is available and safe during a bushfire event.				
<b>Note:</b> <i>only certain provisions apply to a development application for a single dwelling.</i>				
P3i The design and capacity of vehicular access and egress is to provide for the community to evacuate to a suitable destination before a bushfire arrives at the site, allowing emergency services personnel to attend the site and/or hazard vegetation.	<b>A3.1 Public roads</b>	N/A		A3.1 does not apply to a development application for a single dwelling.
	<b>A3.2a Multiple access routes</b> Public road access is to be provided in two different directions to at least two different suitable destinations with an all-weather surface (two-way access). If the public road access to the subject site is via a no-through road which cannot be avoided due to demonstrated site constraints, the road access is to be a maximum of 200 metres from the subject lot(s) boundary to an intersection where two-way access is provided. The no-through road may exceed 200 metres if it is demonstrated that an alternative access, including an emergency access way, cannot be provided due to site constraints and the following requirements are met: the no-through road travels towards a suitable destination; and the balance of the no-through road, that is greater than 200 metres from the subject site, is wholly within BAL-LOW, or is within a residential built-out area – Figure 23.	N/A		A3.2a does not apply to a development application for a single dwelling.
	<b>A3.2b Emergency access way</b>	N/A		A3.2b does not apply to a development application for a single dwelling.



Bushfire Protection Criteria	Method of Compliance	AS	PP	Proposed Bushfire Management Strategies
	<b>A3.3 Through-roads</b>	N/A		A3.3 applies to a subdivision but does not apply to a development application for a single dwelling.
<b>P3ii</b> The design of vehicular access and egress provides: <ul style="list-style-type: none"> <li>• access and egress for emergency service vehicles while allowing the community to evacuate;</li> <li>• a defensible space for emergency services personnel on the interface between classified vegetation and development site; and</li> <li>• hazard separation between classified vegetation and the subject site to reduce the potential radiant heat that may impact a lot(s).</li> </ul>	<b>A3.4a Perimeter roads</b>	N/A		A3.4a applies to a subdivision but does not apply to a development application for a single dwelling.
	<b>A3.4b Fire service access route</b>	N/A		A3.4b applies to a subdivision but does not apply to a development application for a single dwelling.
	<b>A3.5 Battle-axe access legs</b>	NA		The site has direct frontage to Carnarvon Castle Drive.



Bushfire Protection Criteria	Method of Compliance	AS	PP	Proposed Bushfire Management Strategies
<p><b>P3iv</b> Vehicular access is provided which allows emergency service vehicles to directly access all habitable buildings and water supplies and exit the lot without entrapment.</p>	<p><b>A3.6 Private driveways</b></p> <p>There are no private driveway technical requirements where the private driveway is:</p> <ul style="list-style-type: none"> <li>• within a lot serviced by reticulated water;</li> <li>• no greater than 70 metres in length between the most distant external part of the development site and the public road measured as a hose lay; and</li> <li>• accessed by a public road where the road speed limit is not greater than 70 km/h.</li> </ul> <p>In circumstances where all of the above conditions are not met, or the private driveway is in a non-reticulated water area, the private driveway is to meet all the following require:</p> <ul style="list-style-type: none"> <li>• requirements in Table 6, Column 4;</li> <li>• passing bays every 200 metres with a minimum length of 20 metres and a minimum additional trafficable width of two metres (i.e. the combined trafficable width of the passing bay and constructed private driveway to be a minimum six metres); and</li> <li>• turn-around area as shown in Figure 28 and within 30 metres of the habitable building.</li> </ul>	<p>N/A</p>		<p>The dwelling and private driveway is less than 70 m from a public road. The requirements for a private driveway in Table 6, Column 4, are not applicable.</p>



Bushfire Protection Criteria	Method of Compliance	AS	PP	Proposed Bushfire Management Strategies
<b>Element 4: Water</b>				
<b>Intent:</b> To ensure that water is available to the subdivision, development or land use to enable people, property and infrastructure to be defended from bushfire				
<p><b>P4</b> Provide a permanent water supply that is: – sufficient and available for firefighting purposes; – constructed from non-combustible materials (e.g. steel), or able to maintain its integrity throughout a bushfire; and – accessible, with legal access for maintenance and re-filling by tankers and emergency service vehicles.</p>	<p><b>A4.2 Provision of water for firefighting purposes</b></p> <p>Where a reticulated water supply is existing or proposed, hydrant connection(s) should be provided in accordance with the specifications of the relevant water supply authority. Where these specifications cannot be met, then the following applies:</p> <p>The provision of a water tank(s), in accordance with the requirements of Schedule 2; and</p> <p>Where the provision of a strategic water tank(s) is applicable, then the following requirements apply:</p> <ul style="list-style-type: none"> <li>– land to be ceded free of cost to the local government for the placement of the tank(s);</li> <li>– the lot or road reserve where the tank is to be located is identified on the plan of subdivision;</li> <li>– tank capacity, construction, and fittings, provided in accordance with the requirements of Schedule 2; and</li> <li>– a strategic water tank is to be located no more than 10 minutes from the subject site (at legal road speeds).</li> </ul> <p>Where a subdivision includes an existing habitable building(s) that is to be retained, a water supply should be provided to this existing habitable building(s), in accordance with the requirements listed above.</p>	✓		<p>The site has access to a reticulated water supply.</p> <p>A hydrant is located within east and within 50 m of the site on Carnarvon Castle Drive.</p>



## 5.2 Bushfire Management Strategies

No further bushfire management strategies to those addressed under the bushfire protection criteria are required in this instance.

## 5.3 Spatial representation of the bushfire management strategies

Further to the assessment against the bushfire protection criteria, the key features demonstrating compliance should be represented spatially in the *Spatial representation of the bushfire management strategies*. It represents the required bushfire risk management measures that must be implemented and maintained.

The Spatial representation of the bushfire management strategies is provided in Figure EX1.

## 6. RESPONSIBILITIES FOR IMPLEMENTATION AND MANAGEMENT OF THE BUSHFIRE MEASURES

The responsibilities for implementation and management of the bushfire measures, summarises the measures identified to achieve compliance with the bushfire protection measures following SPP 3.7. This has been provided in the Executive Summary. The details contained within the planning application authorised by the responsible decision maker are enforceable under section 214 of the *Planning and Development Act 2005*. The items addressed in the table responsibilities for implementation and management of the bushfire measures form part of the planning authorisation and where there is conflict supersede the detail of the planning application.

The responsibilities assigned to the City reflect the current activities of the City and are not to be relied upon nor are they binding upon the City as a consequence of this Bushfire Management Plan.

1. The site (29 Carnarvon Castle Drive Eagle Bay) is to be maintained in compliance with the Asset Protection Zone standard describe in Element 2, Schedule 1 Standards for Asset Protection Zones (Guidelines Planning in Bushfire Prone Areas v1.4).

*Note: The Asset Protection Zone standard is not baren land but a distribution of vegetation and is a default landscape management plan.*

2. The dwelling to be constructed to BAL-29, commensurate to the BAL rating (following the maintenance of the site in APZ standard). The BAL-29 construction requirements are described in s. 3 and 7 in AS 3959:2018.

*The building should be checked annually to ensure it remains in compliance with BAL-29 construction standard.*

3. Fire hoses are provided at the dwelling capable of applying water to all external surfaces of the dwelling and for a distance of 10 m around the dwelling. The hoses are to comply with AS/NZS1221 and be shielded from radiant heat.

*The purpose is a facility to prepare the site for an oncoming fire and to attend to any small fires after the passing of the firefront.*

### Advisory notes

1. The landowner acknowledges the accumulation of flammable material against the building or the placement of flammable material against or near the building, where its ignition would expose the building to direct flame contact, would exceed the design performance of the



building to resist bushfire attack, and could lead to significant damage or the loss of the building.

2. The landowner is responsible for availing themselves of any promotions and information to assist owners in preparing for and responding to a bushfire event as may be made by the City or the Department Fire and Emergency Services
3. Where there is conflict between this Bushfire Management Plan and the City Firebreak Notice the higher level of bushfire protection will prevail

**The City of Busselton (existing controls)**

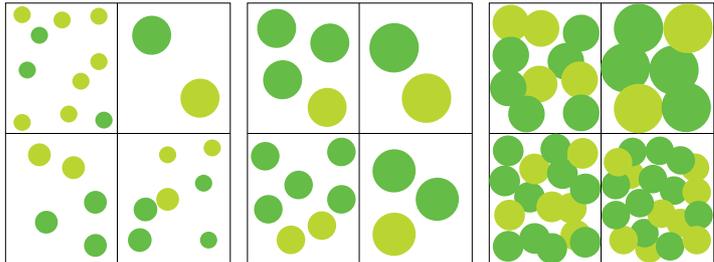
- |   |         |
|---|---------|
| 1. Developing and maintaining district bushfire fighting services and facilities.   | Ongoing |
| 2. Administer the <i>Bushfire Act 1954</i> and monitor landowner compliance to maintain land in a state not conducive to the ignition or spread of bushfire. This is a statutory responsibility assigned to local government. | Ongoing |
| 3. Promoting education and awareness of bushfire prevention and preparation measures through the community.   | Ongoing |
| 4. Administering the requirements of the <i>Planning and Development Act 2005</i> and the <i>Building Act 2011</i> .  | Ongoing |



## APPENDIX 1 - APZ Guidelines

**ELEMENT 2: SITING AND DESIGN OF DEVELOPMENT**

**SCHEDULE 1: STANDARDS FOR ASSET PROTECTION ZONES**

OBJECT	REQUIREMENT
Fences within the APZ	<ul style="list-style-type: none"> <li>Should be constructed from non-combustible materials (for example, iron, brick, limestone, metal post and wire, or bushfire-resisting timber referenced in Appendix F of AS 3959).</li> </ul>
Fine fuel load (Combustible, dead vegetation matter <6 millimetres in thickness)	<ul style="list-style-type: none"> <li>Should be managed and removed on a regular basis to maintain a low threat state.</li> <li>Should be maintained at &lt;2 tonnes per hectare (on average).</li> <li>Mulches should be non-combustible such as stone, gravel or crushed mineral earth or wood mulch &gt;6 millimetres in thickness.</li> </ul>
Trees* (>6 metres in height)	<ul style="list-style-type: none"> <li>Trunks at maturity should be a minimum distance of six metres from all elevations of the building.</li> <li>Branches at maturity should not touch or overhang a building or powerline.</li> <li>Lower branches and loose bark should be removed to a height of two metres above the ground and/or surface vegetation.</li> <li>Canopy cover within the APZ should be &lt;15 per cent of the total APZ area.</li> <li>Tree canopies at maturity should be at least five metres apart to avoid forming a continuous canopy. Stands of existing mature trees with interlocking canopies may be treated as an individual canopy provided that the total canopy cover within the APZ will not exceed 15 per cent and are not connected to the tree canopy outside the APZ.</li> </ul> <p data-bbox="564 1283 1134 1346"><b>Figure 19:</b> Tree canopy cover – ranging from 15 to 70 per cent at maturity</p> 
Shrub* and scrub* (0.5 metres to six metres in height). Shrub and scrub >6 metres in height are to be treated as trees.	<ul style="list-style-type: none"> <li>Should not be located under trees or within three metres of buildings.</li> <li>Should not be planted in clumps &gt;5 square metres in area.</li> <li>Clumps should be separated from each other and any exposed window or door by at least 10 metres.</li> </ul>
Ground covers* (<0.5 metres in height. Ground covers >0.5 metres in height are to be treated as shrubs)	<ul style="list-style-type: none"> <li>Can be planted under trees but must be maintained to remove dead plant material, as prescribed in 'Fine fuel load' above.</li> <li>Can be located within two metres of a structure, but three metres from windows or doors if &gt;100 millimetres in height.</li> </ul>



## ELEMENT 2: SITING AND DESIGN OF DEVELOPMENT

### SCHEDULE 1: STANDARDS FOR ASSET PROTECTION ZONES

OBJECT	REQUIREMENT
Grass	<ul style="list-style-type: none"> <li>• Grass should be maintained at a height of 100 millimetres or less, at all times.</li> <li>• Wherever possible, perennial grasses should be used and well-hydrated with regular application of wetting agents and efficient irrigation.</li> </ul>
Defendable space	<ul style="list-style-type: none"> <li>• Within three metres of each wall or supporting post of a habitable building, the area is kept free from vegetation, but can include ground covers, grass and non-combustible mulches as prescribed above.</li> </ul>
LP Gas Cylinders	<ul style="list-style-type: none"> <li>• Should be located on the side of a building furthest from the likely direction of a bushfire or on the side of a building where surrounding classified vegetation is upslope, at least one metre from vulnerable parts of a building.</li> <li>• The pressure relief valve should point away from the house.</li> <li>• No flammable material within six metres from the front of the valve.</li> <li>• Must sit on a firm, level and non-combustible base and be secured to a solid structure.</li> </ul>

\* Plant flammability, landscaping design and maintenance should be considered – refer to explanatory notes



## APPENDIX 2 –City Firebreak Notice

**PERMITS TO BURN**

Permits to Burn are required for the whole of the Restricted Burning Times and can only be obtained from the Fire Control Officer for your area

A list of Fire Control Officers is available on the **Fire and Emergency Services** page on the City's website

Most of our Fire Control Officers are volunteers, make sure you **plan ahead** if you intend to apply for a Permit to Burn

A Permit to Burn must be obtained before any burning takes place and the permit holder **must be in possession** of the permit throughout the duration of the burn

The permit holder shall ensure **all conditions** of the permit, as shown on the permit, are **fully complied with**

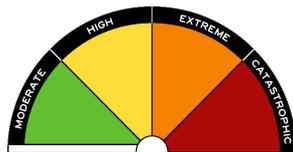
**NOTIFY DFES COMMUNICATIONS CENTRE**

DFES Communications Centre (9395 9209) are to be advised whenever the holder of a permit **commences burning**; and/or whenever a person is carrying out burning activities **outside of the Prohibited and Restricted Burning Times**

**HARVEST VEHICLE MOVEMENT BANS**

The City may declare **Harvest Vehicle Movement Bans (HVMBs)** at any time. For further information or to receive notification of HVMBs please visit the **Fire and Emergency Services** page on the City's website

**The new Australian Fire Danger Rating System**



Moderate: plan and prepare

High: be ready to act

Extreme: take action now to protect life and property

Catastrophic: for your survival, leave bushfire risk areas

**GENERAL INFORMATION**

**Burning of Garden Refuse:** pursuant to section 24G(2) of the *Bush Fires Act 1954*, the burning of garden refuse is prohibited throughout the District during Prohibited Burning Times, and prohibited in urban areas of the District during Restricted Burning Times

During Restricted Burning Times, a Permit to Burn is required for the burning of garden refuse in Rural Residential or Rural areas

**Camping and/or Cooking Fires:** pursuant to section 25(1a) of the *Bush Fires Act 1954*, the lighting of fires in the open for the purpose of camping and/or cooking is prohibited throughout the District during Prohibited Burning Times

The lighting of fires in the open for the purpose of camping and/or cooking is prohibited at all times when the **Fire Danger Rating** for the District is **\*High or above** without the written approval of the City

**Fire Pits, Chimineas, and/or Braziers:** the lighting of fire pits, Chimineas and/or braziers is prohibited during Prohibited Burning Times, and otherwise prohibited if the **Fire Danger Rating** for the District is **\*High or above**

**Conditions for the Lighting and Extinguishing of Fires in the Open:** when burning garden refuse; or lighting camping and/or cooking fires; or when lighting fire pits, Chimineas and/or braziers the space of ground around the site of the fire, having a radius of at least 3 metres from the site at the centre, shall be clear of all vegetation and other flammable materials

The person who lit the fire, or a person left in attendance at the fire as the case may be, shall completely extinguish the fire by the application of water and/or earth before that person leaves the site unattended

*\*The new Australian Fire Danger Rating System (AFDRS) will go live on 1 September 2022. The Australia wide system uses current science that significantly improves the ability to predict fire behaviour*

*Work is underway at a State level to amend legislation to reflect the new AFDRS. Importantly, these changes will not affect the requirement for landowners or occupiers of land to fully comply with this Notice*

For more information about the AFDRS visit

[www.afac.com.au/initiative/afdrs](http://www.afac.com.au/initiative/afdrs)

**KEY DATES**

Dates **may change due to seasonal fire conditions** in which case details will be published in local newspapers and on the City's website

**PROHIBITED BURNING TIME**  
(BURNING IN THE OPEN IS PROHIBITED)

1 December 2022 to 28 February 2023

**RESTRICTED BURNING TIMES**  
(BURNING PERMITS ARE REQUIRED)

15 October 2022 to 30 November 2022  
and 1 March 2023 to 30 April 2023  
(Burning on Public Holidays Prohibited)

**COMPLIANCE DATES**

**Rural Residential / Urban / Industrial Land**

Compliance with this Notice must be achieved no later than **15 November 2022** and maintained until **30 April 2023**

**Rural Land**

Compliance with this Notice must be achieved no later than **15 December 2022** and maintained until **30 April 2023**

**FIREBREAK INSPECTIONS AND RIGHT OF ENTRY**

The City will commence its annual firebreak inspection program on **15 November 2022**

Rangers are appointed Bush Fire Control Officers under the provisions of the *Bush Fires Act 1954* and carry out annual inspections

**Under the provisions of the Act, Bush Fire Control Officers may in the performance of their duties, enter any land or building including private property**

**FIREBREAK VARIATIONS**

Where there are valid environmental and/or on-ground considerations that prevent full compliance with this Notice, landowners may apply to the City for a firebreak variation. A variation must be lodged in writing on an Application for Variation form that is available on the City's website.

Applications for variation must be submitted by **31 October 2022**



**FIREBREAK AND FUEL HAZARD REDUCTION NOTICE**

**2022/2023 BUSH FIRE SEASON**

**FIRST AND FINAL NOTICE**

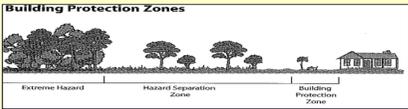
Take notice that pursuant to Part 3 Division 6 Section 33 of the *Bush Fires Act 1954*, landowner(s) or occupier(s) of land shall construct firebreaks and carry out fire prevention work in accordance with this Notice

Failure to comply with this Notice may result in a fine of up to

**\$5,000**

**This year the City will be prioritising the inspection of properties that were issued Work Orders during the 2021/2022 bush fire season**

Should you require assistance or clarification of the requirements of this Notice, please contact the City's Ranger Services on 9781 0444

<p style="text-align: center;"><b>CATEGORY</b></p> <p><i>It is the land owner's responsibility to identify the category that relates to their property and to ensure the necessary fire prevention works are completed on time. Please contact the City if you are unsure of your category.</i></p>	A	B	C	D	<p style="text-align: center;"><b>FIREBREAK CATEGORY CODE AND SUMMARY OF REQUIREMENTS</b></p> <p style="text-align: center;"><b>ALL REQUIREMENTS IN THIS NOTICE ARE TO BE MAINTAINED THROUGHOUT THE ENTIRE DURATION OF THE FIRE SEASON</b></p> <p style="text-align: center;"><b>FAILURE TO COMPLY MAY RESULT IN A \$5,000 FINE</b></p> <p style="text-align: center;">PLEASE BE ADVISED THAT YOUR PROPERTY MUST COMPLY WITH CATEGORY REQUIREMENTS AS NOTED BY A TICK IN COLUMN A, B, C OR D</p>
<p><b>CATEGORY 1</b></p> <p><b>RURAL</b></p> <p>Except plantations and vineyards (for tourist chalets, refer to Estate Fire Management Plan or Individual Fire Management Plan) Sections A, C and D apply to this category.</p>	✓		✓	✓	  <p><b>A - Firebreak</b> – The term firebreak includes a mineral earth firebreak. A mineral earth firebreak means a 3 metre wide area of the owner(s)/occupiers(s) land, cleared and maintained totally clear of all vegetation material (living or dead) so there is only mineral earth left. Any overhanging trees and other vegetation must be pruned to a height of 5 metres above the ground level of a mineral earth firebreak.</p> <p><b>Category 1 – Rural:</b> A mineral earth FIREBREAK shall be constructed 3 metres wide, except in pasture or crop areas where a FIREBREAK shall be 2 metres wide. FIREBREAKS shall be located adjacent to all external boundaries of the land. Where the land area exceeds 120 hectares, an additional FIREBREAK must divide the land into areas of not more than 120 hectares with each part completely surrounded by a FIREBREAK.</p> <p><b>Category 2 - Urban Residential and Industrial-Commercial:</b> Where the area of land exceeds 2024m<sup>2</sup> (½ acre) a mineral earth FIREBREAK shall be constructed and maintained at least 3 metres wide and within 6 metres of the inside of all external boundaries of the land. Where the area of land is 2024m<sup>2</sup> (½ acre) or less, hazardous material must be removed in accordance with section B - Fuel Reduction (refer to B1).</p> <p><b>Category 5 - Protea Plantations/Vineyards:</b> A mineral earth FIREBREAK shall be 3 metres wide. A low fuel area is to be maintained in accordance with section B - Fuel Reduction (refer to B2).</p> <p><b>Category 6 and 7 - Rural Residential:</b> A mineral earth FIREBREAK shall be constructed 3 metres wide. On Category 6 Rural Residential land with pasture or crop, a FIREBREAK shall be 2 metres wide and located within 6 metres of all external boundaries of the land. For Category 7 Rural Residential land, free access along a Strategic FIREBREAK is to be maintained at all times and including across the boundary of a lot, by means of a 3.5 metres wide field gate in the adjoining lot boundary fence.</p> <p><b>B - Fuel Reduction</b></p> <p>1) <b>Category 2 - Urban Residential and Industrial-Commercial:</b> Where the area of land is 2024m<sup>2</sup> (½ acre) or less, ALL HAZARDOUS MATERIAL must be removed from the whole of the land except living trees. In the area remaining, vegetation is to be maintained to a height of no greater than 10 centimetres; this includes piles of timber, branches and other vegetation. Trees shall be pruned in accordance with section E – Interpretation and Additional Requirements (refer to E1).</p> <p>2) <b>Category 5 - Protea Plantations/Vineyards:</b> A 5 metre low fuel area is to be maintained between the 3 metre FIREBREAK and the plantation/vineyard area. In this area, vegetation is to be maintained to a height of no greater than 10 centimetres; this includes piles of timber, branches and other vegetation.</p> <p>3) <b>Category 6, 7 and 8 - Rural Residential:</b> Parkland clearing must be carried out in all open paddocks and along the boundary of the property. Clearing means that all dead vegetation and dry grasses (excluding approved crops, pasture areas and living trees/shrubs) including piles of timber and disused materials must be maintained to a height of no greater than 10 centimetres.</p> <p><b>C - Building Protection Zones (BPZ)</b> – This is a modified area of reduced fuel immediately surrounding a building</p> <p>BPZ's starve the fire by reducing the fuel levels around your house. These requirements are designed to reduce the fire's intensity and minimise the likelihood of flame contact with buildings. The BPZ gives more protection to families should a fire threaten suddenly and they cannot leave. It also provides extra protection for fire fighters and property owners who may decide to stay with their property.</p> <p>A BPZ shall be provided for buildings in bush fire prone areas. The surroundings of buildings must comply with the following requirements:</p> <ol style="list-style-type: none"> <li>The BPZ for existing buildings must be at least 20 metres from any external wall of the building unless varied under an approved Fire Management Plan (FMP) in accordance with section E - Interpretation and Additional Requirements (refer to E4).</li> <li>The minimum BPZ for buildings constructed after 1 November 2011, in all cases shall be 25 metres.</li> <li>The BPZ must be located within the boundary of the lot that the building is situated on.</li> <li>Hazardous/flammable materials must not exceed the maximum fuel load specified in Point 5 below with grass areas not exceeding a height greater than 10 cm.</li> <li>Fuel loads must be reduced and maintained at 2 tonne per hectare.</li> <li>Isolated trees and shrubs may be retained, however, the first 5 metres around all buildings is to be clear of all hazardous/flammable materials.</li> <li>Reticulated gardens in the BPZ shall be maintained to a height of no greater than 500 millimetres.</li> <li>Wood piles must be at least 10 metres away from habitable dwellings.</li> <li>Trees in the BPZ must comply with section E - Interpretation and Additional Requirements (refer to E1).</li> <li>Where the land has an approved FMP, compliance must be achieved in accordance with the FMP. The FMP may vary the above BPZ requirements.</li> <li>A Hazard Separation Zone (HSZ) is also recommended in the absence of a Fire Management Plan. Section E - Interpretation and Additional Requirements (refer to E3).</li> </ol> <p><b>D – Fuel Storage &amp; Haystack Protection Zones</b></p> <p>A 3 metre mineral earth FIREBREAK shall be located within 6 metres of fuel storage tanks, sheds, gas cylinders and haystacks. The mineral earth firebreak shall be maintained so that it is totally clear of all material (living or dead).</p> <p><b>E – Interpretation and Additional Requirements</b></p> <p>1) <b>Trees</b> On Urban, Industrial, Rural, and Rural Residential land, all tree branches must be removed or pruned to ensure a clear separation of at least 3 metres back from the eaves of all buildings and 5 metres above the top of the roof. Branches that may fall on the house must also be removed. In the BPZ the following is 'recommended': the spacing of individual or groups of trees should be 15 metres apart to provide for a 5 metres separation between tree crowns. There is also a requirement of 2.5 metres between trees and power lines so they do not come into contact and start a fire or bring down a power line.</p> <p>2) <b>Hazardous and Flammable Materials</b> means the accumulation of fuel including burn piles (living or dead) such as leaf litter, twigs, trash, bush, dead trees and scrub capable of carrying a running fire, but excludes standing living trees and isolated shrubs. NOTE: All remaining vegetation, piles of timber, branches and other living vegetation must be maintained to a height of no greater than 10 centimetres. To measure and determine fuel loads use DFES's Visual Fuel Load Guide at <a href="http://www.dfes.wa.gov.au/safevinformation/fire/bushfire/pages/publications.aspx#5">http://www.dfes.wa.gov.au/safevinformation/fire/bushfire/pages/publications.aspx#5</a> and select Visual Fuel Load Guide Swan Coastal (Part 1 &amp; 2). Surface bush fire fuels should be kept low to the ground.</p> <p>3) <b>Hazard Separation Zones (HSZ)</b> A HSZ is a modified area of reduced fuel load outside of the BPZ and is recommended to assist in reducing the fires intensity when flames are approaching buildings. Both the BPZ and the HSZ are essential strategies for the protection of buildings. A HSZ covers the area 75 metres outside the BPZ. The HSZ should be modified to have a maximum fuel load of 6-8 tonne per hectare. This can be implemented by fuel reduction methods such as burning, mowing and slashing to remove the hazard. This should not require the removal of living trees or shrubs. REMEMBER: reduce the fuel level of the fire to lower the intensity of the blaze. Further information on fuel loading can be found in the Visual Fuel Load Guide available by calling DFES or via their website at <a href="http://www.dfes.wa.gov.au">www.dfes.wa.gov.au</a></p> <p>4) <b>Fire Management Plan (FMP)</b> A FMP is a comprehensive plan for the prevention and control of bushfires which may apply to individual land holdings. A notification, pursuant to the Transfer of Land Act 1893 (as amended) may be placed on the Certificate(s) of Title of the land for medium to long term fire management to reduce the occurrence and minimise the impact of uncontrolled bush fires, thereby reducing the threat to life, property and the environment. The land owner must comply with the FMP. Building in bush fire prone areas, new dwellings and other forms of accommodation, as well as additions to existing buildings are to be constructed in accordance with Australian Standard 3959-2009. In designated bush fire prone areas, the minimum BPZ in all cases shall be 25 metres. Further information on this and other information relating to fire safety issues can be found on the City's website <a href="http://www.busselton.wa.gov.au">www.busselton.wa.gov.au</a></p> 
<p><b>CATEGORY 2</b></p> <p><b>URBAN RESIDENTIAL &amp; INDUSTRIAL-COMMERCIAL</b></p> <p>Sections A, B, D and E1 Trees, apply to this category. Refer to section E - Interpretation and Additional Requirements (E1 Trees).</p>	✓	✓	✓		
<p><b>CATEGORY 3 &amp; 4</b></p> <p><b>PLANTATIONS</b></p> <p>Fire Management Plan applies</p>	N/A	N/A	N/A	N/A	
<p><b>CATEGORY 5</b></p> <p><b>PROTEA PLANTATIONS / VINEYARDS</b></p> <p>(For tourist chalets, refer to Estate Fire Management Plan or Individual Fire Management Plan) Sections A, B, C and D apply to this category.</p>	✓	✓	✓	✓	
<p><b>CATEGORY 6</b></p> <p><b>RURAL RESIDENTIAL - LOTS WITH INDIVIDUAL (MINERAL EARTH) BOUNDARY BREAKS</b></p> <p>Sections A, B, C and D apply to this category unless the property is subject to Estate Fire Management Plan or Individual Fire Management Plan</p>	✓	✓	✓	✓	
<p><b>CATEGORY 7</b></p> <p><b>RURAL RESIDENTIAL - LOTS WITH A STRATEGIC FIREBREAK ON ONE OR MORE BOUNDARIES</b></p> <p>Sections A, B, C and D apply to this category unless the property is subject to Estate Fire Management Plan or Individual Fire Management Plan</p>	✓	✓	✓	✓	
<p><b>CATEGORY 8</b></p> <p><b>RURAL RESIDENTIAL - LOTS WITH A STRATEGIC FIREBREAK AREA WITH NO STRATEGIC FIREBREAKS ON THE LOT BOUNDARIES</b></p> <p>Sections B, C and D apply to this category unless the property is subject to Estate Fire Management Plan or Individual Fire Management Plan</p>		✓	✓	✓	



## APPENDIX 3 – References



## **GENERAL REFERENCES**

SA Department of Environment and Natural Resources, Government of South Australia, 2012 *Overall Fuel Hazard Guide for South Australia*

Standards Australia, *Australian Standard AS 3745:2010 Planning for emergencies in facilities*

Standards Australia, *AS 3959:2018 Construction of buildings in bushfire-prone areas*, Sydney

Western Australian Planning Commission (WAPC) *Position Statement: Planning in bushfire prone areas – Demonstrating Element 1: Location and Element 2: Siting and design* November 2019

WA Department of Planning Land and Heritage 2016, *Visual Guide for bushfire risk assessment in Western Australia*

Western Australian Planning Commission (WAPC) 2015, *State Planning Policy 3.7 Planning in Bushfire Prone Areas*, Western Australian Planning Commission, Perth, Perth

Western Australian Planning Commission and Department of Fire and Emergency Services (WAPC) 2017, *Guidelines for Planning in Bushfire Prone Areas 2021 Version 1.4*, Western Australia

## **Online references**

Office of Bushfire Risk management (OBRM), Map of Bush Fire Prone Areas,  
< <https://maps.slip.wa.gov.au/landgate/bushfireprone/> >

**TOWN PLANNING ASSESSMENT – BED AND BREAKFAST (CHANGE OF USE OF EXISTING SINGLE HOUSE AND NEW DEVELOPMENT) - LOT 301 (NO. 29) CARNARVON CASTLE DRIVE, EAGLE BAY**

**Details: Outline of Development Application**

Local Planning Scheme	City of Busselton Local Planning Scheme No. 21
Local Planning Scheme Zoning	Residential R2.5
Structure Plan/Precinct Plan	Eagle Bay Structure Plan;  Development Guide Plan for Lot 50 Eagle Crescent and Portion Location 1348 Carnarvon Castle Drive, Eagle Bay.
Use Class and permissibility:	Single Dwelling- 'P' permitted use;  Bed and Breakfast- 'D' discretionary use.
Land Use Definitions	Single House:  'A dwelling standing wholly on its own green title or survey strata lot, together with any easement over adjoining land for support of a wall or for access or services and excludes dwellings on titles with areas held in common property'.  Bed and Breakfast:  'means a dwelling, used by a resident of the dwelling, to provide accommodation for persons away from their normal place of residence on a short-term commercial basis and includes the provision of breakfast'.
Lot Size:	2588m <sup>2</sup>
Existing Land Use:	Holiday Home (Single House) four (4) people.
State Heritage Register	No
Local Heritage	<input checked="" type="checkbox"/> N/A  <input type="checkbox"/> Heritage List  <input type="checkbox"/> Heritage Area
Bushfire Prone Area	Yes

**The proposal**

The application proposes the Bed and Breakfast (Change of use of Existing Single House and New Development) - Lot 301 (No. 29) Carnarvon Castle Drive, Eagle Bay (the Subject Site). This includes a Single House and incidental development including an outbuilding, water tank, and Ancillary Dwelling use of the previously approved House on the site currently under construction.

**Background**

The Council is asked to consider a development application for a proposed ‘Bed and Breakfast (Change of use of Existing Single House and New Development).

A summary of the background is provided as follows:

Gazettal of on 7 June 2019.	Scheme amendment AMD21/0029 was gazetted. This amendment rezoned Lots 301-304 Carnarvon Castle Drive and Lots 305-310 Vixen Close Eagle Bay from Rural Residential to Residential R2.5.
21 May 2021	Approval granted for Holiday Home (Single House) 4 People under DA21/0250.
4 July 2022	Approval granted for modifications to Holiday Home (Single House) 4 People under DA21/0250.01.

Key information includes for the application includes:

1. **Landowner:** Teena and Lee Smith
2. **Applicant:** MNB Residential
3. **General description of the site:**  
 Lot 301 (No. 29) Carnarvon Castle Drive (‘the Site’) is a corner lot on 90 degree bend of Carnarvon Castle Drive. The site is zoned ‘Residential’ under the Scheme with a density coding of R2.5. The adjoining properties to the south and east are similarly zoned ‘Residential’ with a density of coding of R2.5, while the properties to the west on the opposite side of Carnarvon Castle Drive are zoned ‘Rural Residential’.
4. **Current development/use:** Holiday Home (Single House) 4 People
5. **Applicable zoning and special control area designations:**
  - Residential R2.5 zoning;
  - Eagle Bay Special Character Area; and
  - Landscape Value special control area.
6. **Land use Permissibility:**
  - Single House ‘P’-permitted;
  - Bed and Breakfast ‘D’-discretionary.
7. **The proposal seeks development approval for following discretions:**
  1. The Subject Site is within the Landscape Value special control area in which all development requires development approval;
  2. The site is within the Eagle Bay special character area in which all development requires development approval;

3. Bed and Breakfast is a 'D'- discretionary use class which is not permitted unless the Local Government has exercised its discretion by granting development approval;
4. Change of use of existing Single House to Ancillary Dwelling with a plot ratio area of 82.7m<sup>2</sup> in lieu of the permitted 70m<sup>2</sup> under the deemed-to-comply standards of Residential Design Codes (R-Codes) clause 5.5.1;
5. The dwelling is proposed to reach a height of 8.45m, inclusive of a chimney height, in lieu of the Eagle Bay Structure Plan (the Structure Plan) development standard 8 and Development Guide Plan for Lot 50 Eagle Crescent and Portion Location 1348 Carnarvon Castle Drive, Eagle Bay (the DGP) development standard 4 permitted height of 7.5m above natural ground level;
6. An upper storey floor area covering 89% of the ground floor building footprint in lieu of 50% permitted by the DGP development standard 4 and Structure Plan development standard 8;
7. 1.1m high earthworks are proposed to support the slab in lieu of 0.5m permitted under Local Planning Policy 3.3 Eagle Bay Special Character Area (LPP 3.3) Element I Site Works is proposed;
8. An outbuilding wall height wall height of 3.34m is proposed in lieu of Residential Design Codes deemed-to-comply limit of 2.4m, Local Planning Scheme No. 21 wall height of 2.7m deemed to meet the Residential Design Codes design principles, and Local Planning Policy 4.10 Outbuildings and Other Non-Habitable Buildings 3.1m wall height limit deemed to meet the Residential Design Codes Design Principles.

### **Legislation and Policy**

#### Legislation

*Planning and Development Act 2005*

*Planning and Development (Local Planning Schemes) Regulations 2015*

*Local Planning Scheme No. 21*

#### State Government Policies

*State Planning Policy 7.3 Residential Design Codes*

*State Planning Policy 3.7 Planning in Bushfire Prone Areas*

#### Structure Plans/Development Guide Plans

*Eagle Bay Structure Plan*

*Development Guide Plan for Lot 50 Eagle Crescent and Portion Location 1348 Carnarvon Castle Drive, Eagle Bay*

#### Local Planning Policies

*Local Planning Policy 2.1 Car Parking*

*Local Planning Policy 3.1 Reflective Building Materials*

*Local Planning Policy 3.3 Eagle Bay Special Character Area*

*Local Planning Policy 4.2 Bushfire*

*Local Planning Policy 4.10 Outbuildings and Other Non-Habitable Buildings*

### **Consultation**

The application was advertised with letters mailed to surrounding landowners during the period commencing on 13 March 2023 and concluding on 29 March 2023. During this period, the City received two (2) submissions objecting to the proposal. Consultation is discussed in detail in Attachment 4.

The proposal has not been referred to external government or service agencies.

### **OFFICER COMMENT**

The Proposal will be assessed against each item of policy and legislation listed in this document. Accordingly, the officer comment will include the following subheadings:

- Local Planning Scheme No. 21 Assessment;
- State Planning Policy 7.3 Residential Design Codes Assessment;
- Residential Design Codes Design Principles Assessment
- State Planning Policy 3.7 Planning in Bushfire Prone Areas;
- State Planning Policy 3.7 Planning in Bushfire Prone Areas Table;
- Eagle Bay Structure Plan Assessment;
- Comment on Variation to the Structure Plan;
- Development Guide Plan for Lot 50 Eagle Crescent and Portion Location 1348 Carnarvon Castle Drive, Eagle Bay Assessment;
- Setback Assessment;
- Height Assessment;
- Comment on Building Height Discretion;
- Local Planning Policy 2.1 Car Parking Assessment;
- Local Planning Policy 3.1 Reflective Building Materials Assessment;
- Local Planning Policy 3.3 Eagle Bay Special Character Area Assessment;
- Local Planning Policy 4.10 Outbuildings and Other Non-Habitable Buildings Assessment;
- Assessment Against LPP 4.10 Objectives for the Assessment of Applications;
- Local Planning Policy 4.2 Bushfire Assessment;
- Local Planning Policy 6.1 Stormwater Management Assessment; and
- Conclusion

**Local Planning Scheme No. 21 Assessment**

<b>Provision</b>	<b>Requirement</b>	<b>Proposal</b>	<b>Assessment</b>
<b>Residential Zone Objectives</b>			
Objective (a)	To provide for a range of housing and a choice of residential densities to meet the needs of the community.	Single House, Change of Use (Ancillary Dwelling and Bed and Breakfast), Water Tank and Outbuilding.	<p>The proposal of single house is in keeping with this objective.</p> <p>The proposal will result in an Ancillary Dwelling standing on the site incidental to the main house. Although it is subject to the proposed Bed and Breakfast use, the option to rent out the Ancillary Dwelling or to house family members on a long term basis still exists. Therefore, it provides a boost to housing supply which is considered to be of community benefit.</p>
Objective (b)	To facilitate and encourage high quality design, built form and streetscapes throughout residential areas.	Bed and Breakfast (Change of Use (Ancillary Dwelling and Bed and Breakfast), Water Tank and Outbuilding.	The house is generally compliant with development standards concerned with providing a high quality streetscape. Refer to the specific discussion of each proposed variation and the acceptability of the resulting outcome in this document.
Objective (c)	To provide for a range of non-residential uses, which are compatible with and complementary to residential development.	Single House, Change of Use (Ancillary Dwelling and Bed and Breakfast), Water Tank and Outbuilding.	<p>The Bed and Breakfast use may only operate when residents are living on the site. This is a reduction in the intensity of the short-stay accommodation use on the site from the existing Holiday Home (Single House) approval on the site approved under DA21/0250.</p> <p>The Bed and Breakfast use is ancillary to the residential use of the site, as discussed above, and is therefore considered in keeping with Objective (c). This considers the Bed and Breakfast use cannot lawfully occur independently of the residential use of the site.</p>
<b>Clause 4.3.1 Modifications of the Residential Design Codes</b>	<b>Notwithstanding any other provision of the Scheme, the following variations and exclusions to the R-Codes apply:</b>		

4.3.1 (e)	All Special Character areas provided for in Schedule 4 and pursuant to clause 5.7 are excluded as necessary from the provisions of the R-Codes to the extent of any conflict between the standards set out in Schedule 4 and the R-Codes.	The Site is within the Eagle Bay Special Character Area.	Application is assessed against Schedule 4 and the special character area Local Planning Policy 3.3 Eagle Bay Special Character Area (LPP 3.3). These assessments are included later in this document.
4.3.1 (f)	Notwithstanding the deemed to comply provisions of the R-Codes, outbuildings that do not exceed a wall height of 2.7 metres and ridge height of 4.5 metres are deemed to meet the relevant performance criteria.	<b>A 3.34m outbuilding wall height is proposed.</b>	<b>This will assessed against the performance criteria of Local Planning Policy 4.10 Outbuildings and Other Non-Habitable Buildings (LPP 4.10) in the relevant section of this document.</b>
4.3.1 (g)	On land coded R2, R2.5 or R5, all building development is to be contained within a building envelope area which does not exceed 1,000m <sup>2</sup> , or as otherwise required by a plan adopted pursuant to Part 4 of the Deemed Provisions.	The site plan indicates that all proposed buildings are within the existing endorsed building envelope.	Complies. A building envelope has been endorsed through the Endorsed Building Envelope Plan associated with the DGP. All proposed buildings are contained within the designated building envelope.
<b>Clause 4.11 Water Supply</b>	Each dwelling shall be provided with a supply of potable water from a reticulated system, an underground bore or a rainwater storage system to the satisfaction of local government. Water tanks or other rainwater storage systems shall have a minimum capacity of	Dwelling will be connected to reticulated water.	Complies.

	135,000 litres and shall be directly connected to a suitable means of rainfall catchment having an area of not less than 150m <sup>2</sup> . All water tanks shall be fitted with couplings to the specification of the Department of Fire and Emergency Services.		
<b>Clause 4.12 Effluent Disposal</b>			
4.12.1	If no reticulated sewerage system is available, development approval shall not be granted for development in excess of a single house unless -	No sewer connection available to the site.	Refer to comments below against cl. 4.12.1 (a).
4.12.1 (a)	adequate and appropriate provision can be made for the on-site disposal of effluent, having particular regard to the comments or requirements specified by the Department of Health.	An Aerobic Treatment Unit (ATU) on site effluent disposal system is proposed to service the development.	Complies. This system will require approval by the health department at the building permit stage. This process will ensure the system provides adequate capacity and meets the relevant environmental health legislative standards.
4.12.2	If no reticulated sewerage system is available, liquid and/or solid wastes shall be disposed of on-site by means of an effluent disposal system approved by the local government. No house shall be occupied without the prior approval and installation of such a disposal system.	ATU proposed to service the site as mentioned in the comment above.	Complies, refer to comment above.
<b>Clause 4.18 Holiday Homes and Bed and Breakfast</b>			

4.18.2	The maximum number of rooms to be occupied by guests under the Bed and Breakfast use class is four (4) rooms for -		
4.18.2 (a)	Residential zoned lots 1,500m <sup>2</sup> or greater; or	A maximum of Four (4) guests are proposed to be accommodated by the Bed and Breakfast.	Complies. The lot is zoned 'Residential' and is greater than 1500m <sup>2</sup> .
<b>Clause 4.32 Clearing of Land in Rural Areas or Land Coded R2, R2.5 or R5</b>			
4.32.1	On land coded R2, R2.5 or R5 or located in the Rural Residential, Rural Landscape or Bushland Protection zones, vegetation may only be felled, removed or damaged when -	The lot is zoned Residential with a density coding of R2.5.	The proposal will be assessed the subclauses of 4.32 accordingly.
4.32.1 (a)	associated with implementing approved development;	Scrub vegetation will be removed to accommodate building footprint, driveway and Asset Protection Zone (APZ). Mature trees are to be retained as indicated on the site plan.	Complies. Vegetation removal is proposed within the proposed building footprints which this standard permits. All proposed buildings are proposed to be situated within the existing building envelope designated for the site.
4.32.1 (b)	essential for achieving adequate fire protection	All vegetation on the site will be managed to asset protection zone standards in accordance with the submitted Bushfire Management Plan (BMP).	Complies. The APZ is required to achieve adequate fire protection in accordance with State Planning Policy 3.7 Planning in Bushfire Prone Areas (SPP 3.7). This considers the BMP will facilitate the reduction of the initial determined Bushfire Attack Level (BAL)-Flame Zone (FZ) rating of the Single House to an acceptable BAL-29 rating in accordance with the standards of SPP 3.7 through the APZ establishment.  The entire site is currently required to be managed to APZ under the existing BMP endorsed through previous

			development approval DA21/0250 and as modified in DA21/0250.01.
<b>Clause 5.1 Operation of Special Control Areas</b>			
5.1.1	<p>The following special control areas are shown on the Scheme map:</p> <p>Development Investigation;</p> <p>Special Provision;</p> <p>Landscape Value;</p> <p>Wetland;</p> <p>Coastal Management;</p> <p>Special Character;</p> <p>Airport Protection;</p> <p>Port Geographe Development;</p> <p>Waste Water Exclusion;</p> <p>Waste Water Buffer;</p> <p>Floodway;</p> <p>Development Contribution; and</p> <p>Drive-Through Facility Control.</p>	The site is situated within the Landscape Value and Special Character special control areas.	Noted.

5.1.2	In respect of a special control area shown on a Scheme map, or otherwise set out in the Scheme, the provisions applying to the special control area apply in addition to the provisions applying to any underlying zone or reserve and any general provisions of the Scheme.	As per assessment comment.	Noted. The assessment considers the standards of the special control areas additional to the previous Scheme standards.
<b>Clause 5.4 Landscape Value Area</b>			
5.4.1	Development within the Landscape Value Area requires the prior development approval of the local government.	Application for development approval lodged.	Development approval is applied for through this development application, which is being considered by Council.
5.4.2	The local government shall not grant development approval for the clearing or development of any land identified within a Landscape Value area on the Scheme map, unless it has considered –		
5.4.2 (a)	whether the development will be compatible with the maintenance and enhancement, as far as is practicable, of the existing rural and scenic character of the locality;	Single House, Ancillary Dwelling and Bed and Breakfast use, and incidental buildings proposed.	<p>The proposal is considered to comply.</p> <p>This notes the land uses proposed are permitted in the case of the Single House and discretionary in the case of the Bed and Breakfast use.</p> <p>The proposed Bed and Breakfast use is a reduction in the intensity of short-stay accommodation use on the site from the existing approved Holiday Home (Single House) use as mentioned previously against Objective (c) of the Residential Zone previously in this document. The Bed and Breakfast use will replace the Holiday Home use.</p>

Built form development standards will be assessed and discussed in their respective sections of this document.

The entire site currently required to be managed to APZ standards under the existing BMP endorsed with previous approval DA21/0250 on the site. Other clearing is limited to the proposed building footprints and to establish the APZ prescribed by the BMP accompanying the current development application. Clearance of vegetation for building footprints and management of vegetation for bushfire purposes is common within the established character of the area. The aerial image provided below indicates this:



Removal of vegetation within building footprints and for establishing an APZ is permitted elsewhere in the Scheme under clause 4.32 (a) and (b) as previously discussed.

			<p>Built form development standards will be assessed and discussed in their respective sections of this document.</p> <p>The entire site currently required to be managed to APZ standards under the existing BMP endorsed with previous approval DA21/0250 on the site. Other clearing is limited to the proposed building footprints and to establish the APZ prescribed by the BMP accompanying the current development application. Clearance of vegetation for building footprints and management of vegetation for bushfire purposes is common within the established character of the area. The aerial image provided below indicates this:</p>  <p>Removal of vegetation within building footprints and for establishing an APZ is permitted elsewhere in the Scheme under clause 4.32 (a) and (b) as previously discussed.</p>
5.4.2 (b)	whether the development will materially affect any wildlife	Vegetation removal within proposed building footprints and	Considered to comply.

	<p>refuge, significant wetland, coastal environment or any identified site containing Aboriginal archaeological relics; and</p>	<p>for establishment of APZ is proposed.</p>	<p>Available data indicates no recognised aboriginal heritage sites are impacted by the proposal.</p> <p>The site is not within a designated wetland or coastal environment area.</p> <p>The vegetation removal and is proposed for the establishment of the APZ and the building footprints. This is deemed acceptable under clause 4.32 (a) and (b) of the Scheme as previously discussed. All buildings are contained within the existing endorsed building envelope. The site would become undevelopable if no allowance were given for vegetation removal for building footprints and for bushfire purposes in accordance with the requirements of SPP 3.7.</p> <p>It is noted the entire site is currently required to be managed to APZ standard as discussed against clause 5.4.2 (a) above.</p> <p>Mature trees on the site are proposed to be retained as indicated on the site plan in Attachment B which is considered a positive outcome under 5.4.2 (b) for preserving wildlife refuge.</p>
<p>5.4.2 (c)</p>	<p>disturbance to the natural environment, including –</p> <ul style="list-style-type: none"> <li>i. visual effects of clearing for development;</li> <li>ii. maintenance of rural character; and</li> </ul>	<ul style="list-style-type: none"> <li>i. Removal of scrub vegetation for building footprint and vegetation management on the site to asset protection zone standard.</li> <li>ii. Proposal includes permitted development of Single House</li> </ul>	<p>Compliant:</p> <ul style="list-style-type: none"> <li>i. Refer to above comment in response to 5.4.2 (b) and previous discussion in response to Clause 4.32 of the Scheme.</li> <li>ii. Single House is a permitted use in the Residential zone and is therefore compliant.</li> </ul>

	<p>iii. habitat disturbance.</p>	<p>use and discretion sought for Bed and Breakfast use.</p> <p>iii. Refer to (i). No other potential impacts to habitat.</p>	<p>The proposed Bed and Breakfast is discretionary use. The proposed Bed and Breakfast is compliant with all applicable development standards and therefore not considered to be in conflict with the character of the area under (ii). This also notes that, while falling within the Landscape Value special control area, the site sits within a low density residential area rather than a rural area with respect to character.</p> <p>iii. Refer to the response to (i), 5.4.2 (a), 5.4.2 (b), and clause 4.32 of the Scheme. Only permitted vegetation removal is proposed. No water courses or water bodies are impacted.</p>
5.4.3	<p>The local government shall not grant development approval for the carrying out of development on land within the Landscape Value area or on land on or near any ridgelines where, in the opinion of the local government, that development is likely to substantially detract from the visual amenity of the area, having regard to, among other things, the cumulative visual effect of the development related to other development that may be anticipated in the locality and in the area generally.</p>	<p>The Site sits on a slope with the building envelope area ranging from 51m to 49.2 Australian Height Datum (AHD).</p>	<p>Complies. The site is not at the top of a ridgeline given the landscape features a ridge height at 60m AHD.</p> <p>The lot was subdivided in 2009 with the zoning allowing for residential development within the endorsed building envelope area.</p>
5.4.4	<p>Before granting development approval for the erection of a building on land within the Landscape Value area, the local</p>		

	government shall make an assessment as to whether it should impose conditions relating to –		
5.4.4 (a)	the siting of the proposed building;	All development contained within endorsed building envelope.	Complies.
5.4.4 (b)	the use of prescribed materials on the external surfaces of the building; and	<p>‘Harvest’ colour brick walls are proposed. Harvest sample is shown below:</p>  <p>Colourbond material roof is proposed.</p> <p>Exterior colourbond cladding is to be in ‘Monument’ colour:</p>  <p>Monument™</p>	Complies. The proposed colours are non-reflective and comply with Local Planning Policy 3.1 Reflective Building Materials, as neither fall under the policy’s light or very light colour classification or designated non-permitted materials.
5.4.5	In clause 5.4.4 –  "external surfaces" means the external walls and cladding (if any),		Noted.

	<p>external doors, external door and window frames, columns, roofs, fences and any surface of a building or work visible from the exterior of a building or work; and</p> <p>"prescribed materials" means materials with dark tones or dark colouring and of low reflective quality or materials which are painted or similarly treated with dark toned or dark coloured paint or pigment of low reflective quality.</p>		
<b>Clause 5.7 Special Character Area</b>			
5.7.1	Development within the Special Character Area requires the prior development approval of the local government.	Development application lodged.	Development approval is sought through this development application.
5.7.2	The local government may devise development objectives, provisions and/or controls to reinforce, retain or change the characteristics, form and nature of a designated Special Character area, with such objectives, provisions and/or controls being specified in Schedule 4.	Assessed against development the given standards in the relevant sections of this document.	Noted.
5.7.3	Where such objectives, provisions and/or controls are specified in Schedule 4 in relation to a designated Special Character area, those objectives, provisions and/or	Assessed against the given development standards in the relevant sections of this document.	Noted.

	controls act in conjunction with the other provisions of this Scheme relevant to that area with the exception that the provisions of Schedule 4 will take precedence over any other provisions of this Scheme.		
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**State Planning Policy 7.3 Residential Design Codes Assessment**

Deemed to Comply Standard	Requirement	Proposal	Assessment
<b>Clause 5.1.4 Open Space</b>			
C4	<p>Open space provided in accordance with Table 1 (refer Figure Series 6). The site of the grouped dwelling, for the purpose of calculating the open space requirement, shall include the area allocated for the exclusive use of that dwelling and the proportionate share of any associated common property.</p> <p>Note: Table 1 requires 80% of the site meet the definition of open space.</p>	86% of the site is provided as open space.	Complies.
<b>Clause 5.2.2 Garage Width</b>			
C2.4	<p>A garage door and its supporting structures (or a garage wall where a garage is aligned parallel to the street) facing the primary street is not to occupy more than 50 per cent of the frontage at the setback line as viewed from the street (refer Figure 8c). This may be</p>	The garage occupies 27.13% of the primary street frontage width at the applicable front setback line.	Complies.

	increased up to 60 per cent where an upper floor or balcony extends for more than half the width of the garage and its supporting structures (or a garage wall where a garage is aligned parallel to the street) and the entrance to the dwelling is clearly visible from the primary street.		
<b>Clause 5.3 Street Surveillance</b>			
C3.1	Street elevation of the dwelling to address the street with clearly definable entry points visible and accessed from the street.	The front door is clearly visible and addresses the western Carnarvon Castle Drive frontage.	Complies.
C3.2	One major opening from a habitable room of the dwelling faces the street and pedestrian or vehicular approach to the dwelling.	Guest bed 2 and upper floor balcony address the street and vehicular access with line of sight over the front entry and vehicular access.	Complies.
<b>Clause 5.2.5 Sight Lines</b>			
C5	Walls, fences and other structures truncated or reduced to no higher than 0.75m within 1.5m of where walls, fences, or other structures adjoin:	No obstructions to required sightlines are proposed.	Complies.

	<ul style="list-style-type: none"> <li>i. a driveway that intersects a street, right-of-way or communal street;</li> <li>ii. a RoW or communal street that intersects a public street; and</li> <li>iii. two streets that intersect (refer Figure 9a).</li> </ul>		
<b>Clause 5.3.2 Landscaping</b>			
C2.2	<p>Landscaping of single houses, grouped dwellings and multiple dwellings to include the following:</p> <ul style="list-style-type: none"> <li>i. the minimum number of trees and associated planting areas in the table below; and</li> <li>ii. Landscaping of the street setback area, with not more than 50% of the area to consist of impervious surfaces.</li> </ul> <p>Note: under (i) the table requires a minimum of one (1) tree in a 2mx2m planting area.</p>	<ul style="list-style-type: none"> <li>i. Multiple native trees are being retained on the site.</li> <li>ii. 7.76% of the front setback area is proposed to be occupied by impervious surfaces.</li> </ul>	Complies.
<b>Clause 5.3.3 Car Parking</b>			

C3.1	Two (2) parking bays required for single House and one (1) additional parking bay required for the Ancillary Dwelling	Four (4) parking bays are provided.	Complies.
<b>Clause 5.3.5 Vehicular Access</b>			
C5.1	<p>Access to on-site car parking spaces to be provided:</p> <ul style="list-style-type: none"> <li>• From a communal street or RoW if available.</li> <li>• From a secondary street where no Communal Street or RoW exists.</li> <li>• From the primary street frontage where no secondary street, communal street or RoW exists.</li> </ul>	Access taken from western Carnarvon Castle Drive Street frontage, which the front door is proposed to address, in lieu of the of northern Carnarvon Castle Drive frontage.	<b>Variation. Access proposed through primary street frontage.</b>
C5.2	<p>Driveways to primary or secondary street provided as follows:</p> <ul style="list-style-type: none"> <li>• driveways serving four dwellings or less not narrower than 3m at the street boundary;</li> <li>• no driveway wider than 6m at the street boundary and driveways in</li> </ul>	The driveway is 3m wide at the boundary.	Complies.

	aggregate no greater than 9m for any one property.		
C5.3	<p>Driveways shall be:</p> <ul style="list-style-type: none"> <li>• no closer than 0.5m from a side lot boundary or street pole;</li> <li>• no closer than 6m to a street corner as required under AS2890.1 Parking Facilities: Off street Parking (as amended);</li> <li>• aligned at right angles to the street alignment;</li> <li>• located so as to avoid street trees, or, where this is unavoidable, the street trees replaced at the applicant's expense or re-planting arrangements to be approved by the decision-maker; and</li> <li>• adequately paved and drained.</li> </ul>	<ul style="list-style-type: none"> <li>• The driveway is setback greater than 0.5m from either side boundary.</li> <li>• The driveway is 25m from the nearest corner tangent point.</li> <li>• The driveway is aligned at a right angle to the street.</li> <li>• The driveway does not impact any street trees or any other public assets.</li> <li>• Drainage for the overall development will be addressed by a condition of approval requiring adequate management of storm water within the site at a rate of 1m<sup>3</sup> per 65m<sup>2</sup> impervious area in keeping with the City's Local Planning Policy 6.1 Stormwater Management.</li> </ul>	Complies with all points subject to conditions of approval.
<b>Clause 5.3.7 Site Works</b>			
C7.1	Retaining walls, fill and excavation between the street boundary and the street setback, not more than 0.5m	No fill or excavation is proposed between the street boundary and the street setback.	Complies.

	above or below the natural ground level, except where necessary to provide for pedestrian, universal and/or vehicle access, drainage works or natural light to a dwelling.		
C7.2	Retaining walls, fill and excavation within the site and behind the required street setback to comply with Table 4.		Refer to separate setback assessment within this document.
<b>Clause 5.3.9 Stormwater Management</b>			
C9	All water draining from roofs, driveways, communal streets and other impermeable surfaces shall be directed to garden areas, sumps or rainwater tanks within the development site where climatic and soil conditions allow for the effective retention of stormwater on-site.	Refer to compliance comment.	A recommended condition will require stormwater to be managed onsite in accordance with the City's Local Planning Policy 6.1 Stormwater Management.
<b>Clause 5.4.1</b>			
C1.1	Major openings and unenclosed outdoor active habitable spaces, which have a floor level of more than 0.5m above natural ground level and overlook any	All areas of the dwelling are setback greater than 7.5m to all boundaries with neighbouring properties.	Complies.

	<p>part of any other residential property behind its street setback line are:</p> <p>setback, in direct line of sight within the cone of vision, from the lot boundary, a minimum distance as prescribed in the table below (refer Figure Series 10):</p> <p>Major openings to bedrooms and studies 4.5m;</p> <p>Major openings to habitable rooms other than bedrooms and studies 6m; and</p> <p>Unenclosed outdoor active habitable spaces 7.5m.</p>		
<b>Clause 5.4.3 Outbuildings</b>			
C3	<p>The outbuilding addresses C3 (b) for large and multiple outbuildings which provides:</p>	<p>i. The proposed outbuilding is 49m<sup>2</sup>.</p> <p>ii. Refer to separate setback assessment within this document.</p>	<b>Variation to (iii).</b>

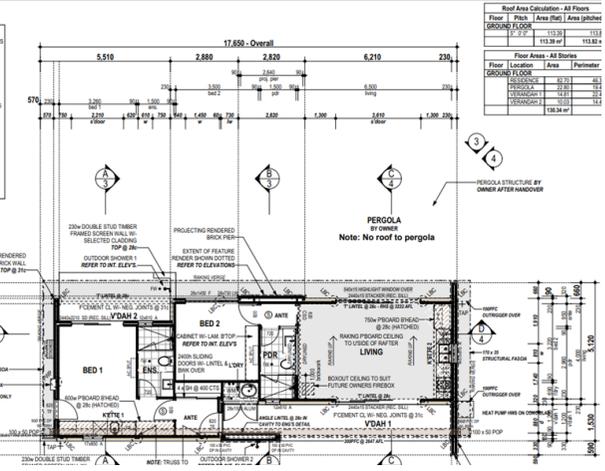
	<ul style="list-style-type: none"> <li>i. individually or collectively does not exceed 60m<sup>2</sup> in area or 10 per cent in aggregate of the site area, whichever is the lesser;</li> <li>ii. Setback in accordance with Table 2a;</li> <li>iii. Does not exceed a wall height of 2.4m;</li> <li>iv. Does not exceed a ridge height of 4.2m;</li> <li>v. Not located within the primary or secondary street setback area; and</li> <li>vi. Does not reduce the open and outdoor living area requirements in Table 1.</li> </ul>	<ul style="list-style-type: none"> <li>iii. A wall height of 3.34m is proposed.</li> <li>iv. A ridge height of 3.34m is proposed.</li> <li>v. The outbuilding is compliant with the applicable street setbacks. Refer to separate setback assessment in this document.</li> <li>vi. Proposal complies with open space requirement as noted against Clause 5.1.4. Outdoor living area requirements are not applicable to Lots with a density coding or R2.5.</li> </ul>	
<b>Clause 5.5.1 Ancillary Dwellings</b>			
C1	<p>Ancillary dwelling associated with a single house and on the same lot where:</p> <ul style="list-style-type: none"> <li>i. the lot is not less than 350m<sup>2</sup> in area;</li> <li>ii. there is a maximum plot ratio area of 70m<sup>2</sup>;</li> </ul>	<ul style="list-style-type: none"> <li>i. The lot is greater than 350m<sup>2</sup>.</li> <li>ii. Plot ratio area of 82.7m<sup>2</sup> is proposed.</li> <li>iii. A total of four (4) parking bays is proposed on the site.</li> <li>iv. The Ancillary Dwelling is subject only to a change of use and is existing. All</li> </ul>	<b>Variation to (ii) is proposed.</b>

	<p>iii. parking is provided in accordance with clause 5.3.3 C3.1;</p> <p>iv. ancillary dwelling is located behind the street setback line;</p> <p>v. ancillary dwelling is designed to be compatible with the colour, roof pitch and materials of the single house on the same lot;</p> <p>vi. ancillary dwelling does not preclude the single house from meeting the required minimum open space and outdoor living area; and</p> <p>vii. ancillary dwelling complies with all other R-Code provisions, only as they apply to single houses, with the exception of clauses:</p> <p>a) 5.1.1 Site area;</p> <p>b) 5.2.3 Street surveillance (except where located on a lot with secondary street or right-of-way access); and</p> <p>c) 5.3.1 Outdoor living areas.</p>	<p>setbacks for the Ancillary Dwelling are already approved.</p> <p>v. The Ancillary Dwelling is rendered brick and colourbond. The proposed Single House is brick and colourbond.</p> <p>vi. The proposal is compliant with the open space requirement of 80% under Clause 5.1.4 Open Space. The outdoor living area requirements are not applicable.</p> <p>vii. Compliance with points (a), (b) and (c) are achieved as discussed in the respective R-Code assessment for each respective clause.</p>	
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**Residential Design Codes Design Principles Assessment**

<b>Design Principle</b>	<b>Requirement</b>	<b>Proposal</b>	<b>Assessment</b>
Clause 5.3.5 P5	Vehicular access provided for each development site to provide:		
P5 bullet point 1	Vehicle access safety;	Access taken from primary street frontage in lieu of secondary frontage.	<p>Complies. The proposal is not considered to compromise vehicle access safety.</p> <p>Both street frontages provide access to the same road, being Carnarvon Castle Drive. Therefore, taking access from the primary street does not result in access being taken from a higher traffic volume road in lieu of the availability of a lower traffic volume road.</p> <p>The access is compliant with the deemed-to-comply sightline standards of R-Codes clause 5.2.5 and the deemed-to-comply standards of clause 5.3.5 C5.3 such as adequate setbacks to the nearest street intersection and to adjoining lot boundaries.</p>
P5 bullet point 2	Reduced impact of access points on the streetscape;	Access taken from primary street frontage in lieu of secondary frontage.	Complies. The Site has two frontages to the same road as mentioned above in the discussion against P5 bullet point 1. Therefore, if the crossover addressed the secondary street frontage, then it would still present to the same street.
P5 bullet point 3	Legible access;	Access taken from primary street frontage in lieu of secondary frontage.	Complies. The access is clearly visible and clearly associated with the subject Site at a glance.

P5 Bullet point 4	Pedestrian Safety	Access taken from primary street frontage in lieu of secondary frontage.	<p>Complies. There is not considered to be any increased impact to pedestrian safety compared to if access was taken from the northern Carnarvon Castle frontage. There is no footpath impacted by the crossover.</p> <p>The western street frontage is wider than the northern secondary frontage which will provide greater visibility of cars entering and exiting through the proposed access compared to the alternative on the northern street frontage.</p>
P5 bullet point 5	Minimal crossovers	Access taken from primary street frontage in lieu of secondary frontage.	Complies. A single crossover is proposed.
P5 bullet point 6	High quality landscaping features.	Access taken from primary street frontage in lieu of secondary frontage.	<p>Complies. The landscaping plan approved through DA21/0250 will ensure a portion of the Site's frontage to Carnarvon Castle Drive will be landscaped.</p> <p>This standard is intended to apply to smaller lots in urban areas with less vegetation present on the site. Existing vegetation will present towards much of the western Carnarvon Castle Drive frontage.</p>
<b>Clause 5.4.3 Outbuildings Design Principles</b>			The variations will be assessed against the LPP 4.10 Objectives for the Assessment of Applications. LPP 4.10

			<p>guides the City's application of discretions for outbuilding proposals.</p>
<p><b>Clause 5.5.1 Ancillary Dwellings Design Principles</b></p>			
<p>P1</p>	<p>Ancillary Dwelling is of a small scale and designed to support people living independently or semi-dependently to the residents of the single house, sharing some site facilities and services.</p>	<p>Ancillary dwelling plot ratio area of 82.7m<sup>2</sup> in lieu of 70m<sup>2</sup>.</p>	<p>The proposed ancillary plot ratio is 82.7m<sup>2</sup>, and the floor plan is shown below for reference:</p>  <p>The Ancillary Dwelling contains two (2) bedrooms and a single living area only. This is considered an acceptable scale in terms of the maximum scale of occupancy that can be accommodated. Two (2) bedroom ancillary dwellings are considered a commonplace scale of</p>

			<p>Ancillary dwelling occupancy both within the City area and throughout Western Australia.</p> <p>The total plot ratio area is bulked up by the inclusion of spaces such as a second bathroom and the 'ante' area. These spaces do not increase the maximum scale of the use of the Ancillary Dwelling.</p> <p>Further to the above, the total scale of dwelling and Ancillary Dwelling scale is considered acceptable. This considers that the proposed dwelling contains only three (3) bedrooms. Therefore, the total number of bedrooms on the site is proposed to be five (5) and contained within a combined total dwelling footprint of 323.7m<sup>2</sup>. The total floor area within the building footprint area of the Single House and Ancillary Dwelling including the garage, but excluding unenclosed outdoor living areas, reaches 431.23m<sup>2</sup>. The proposed scale of dwelling occupancy and building footprint on the site is consistent with the established character of the area, given the following examples:</p> <ul style="list-style-type: none"><li>• The southern adjoining property Lot 305 Vixen Close has been developed with a four (4) bedroom dwelling with a number of other large habitable rooms capable of bedroom use. The dwelling footprint is 383m<sup>2</sup> exclusive of outdoor</li></ul>
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			<p>living areas. This lot is still eligible to be developed with an Ancillary Dwelling if desired by the owners;</p> <ul style="list-style-type: none"> <li>• Lot 335 (No. 52) Carnarvon Castle Drive which is developed to its full development potential including a dwelling with a footprint of approximately 428m<sup>2</sup> exclusive of incidental development and unenclosed outdoor living areas and includes five (5) bedrooms. There is also an associated single bedroom 63m<sup>2</sup> Ancillary Accommodation on this site;</li> <li>• The approximate dwelling footprint at Lot 334 (No. 28) Carnarvon Castle Drive is 423m<sup>2</sup> with three (3) bedrooms plus a single bedroom ancillary dwelling that is two (2) storey and covering a building footprint of 51m<sup>2</sup>.</li> </ul> <p>The examples above demonstrate that the proposal consistent with the established character of the area with respect to dwelling footprint and occupancy scale.</p> <p>The Ancillary Dwelling is proposed to share site facilities such as the access, parking area and ATU system.</p>
P2	Ancillary Dwellings to positively contribute to its setting, including the existing single house and, where	Ancillary dwelling plot ratio area of 82.7m <sup>2</sup> in lieu of 70m <sup>2</sup> .	With respect to the built form outcome, the Ancillary Dwelling is already under construction as it was initially approved as a single dwelling.

	visible from the street or adjoining properties, to the amenity of the streetscape and context.		Despite the above, the Ancillary Dwelling provides visual interest and engagement with the northern streetscape through the pergola and outdoor living areas addressing the streetscape that are also backed by major openings. To the primary living area that they adjoin.
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**State Planning Policy 3.7 Planning in Bushfire Prone Areas Assessment**

The proposal is accompanied by a Bushfire Management Plan (BMP). The BMP has been produced by an appropriately accredited level 3 bushfire planning practitioner. The proposal requires a BMP due to the dwelling being subject to a determined Bushfire Attack Level (BAL) rating of Flame Zone (FZ). In summary, the BMP demonstrates the proposal will achieve compliance with the SPP 3.7 Guidelines, including the reduction of the BAL rating to an acceptable rating of BAL-29. The assessment against the Appendix Four Bushfire Protection Criteria of the SPP 3.7 Guidelines is included below.

**State Planning Policy 3.7 Assessment Table**

Provision	Requirement	Proposal	Assessment
<b>Element 1 Location</b>			
A1.1 Development Location	The strategic planning proposal, subdivision and development application is located in an area that is or will, on completion, be subject to either a moderate or low bushfire hazard level, or BAL-29 or below.	The proposal is subject to an initial BAL-FZ rating. The BMP proposes the implementation of an APZ that will result in a rating of BAL-29.	Complies. The BMP prescribes an APZ that will result in the achievement of BAL-29 for the site. The APZ will be required to be maintained in perpetuity to maintain the BAL-29 rating.
A2.1 Asset Protection Zone	<p>Every habitable building is surrounded by, and every proposed lot can achieve, an APZ depicted on submitted plans, which meets the following requirements:</p> <ul style="list-style-type: none"> <li>• Width: Measured from any external wall or supporting post or column of the proposed building, and of sufficient size to ensure the potential radiant heat impact of a bushfire does not exceed 29kW/m<sup>2</sup> (BAL-29) in all circumstances.</li> <li>• Location: the APZ should be contained solely within the boundaries of the lot on which the building is situated, except in instances where the</li> </ul>	<ul style="list-style-type: none"> <li>• The APZ width is prescribed width that is adequate to capture the vegetation responsible for the initial BAL-FZ rating.</li> <li>• The APZ is contained within the lot boundaries.</li> <li>• The BMP prescribes the APZ to be managed in accordance with 'Standards for Asset Protection Zones'.</li> </ul>	Complies with each of the three (3) points.

	<p>neighbouring lot or lots will be managed in a low-fuel state on an ongoing basis, in perpetuity (see explanatory notes).</p> <ul style="list-style-type: none"> <li>• Management: the APZ is managed in accordance with the requirements of 'Standards for Asset Protection Zones.</li> </ul>		
A4.2 Provision of Water for Firefighting Purposes	Where a reticulated water supply is existing or proposed, hydrant connection(s) should be provided in accordance with the specifications of the relevant water supply authority.	The site has access to a reticulated water supply. A hydrant is located within 50m of the site on Carnarvon Castle Drive.	Complies.

**Eagle Bay Structure Plan Assessment**

Provision	Requirement	Proposal	Assessment
<p>Planning Policy Statement 8</p>	<p>All buildings are restricted to a height of 7.5m above natural surface level with no more than 50% of the ground floor of the building to be two (2) storey development. Lots fronting Eagle Bay Road may require height limits of 4.5m to protect the tree canopy line.</p>	<p>The proposed Single House reaches a maximum building height of 7.87m above natural ground level.</p> <p>The upper floor covers 89% of the ground floor footprint in lieu of 50%. This considers 214m<sup>2</sup> roofed building area covered by the upper floor relative to the total ground floor building footprint area of 240m<sup>2</sup>. No definition of 'building' is provided for the application of the Structure Plan so unenclosed roofed outdoor living areas have been included.</p> <p>No definition of 'building' is provided for the application of the Structure Plan so all roofed areas inclusive of unenclosed roofed building areas of the Single House have been considered for the calculation.</p>	<p><b>Variation to 7.5m height limit and proportion of the footprint occupied by upper floor is proposed.</b></p> <p><b>The height limit variation is discussed in a separate section of this report.</b></p>

**Comment on Variations to the Eagle Bay Structure Plan**

The variation sought to Structure Plan Planning Policy Statement 8 is assessed against the relevant subclauses of clause 67 (2) of the Planning and Development (Local Planning Schemes) Regulations 2015 (the Regulations 2015).

Clause 67 (2) of the Regulations 2015	Comment
<p>m) the compatibility of the development with its setting, including —</p> <p>(i) the compatibility of the development with the desired future character of its setting; and</p> <p>(ii) the relationship of the development to development on adjoining land or on other land in the locality including, but not limited to, the likely effect of the height, bulk, scale, orientation and appearance of the development.</p>	<p>(i) The proposal is considered to comply. Structure Plan Planning Policy Statement 8 attempts to manage the scale of upper floor building bulk. The assessment of the impact will consider the enclosed building floor areas rather than roofed areas total for the purpose of discussing the true built form impact.</p> <p>The standard relies on proportions, not the absolute building floor area. As a result, the proposal includes a high proportion of second storey area relative to ground floor area. However the high proportion is predominantly due to what is considered a reasonably modest sized ground floor area for the context of the area. This considers the proposed ground floor area is 181.82m<sup>2</sup>. The upper floor enclosed building area is 162.8m. The immediate area is predominantly built-out and therefore the character is generally established. The proposal compares to other existing houses in in the area as follows:</p> <ul style="list-style-type: none"> <li>• Approximately 260m<sup>2</sup> upper floor area at Lot 19 (no. 30) Eagle Crescent;</li> <li>• Although compliant with Structure Plan Planning Policy Statement 8, a total of 169m<sup>2</sup> upper floor area was approved at Lot 303 (No. 15) Carnarvon Castle exists;</li> </ul>

	<ul style="list-style-type: none"> <li>• Lot 305 Vixen Drive (No. 35 Carnarvon Castle Drive) which includes a 318.7m<sup>2</sup> dwelling footprint area exclusive of unenclosed components with an upper floor component of 187m<sup>2</sup>; and</li> <li>• 52 Carnarvon Castle Drive which is developed with a dwelling with a footprint of approximately 428m<sup>2</sup> exclusive of incidental development and unenclosed outdoor living areas.</li> </ul> <p>The above examples are generally compliant given the standard is based on proportions. However, they are provide examples of the established pattern of development in the area for true scale of building bulk. The proposed dwelling is of less total building scale than the above examples with respect to both ground and upper floor areas.</p> <p>(ii) This point is addressed in the discussion against (i) above. The southern adjoining property is provided in the examples of existing houses in the area, being Lot 305 Vixen Close (No. 35 Carnarvon Castle Drive). The proposal is of a more modest scale with respect to the ground and upper floor areas. The eastern adjoining property is vacant and the owners provided no submission during the advertising period to raise concerns.</p>
<p>n) the amenity of the locality including the following —</p> <p>(ii) the character of the locality;</p>	<p>(i) The proposal is considered in character with the established character of the area with respect to the proposed variation to the permitted portion of the upper floor coverage in relation to ground floor. Refer to the examples of existing houses built in the area in the discussion against clause 67 (i).</p>



**Development Guide Plan for Lot 50 Eagle Crescent and Portion Location 1348 Carnarvon Castle Drive, Eagle Bay Assessment**

<b>Provision</b>	<b>Requirement</b>	<b>Proposal</b>	<b>Assessment</b>
Planning Policy Statement 4	All buildings are restricted to a height of 7.5m above natural surface level with no more than 50% of the ground floor of the building to be two (2) storey development.	The proposed Single House reaches a maximum building height of 7.87m above natural ground level.  The upper floor covers 89% as per calculation under Planning Policy Statement 8 of the Structure plan.	<b>Variation to 7.5m height limit and proportion of the footprint occupied by upper floor is proposed. This standard is a double up of Planning Policy Statement 8 of the Structure Plan. Refer to the assessment against the Regulations clause 67 (2) for the proportion of the upper floor exceeding 50% relative to the ground floor area.</b>  <b>The variation to the 7.5m height limit is assessed against clause 67 (2) of the Regulations 2015 in a later section of this report.</b>
Planning Policy Statement 5	No clearing of vegetation, including clearing for fire management purposes, will be permitted in the restricted building area.	Scrub removal is required in the restricted building area to accommodate the access.	There are no alternative access options that would avoid scrub removal. The City cannot refuse access to the site.  All vegetation on the site will be managed to asset protection zone standard in accordance with the BMP. This does not require tree removal in the building restriction area.
Planning Policy Statement 7	Effluent disposal to be handled via ATU systems appropriate to the locality, with effluent disposal areas located at least 50m away from watercourses.	An ATU system is proposed to service the development. It is not within 50m of a watercourse.	
Planning Policy Statement 16	Lots 1-4 (includes the lot the subject of this application as lot 1) are subject to building setbacks of 10 metres to the	These setbacks were incorporated into the endorsed building envelope.	The setbacks are assessed separately within this document.

	northern boundary of the lots and 5 metres to all other lot boundaries, except that any building on Lot 4 must be setback 15 metres from the eastern boundary of that lot.		
Planning Policy Statement 17	A single crossover point will be permitted to the new road, being the extension of Carnarvon Castle Drive, into location 1348.	A single crossover accessing Carnarvon Castle Drive is proposed.	

The setback assessment in this document is made on the basis that the Structure Plan and the Endorsed Building Envelope Plan provide the setback standards for the site. Refer to the discussion of the setbacks in the Officer Report to Council.

The setbacks assessed in the table below respects that all buildings must be contained within the building envelope provided by the Endorsed Building Envelope Plan.

The assessment notes that the battered earthworks extending to the South of the dwelling do not include a retaining wall and therefore are not classified as a building and are as a result are subject to R-Codes 5.3.7, not the building envelope setback. Similarly, the pergola fronting the northern Carnarvon Castle frontage does not fall within the classification of a building as is not subject to setback requirements.

**Setback Assessment**

<b>Boundary</b>	<b>Required</b>	<b>Proposed</b>	<b>Compliance</b>
<b>Dwelling:</b>			
Carnarvon Castle (west)	5m	13.5m	Complies
Side (south)	10m	10.32	Complies
Carnarvon Castle (north)	10m	22.7m	Complies
Rear (East)	5m	11.5m	Complies
<b>Outbuilding (shed)</b>			
Carnarvon Castle (west)	5m	5m	Complies
Carnarvon Castle (north)	10m	10m to the truncation and 19m to the northern boundary	Complies
Side (south)	10m	33m	Complies
Rear (east)	5m	32m	Complies
<b>Water Tank</b>			
Carnarvon Castle (west)	5m	13.5m	Complies
Carnarvon Castle (north)	10m	20m	Complies
Side (south)	10m	33.5m	Complies
Rear (east)	5m	26m	Complies
<b>Ancillary Dwelling/Bed and Breakfast</b>			

Carnarvon Castle Drive (west)	5m	17.65m	Complies
Carnarvon Castle Drive (north) including truncation boundary	10m	10.62m  Notes:  The pergola is not classified as a building and is not subject to setback standards.	Complies
Side (south)	10m	39.9m	Complies
Rear (east)	5m	10m	Complies
<b>Earthworks</b>			
Rear (south)	1m (R-Codes cl. 5.3.7)	8.62m	Complies

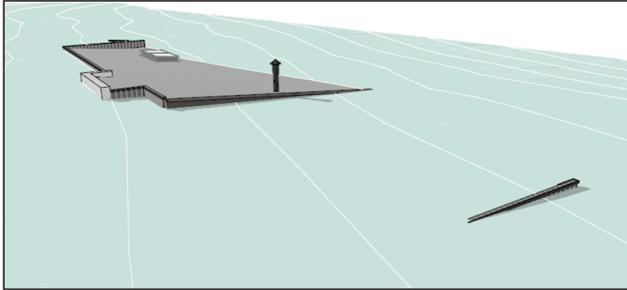
**Building Height Assessment**

Provision	Requirement	Proposal	Assessment
Structure Plan Planning Policy Statement 8	7.5m permitted building height.	8.45m. The Structure Plan does not exclude chimneys from the building height calculation under this provision.	<b>0.95m variation to permitted limit.</b>

**Comment on Building Height Discretion**

The proposed building height of 8.45m in lieu of the permitted is considered the Regulations 2015 clause 67 (2) subclauses relevant to the proposal.

Before providing detailed assessment, the nature of the variation itself can be further clarified. 8.45m height calculated includes the chimney. The chimney is only included due to a technicality. The technicality is that neither the Structure Plan nor DGP provide an exclusion of chimneys or similar projecting features from the height calculation. Standard height assessments throughout the City based on the Scheme exclude features such as chimneys under clause 4.8.4. The maximum height of the building excluding the chimney reaches 7.87m. This is a 0.37m variation to the 7.5m permitted height with respect to the built form.

<p>m) the compatibility of the development with its setting, including —</p> <ul style="list-style-type: none"> <li>(i) the compatibility of the development with the desired future character of its setting; and</li> <li>(ii) the relationship of the development to development on adjoining land or on other land in the locality including, but not limited to, the likely effect of the height, bulk, scale, orientation and appearance of the development.</li> </ul>	<p>(i) Considered to comply. The proposed building height is not expected to be visually perceptible in comparison to if compliance with the 7.5m height limit was achieved. The graphic below represents the scale of the dwelling exceeding the 7.5m height limit:</p> <div style="text-align: center;">  <p>EXTENT OF PARAPET WALLS &amp; ROOF BEYOND 7.5m HEIGHT PLANE</p> </div> <p>The left side of the graphic is the façade of the house that will address the western Carnarvon Castle Drive frontage. The floor level of the</p>
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	<p>proposed house sits greater than 37cm below the ground level of Carnarvon Castle Drive at the east of the site. This notes the house has a finished floor level of 50.50m Relative Level (RL) and Carnarvon Castle Drive sits at 51m RL in line with the eastern façade of the dwelling.</p> <p>(ii) As above, the scale of the proposed height variation is not expected to be visually perceptible in comparison to a compliant building height of 7.5m. Refer to the graphic provided in the discussion under (i).</p> <p>The height variation does not present to either the southern or eastern directly adjoining properties. The bottom of the graphic represents the southern end of the dwelling the right side the eastern side. As visually evident in the graphic the areas of the building presenting to these boundaries are predominantly compliant with the 7.5m height limit, and will obscure view of the non-compliant areas.</p>
<p>n) the amenity of the locality including the following —</p> <p>(ii) the character of the locality.</p>	<p>(iii) The area is characterised by large dwellings, of which a number have been approved with similar or greater variations to the 7.5m height limit. Examples include:</p> <ul style="list-style-type: none"> <li>• An 8.2m building height approved on the southern adjoining lot on the corner of Vixen Close and Carnarvon Castle. This was approved in similar circumstances with portions of the roof reaching 8.2m due to the earthworks in response to the slope of the site.</li> <li>• 8.6m maximum height was approved at Lot 304 (no. 11) Carnarvon Castle Drive.</li> </ul> <p>Following the above, the proposal is considered generally in keeping with the character established by similar approved and constructed dwellings on the same street. Excluding the chimney, the height proposed is lower than both of the above examples.</p>

	<p>The discussion against 67 (m) (i) is also regarded as a significant relevant consideration. As per this previous discussion, the variation is not expected to be visually perceptible in comparison to if the building was compliant with the 7.5m height limit.</p>
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**Local Planning Policy 2.1 Car Parking Assessment**

Provision	Requirement	Proposal	Assessment
Clause 4.6.1	Two (2) parking spaces required for the dwelling and one (1) parking space required to be provided for each Bed and Breakfast guest bedroom.	Four (4) parking spaces are provided on the site within the lot boundaries.	Complies. This notes two (2) guest bedrooms are proposed.

**Local Planning Policy 3.1 Reflective Building Materials Assessment**

Provision	Requirement	Proposal	Assessment
Clause 4.1	<p>The use of the following reflective materials is not permitted within the Landscape Value Area and the Travel Route Corridors as defined by the Scheme and these provisions, unless their use is approved in accordance with clause 3.1.2 of these provisions or it is used in a vertical installation of no greater than 18m<sup>2</sup> in cumulative area:</p> <ul style="list-style-type: none"> <li>• Zinalume® (including any zinc or Zinc-Aluminium coated sheet or polished metal),</li> <li>• Light and Very Light building materials</li> <li>• Stainless steel,</li> <li>• Copper (where sealed against oxidation),</li> </ul>	<p>Harvest’ brick is proposed for all walls. This is non-reflective, appearing as shown below:</p>  <p>The colourbond cladding on the roof and portions of the wall is proposed to be in monument colour.</p>	Compliant. ‘Harvest’ and ‘Monument’ is non-reflective. Bricks and colourbond are not prohibited materials.

	<ul style="list-style-type: none"> <li>• Glazed terra cotta tiles (irrespective of colour),</li> <li>• High Gloss painted finishes with a light reflectance and/or chroma value exceeding 50%,</li> <li>• Large expanses of tinted or clear glass or Perspex® (transparent and translucent acrylic).</li> </ul>		
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**Local Planning Policy 3.3 Eagle Bay Special Character Area Assessment**

Provision	Requirement	Proposal	Assessment
<b>Element A Setbacks</b>	Not applicable. The policy explicitly provides that the DGP takes precedence.	N/A	This is noted given that setbacks are an item in contention and the LPP 3.3 setbacks were not given regard in the setbacks assessment.
<b>Element E Single Dwelling Design</b>			
Bullet point 1.	Residential designs will be closely scrutinised at a planning and building assessment stage to determine whether the building is designed to be used as a Single Dwelling.	Single dwelling proposed.	Complies. There are no spaces proposed that appear to be capable of use used for a purpose other than residential use.
Bullet point 2	Designs that are determined by the City to represent a layout which could reasonably be utilised as a Grouped Dwelling or a Residential Building shall be determined as a use other than a	The Single dwelling is proposed to contain one (1) laundry and one (1) kitchen.	The single dwelling contains no areas that can be occupied separately and independently from the rest of the single dwelling. All areas of the dwelling would be dependent upon the single

	Single House pursuant to the Scheme and R Codes and assessed accordingly.		laundry and kitchen proposed in the floor plan.
<b>Element F Building Height</b>	The Policy explicitly provides that the DGP height takes precedence.		Noted.
Bullet point 1	Remnant vegetation is to be retained in any landscaping proposals including verges.	No landscaping plan provided.	Complies. No landscaping plan is required to be provided.
Bullet point 2	A site survey showing all existing vegetation and justification for the removal of any vegetation as part of a residential design shall accompany all applications for development.	The site is covered by scrub vegetation. Vegetation removal is only required for accommodation of building footprints and to establish the prescribed APZ.	Compliant. Scrub removal is proposed to accommodate building footprints within the building envelope and to establish an APZ prescribed in the BMP to achieve an acceptable BAL-29 rating for the single house. This is compliant with the relevant clauses of the Scheme discussed previously in this document.
Bullet point 3	Clearing of vegetation shall be limited to allow construction of approved development and a reasonable private garden area. Significant vegetation, particularly along property boundaries and in front and rear setback areas, should be retained where possible.	As above.	As above.
Bullet point 4	Where possible remnant vegetation is to be retained and used to reduce the visual mass/bulk of dwellings. Applications shall be accompanied by a site survey showing all existing vegetation and a justification for	Refer to proposal details discussed in response to bullet point 2.	Compliant as per response to bullet point 2.

	removal of any vegetation proposed to be removed.		
<b>Element H Vehicle Access Design</b>			
Bullet point 1	One crossover per dwelling with a maximum single vehicle width plus minimal transitions which can then widen to the required internal driveway width within the lot shall apply within the Area.	One (1) 3m wide crossover is proposed.	Complies.
<b>Element I Site Works</b>			
Bullet point 1	Development shall be carried out so as to minimise disruption to natural ground levels outside the perimeter of any building.	Earthworks in response to the slope of the site are proposed.	Policy intent noted.
Bullet point 2	Cut and fill for the purposes of slab on the ground construction shall not exceed 0.5m at any point.	At the highest point, fill supporting the single house slab reaches 1.1m above natural ground level.	<p><b>0.6m variation to acceptable development standard.</b></p> <p><b>The variation is considered acceptable.</b></p> <p><b>The slope falls away by approximately 1.1m over the length of the building footprint and site works are required in response.</b></p> <p><b>Earthworks with the use of banking without retaining is proposed to support the dwelling which reduces the visual impact of the earthworks.</b></p>

			<p><b>The street of Carnarvon Castle Drive to the West of the site sits between a near equal ground level and 1.9m above the finished level of the earthworks that present to the street at AHD 50.50m.</b></p> <p><b>The eastern side of the dwelling where the earthworks reach the maximum height of 0.6m above the ground level will not be visible to the street. This mitigates the potential for the earthworks to impact the streetscape.</b></p>
Bullet point 3	"Brick up" to slab construction or timber/steel frame stumps shall be the preferred method of development.	Slab on ground used, not 'brick up to slab' or steel frame stumps used.	The policy provides that 'brick up' and stumps are preferred, but this does not categorically provide that these techniques are required as a development standard.
Bullet point 4	No filling, excavation or retaining walls shall be constructed within the street setback area except where the proponent has submitted an Engineers Certificate stating that such is necessary for the prevention of erosion or to ensure site stability.	Past approval included the retaining wall section extending into the street setback area of the northern Carnarvon Castle Drive frontage. The retaining supports the existing Holiday Home (Single House) that is currently proposed to undergo change of use to Ancillary Dwelling with Bed and Breakfast use.	No alterations to the subject past approved retaining within the northern street setback area are proposed.

Bullet point 5	Retaining walls along front/road boundaries or within the street setback areas are discouraged but where necessary due to topography shall have a maximum height of 1.0 metre and be stepped should the need arise for large scale retaining. Materials should be local stone and/or timber and be complemented by landscaping using local species. All proposed retaining walls shall be shown in applications together with a written technical justification.	The retaining referred to above for bullet point 4 reaches a maximum height of 0.57m.	Complies. Technical requirements are dealt with through the building permit.
Bullet point 6	No fencing or other construction is permitted other than low, open 'rural-style' wire or ring-lock fences with wooden posts and star picket droppers within the front setback area of each lot unless specifically approved by the City of Busselton.		

<b>Local Planning Policy 4.10 Outbuildings and Other Non-Habitable Buildings Assessment</b>			
<b>Provision</b>	<b>Requirement</b>	<b>Proposal</b>	<b>Assessment</b>
<b>Clause 4.1 Outbuildings in the Residential or Centre Zones</b>			
4.1 (a)	All Outbuildings associated with Residential Development in the Residential or Centre Zones should		Refer to R-Code Assessment.

	comply with the deemed-to-comply criteria of R-Codes Part 5.4.3.		
4.1 (b)	<p>Further to 4.1 (a) above, in assessing an application for development approval where a discretion is sought to iii, iv and/or v of Part 5.4.3 of the R-codes (indicated in bold above) and the development meets the applicable provisions within Appendix 1:</p> <p>Outbuilding and other Non-habitable Building Standards of this Policy, the application will be deemed to meet the associated Design Principles of the R-codes and therefore development approval will be granted by the City.</p>	Wall height variation as identified in the R-Codes assessment.	Refer to upcoming assessment of the wall height.
<b>Table 1 Wall Height Criteria</b>	Maximum wall height of 3.1m deemed to meet the relevant design principles without further assessment.	Proposed wall height of 3.34m.	0.24m variation.

**Assessment Against LPP 4.10 Objectives for the Assessment of Applications**

Objective	Requirement	Proposal	Assessment
(a)	Outbuildings and other Non-habitable Buildings are to be of a scale that will not unreasonably detract from the streetscape and/or the visual amenity of residents or neighbouring properties.	0.24m greater than the 3.1m height that is deemed-to-meet the R-Code design principles.	Complies. The outbuilding does not directly address any boundaries shared with a neighbouring properties. The outbuilding complies with all setbacks requirements.

			<p>Further to the above, the variation occurs due to the flat roof design. The ridge height is 3.34m only at the rear of the shed, which is significantly below the 4.5m acceptable under the policy. This occurs only due to earthworks supporting the outbuilding in response to the slope of the site, and not due to the structure of the shed. The land slopes 0.5m across the span of the outbuilding footprint.</p> <p>The floor area is proposed to be 49m<sup>2</sup> which is significantly less than the maximum permissible 120m<sup>2</sup>. Therefore, the overall height and scale of the shed is significantly below the maximum permissible limits with only a minor wall height variation included.</p>
(b)	Outbuildings and other Non-habitable Buildings are to be of a form and scale consistent with the Objectives of the Zone in which they are located.	0.24m greater than the 3.1m height that is deemed-to-meet the R-Code design principles.	Complies. Refer to discussion of the building bulk scale above under (a).
(c)	Outbuildings and other Non-habitable Buildings are to be suitably located on a site with adequate setbacks and located within a Building Envelope (where applicable). Where a lot does not have a designated Building Envelope but one is applicable under the Scheme,	0.24m greater than the 3.1m height that is deemed-to-meet the R-Code design principles.	Complies. The outbuilding is situated within the existing building envelope.

	Outbuildings and other Non-habitable Buildings should be located in proximity to, or clustered with, other development (including the dwelling) on the site.		
(d)	Outbuildings and other Non-habitable Buildings are to be located with consideration given to the retention of native vegetation and/or remnant vegetation on site.	0.24m greater than the 3.1m height that is deemed-to-meet the R-Code design principles.	Complies. The type of variation proposed does not conflict with this objective.
(e)	Outbuildings and other Non-habitable Buildings are to be located such that they do not increase the threat of bushfire to habitable buildings on the site.	0.24m greater than the 3.1m height that is deemed-to-meet the R-Code design principles.	Complies. The type of variation proposed does not conflict with this objective. Additionally, the outbuilding is located greater than 6m from any habitable building, therefore not impacting their BAL ratings.

**Local Planning Policy 4.2 Bushfire Assessment**

Provision	Requirement	Proposal	Assessment
<b>4 Policy Statement</b>	<p>In addition to the requirement to satisfy the relevant provisions of SPP3.7, Guidelines and associated documents, the following provisions detail the City's expectations for applications for development in bushfire prone areas.</p> <p>While there is a presumption that the policy provisions detailed below should</p>		

	be satisfied for all relevant applications, the notes listed below each policy provision provide guidance on how the City will assess alternative solutions, should they be required, and form part of the policy.		
4.1	All supporting plans and documents relevant to consideration of bushfire risk have been endorsed by an Appropriately Accredited Professional.	BMP provided by accredited level 3 bushfire planning practitioner, including BAL rating assessment and Bushfire Emergency Evacuation Plan.	Complies.
4.2	<p>The APZ proposed to be provided:</p> <p>a. Is no greater than 25 metres from Relevant Buildings.</p> <p><i>Note: A larger APZ may be considered, but only to the extent necessary to allow the determined BAL requirement to be reduced to BAL29, and where there are considered to be no unacceptable impacts on biodiversity, landscape values or visual amenity impacts.</i></p> <p><i>In respect of visual amenity, this shall generally require a demonstration that the larger APZ will not increase the visibility of the proposed</i></p>	<p>a. The APZ extends less than 25m from relevant buildings.</p> <p>b. The APZ is accommodated entirely within the lot boundaries.</p> <p>c. Vegetation removal does not required a clearing permit as the site is not within a designated environmentally sensitive area.</p>	Complies with all points.

	<p><i>development, when viewed from locations outside the subject lot (and, in cases where the potential impact is considered significant, a visual/landscape assessment may be required, prepared using the methodology set out in the Visual Landscape Planning Manual).</i></p> <p>b. Can be accommodated entirely within the subject lot or, where the APZ cannot be accommodated entirely within the subject lot, the land involved –</p> <ul style="list-style-type: none"><li>i. is developed as a public road, path or is otherwise public land managed such that there is a reasonable expectation that fuel loads will be managed to the APZ standard on an ongoing basis; or</li><li>ii. is part of an APZ or Firebreak already in place and/or required on an adjoining property; or</li></ul>		
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	<p>iii. is otherwise Low-Threat Vegetation or Non-Vegetated Area.</p> <p>c. Can be created and maintained without need to obtain a clearing permit under the (State) Environmental Protection (Clearing of Native Vegetation) Regulations 2004, approval under the (State) Biodiversity Conservation Act 2016, and/or approval under the (Commonwealth) Environmental Protection and Biodiversity Conservation Act 1999.</p>		
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**Local Planning Policy 6.1 Stormwater Management**

Provision	Requirement	Proposal	Assessment
<p><b>Clause 4.2 Volumetric Requirements Table 1</b></p>	<p>Stormwater runoff is retained for use and/or infiltration within the lot at a rate of 1m<sup>3</sup> of storage per 65m<sup>2</sup> of Impervious Surfaces to accommodate 1 Year ARI.</p>	<p>Stormwater details not confirmed.</p>	<p>Complies, subject to conditions of approval. A condition is recommended to require demonstration of stormwater management compliant with clause 4.2 prior to commencement of the development.</p>

**Conclusion**

This document provides an assessment of relevant development standards applicable to the proposal under the town planning framework and consideration of the discretions requiring approval. This assessment forms the basis of the officer recommendation for approval found in the Officer Report to Council.



**SUMMARY OF SUBMISSIONS**  
**PROPOSAL: Bed and Breakfast (Change of Use of Existing Single House and New Development) - Lot 301 (No. 29)**  
**Carnarvon Castle Drive, Eagle Bay**  
**SUBMISSIONS CLOSE: 29 March 2023**  
**OFFICER: Devin Moltoni**

No.	NAME	NATURE OF SUBMISSION	APPLICANT COMMENT	OFFICER COMMENT
<b>Public Submissions</b>				
1.	Name: D. Best on behalf of The Residents of Eagle Bay Association  Locality: Eagle Bay	<p>We would like to make this submission expressing some concerns with certain aspects of this Development Application. Our concerns are as outlined in the following dot point summary:</p> <p><b>Staged DA in Order to Push the Limits:</b></p> <p>It would seem that a DA has been previously issued for this Lot for the development of a single holiday house. Construction of this house commenced in 2022 however once the structure reached plate height, all further construction activity seemed to stop. Presumably, to allow this second DA submission to be submitted to substantially increase the size of the proposed building development on this Lot.</p> <p>Interestingly, the new holiday house, as approved by the first DA for the Lot, has now become a proposed ancillary dwelling in the</p>	<p><b>Staged Development in Order to Push the Limits:</b></p> <p>The construction of the ancillary build is on the builder's timeframe and has nothing to do with this DA for our home.</p> <p>The indicative position of our home (showing both houses for our lot) was included in the DA granted on 4th July 2022.</p> <p>There was always going to be two builds and council has been aware of this since before the first submission for approval was made.</p> <p>The one currently under construction was always going to be an Ancillary for the block. With so many issues in the building sector and getting trades down south at the time, we decided to build the small house first.</p>	<p><b>Staged Development in Order to Push the Limits:</b></p> <p>The cessation of the construction does not impact the assessment of the current application. A site visit confirmed 24 May 2023 confirmed substantial commencement has been undertaken. This dwelling was approved as a Holiday Home (Single Dwelling) It is eligible to be converted to Ancillary Dwelling and Bed Breakfast classifications subject to development approval.</p> <p>No ancillary dwelling floor area limit was applicable given it was the only proposed dwelling on the site at the time of its approval.</p> <p>The City can consider variations to the ancillary dwelling 70m<sup>2</sup> plot ratio limit and the change of use to Bed and Breakfast. Refer to the State Planning Policy 7.3 Residential Design Codes (R-Codes) assessment in Attachment 3.</p>

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		<p>new DA that has recently been submitted. The fact that the partly constructed house, which is now proposed to become the Ancillary Dwelling, has a plot of 82.7m<sup>2</sup> in lieu of the allowable 70m<sup>2</sup> seems like a rather preconceived method of pushing the allowable plot ratio limits.</p> <p>It is obviously not possible to modify what has already been approved and built but we would suggest that consideration of any further planning concessions, that would allow overdevelopment of this Lot, should not be allowed.</p> <p><b>Setback Considerations:</b></p> <p>The setbacks that have been shown in the proposed Site Plan submitted with DA/0817 are not correct. In April 2018, Amendment No 29 to the Town Planning Scheme was approved by City of Busselton. This amendment changed the zoning of</p>	<p>That way we could move down sooner. We hoped that the building supply shortages and additional building cost issues would correct themselves during this time.</p> <p>We used and were assessed using Local Planning Scheme No. 21 - Clause 4.32 (refers to what is now cl. 4.33 after recent Scheme amendment since the submission was received) Ancillary Accommodation in Rural Areas: which allow 80m<sup>2</sup>.</p> <p>This is not overdevelopment.</p> <p><b>Setback Considerations:</b></p> <p>The setbacks shown in our DA are correct for our Lot 301 and as supplied to us when we signed the sale contract in October 2020.</p> <p>We submitted a DA for the smaller house, currently under construction, and that was</p>	<p>Local Planning Scheme No. 21 (the Scheme) clause (cl.). 4.33 is not applicable. Cl. 4.33 ceased to be applicable upon the rezoning of Lot 301 (No. 29) Carnarvon Castle Drive (the Site) from Rural Residential to Residential R 2.5 on June 7 2019. The ancillary dwelling is subject to R-Codes cl. 5.5.1 Ancillary Dwellings.</p> <p>The potential approval of an Ancillary Dwelling plot ratio area does not prevent consideration of other variations from consideration for approval. The proposal must be assessed on its merits.</p> <p><b>Setback Considerations:</b></p> <p>The proposed setbacks are compliant. The City Planning Department takes the position that the Development Guide Plan for Lot 50 Eagle Crescent and Portion Location 1348 Carnarvon Castle Drive, Eagle Bay (the DGP) and associated endorsed Building Envelope Plan provide the setbacks for the site. Refer to the setback assessment in</p>

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		<p>Lots 301 to 310, in Carnarvon Castle Drive and Vixen Close from "Rural Residential" to zoning "R2.5". The setbacks for any development on this Lot should therefore be in accordance with Sector 3 of Table 1 (as per Schedule 4 - Eagle Bay Special Character Area – of the Town Planning Scheme).</p> <p>The defined setbacks therefore being:</p> <p>Front: 20m Rear: 6m Secondary Street: 12m</p> <p>As the driveway and the front entry to the main house will be off the section of Carnarvon Castle Drive that runs north/south we would assume that this is considered the "front" of the Lot and the section of Carnarvon Castle Drive running east/west would be considered a secondary street.</p>	<p>approved by council based on our given setbacks as a Holiday Home for 4 people on July 4th 2022.</p> <p>Within the attached link, to the summary of submissions proposal AMD21/0029 from what we understand as being from April 2018, and relevant to the rezoning of blocks 301 -310 Carnarvon Castle Drive and Vixen Close, the council notes in a reply to L Broomhall, 40 Carnarvon Castle Drive (point 37 on page 23) that:</p> <p>'The existing Structure Plan and Building Envelope Plan that are applicable to these properties identify building exclusion areas to assist in protecting vegetation, and the amenity of residents and neighbouring properties'.</p> <p>And also:</p> <p>'The proposed rezoning will not alter or remove these requirements as they apply to the subject properties'.</p>	<p>Attachment 3 and the Officer Comment in the Report to Council discussing the City Planning Department's application of these setback standards as opposed to the 'sector 3' setback standards.</p>

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		<p>Eagle Bay has been defined in the City's overall planning strategy as a Special Character Area, in order to preserve its unique visual amenity and natural bush aspect. One of the planning tools utilized to prevent overdevelopment of building Lots and retain its special visual amenity is the defined street setback requirements.</p> <p>Based on the setback assumptions noted above the proposed, and already partly constructed, "ancillary building" has by default, already achieved setback concessions (i.e. 10.6m in lieu of the stipulated 12m). From the required front setback point of view (20m) both the proposed shed and water tank are non-compliant. Also a large proportion of the proposed new dwelling is located within the 20m front setback zone. There is potential for the house to be moved to the east to achieve compliance however the tank and the shed would need to be relocated out of</p>	<p>Note- a link has been supplied to this advice but the link is faulty.</p> <p>As per the information supplied above re the April 2018 rezoning that included Lot 301 and the setbacks we were provided at sale, and set submitted plans against, that were approved by council for the smaller first dwelling, we do not agree with the submitter's assumptions.</p> <p>We may move the water tank from that location and have not decided yet. If the council prefers we can make a separate application for the water tank at a later date.</p> <p>The shed will be clad in and architectural standing seam Colorbond in Monument which is the same material as what our main house, in this same application, will be clad in. This is also the same colour as the ancillary building, to all blend seamlessly with each other. Unlike surrounding white houses that can be seen from the top of Eagle Bay Rd, ours will sit well within the environment.</p>	

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		<p>the front setback zone to achieve compliance. Perhaps the tank could be relocated to an alternate location that is out of the front setback zone.</p> <p>We would suggest that to try and squeeze an “Ancillary Building” and a large shed onto Lot 301 will be visually imposing and not in keeping with the character of the Eagle Bay development guidelines. Perhaps if a shed is required it could be reduced in size added to the end of the proposed garage, which would then make it less visually imposing.</p> <p>Carnarvon Castle Drive is the main access road into the adjoining rural residential community and any large and bulky structures, such as a large shed (with a significant sized roller door), adjacent to the street verge will be very visually imposing and non-complaint with defined setback requirements.</p>	<p>Many homes in Eagle Bay have garage doors visible to the street, this is nothing out of character for the area. It complies with the setbacks for our block and the distances required from both buildings to comply with our BAL reports.</p> <p>The ancillary currently under construction was assessed under section 4 and the main house should be considered the same.</p>	

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		<p><b>Height and Bulk of Proposed House:</b></p> <p>The “Endorsed Structure Plan” for this subdivision area in Eagle Bay included provisions to control the visual impacts of building developments within the subdivision. These defined controls were to limit maximum building heights to 7.5m above NGL and also to limit the area of the second story of any building to be not more than 50% of the ground floor building area. However, on the smaller Lot sizes in the subdivision, such as Lot 301, it would seem reasonable to allow some concessions on the floor area of second storey.</p> <p>In terms of height limits, other new developments in this subdivision location have undertaken cut and fill earthworks to ensure they do not exceed to defined 7.5m maximum height limit above natural ground level. In comparison the Lot 301</p>	<p><b>Height and Bulk of Proposed House</b></p> <p>Re height and bulk from within Local Planning Policy No3.3 Eagle Bay Special Character Area.</p> <p>‘1. ELEMENT F - BUILDING HEIGHT</p> <p>The following provisions shall apply;</p> <ul style="list-style-type: none"> <li>• Proposals within 300mm of the maximum permissible height limit shall be required to be verified as complying with the maximum height limit by a licensed surveyor prior to the issue of a Permit to Use. Any noncompliance with the maximum height limit shall be rectified by the proponent and certified by a licensed surveyor prior to the issue of a permit to use’.</li> <li>• Our plans are within 300mm of the maximum height if using 7.5m.</li> </ul>	<p><b>Height and Bulk of Proposed House</b></p> <p>It is noted the objection does not oppose the variation to the proportion of the dwelling situated on the upper floor.</p> <p>The 7.5m height limit is provided by the Eagle Bay Structure Plan (the Structure Plan) and the DGP, for which the City has discretion to approve variations. The applicant has the right to apply for a variation to the 7.5m.</p> <p>The proposed building height has been assessed. It has been determined that no detrimental impact to the character or amenity of the area will result as a consequence of the proposed building height.</p>

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		<p>development plan has proposed fill on the site only, hence the over height building. The required 7.5m height limit could easily be achieved at Lot 301 utilizing some earthworks cut as well as fill.</p> <p>Alternatively the proposed building has ceiling heights of 3m in the over height section of the buildings which could be reduced to enable the defined 7.5m height limits to be achieved</p> <p><b>Parking Requirements for Ancillary Building:</b></p> <p>The R codes relating to an “Ancillary Building” require that parking be provided for the ancillary building. (i.e. a minimum parking area of 2 spaces for the single house plus a parking area of 1 space for the ancillary building.)</p> <p>The proposed site plan does not show any parking provisions in the vicinity of the ancillary dwelling.</p>	<ul style="list-style-type: none"> <li>• We can engage a surveyor to verify that we comply with the maximum height limit.</li> <li>• ‘The applicable height limits will be 7.5m within 150 metres of the high water mark and 9.0 metres for all other areas unless otherwise determined by zoning provisions, structure plan or a DGP’.</li> <li>• We are well more than 150 meters clear of the high-water mark.</li> </ul> <p><b>Parking Requirements for Ancillary Building</b></p> <p>We must provide two parking bays for the Ancillary, not one and we have complied with that.</p> <p>These bays can be relocated from the front of our home, shown on the plans, to the East side along the retaining wall that adjoins the Ancillary.</p>	<p><b>Parking Requirements</b></p> <p>The proposed car parking on the site is compliant with the Rcodes and Local Planning Policy 2.1 Car Parking (LPP 2.1).</p>

**SUMMARY OF SUBMISSIONS**  
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**Carnarvon Castle Drive, Eagle Bay**  
**SUBMISSIONS CLOSE: 29 March 2023**  
**OFFICER: Devin Moltoni**

No.	NAME	NATURE OF SUBMISSION	APPLICANT COMMENT	OFFICER COMMENT
		<p>Without some specific parking provision being provided it is pretty obvious that the street verge will become the default parking area for the ancillary building, with the associated destruction of any natural or replanted verge vegetation.</p> <p>If the large shed and the water tank are relocated from the front setback zone (as per dot point 2 above) then part of this area could be used to provide allocated parking for the ancillary dwelling.</p> <p><b>Closing Statement</b></p> <p>A principle objective of the Eagle Bay Special Character Area Plan is to protect and enhance the unique and special character of the Eagle Bay township. This being achieved through the use of residential design controls to maintain a low overall density and visual impact of developments within the residential area.</p>	<p><b>Response to Closing Statement</b></p> <p>Our home and Ancillary together are smaller in square meters than other single homes within the special character area. Our development cannot be considered as an “over development</p>	<p><b>Consideration of Closing Statement</b></p> <p>The variations to the acceptable development standards have been assessed and determined to be in keeping with the character of the area and are not expected to result in detriment to the amenity of the area.</p>

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		We already have an example of planning concessions allowing an over height and over bulk development at Lot 304 Carnarvon Castle Drive, that in not in keeping with the natural amenity of the area. Let's not have a repeat of this at Lot 301".		
2.	Names: C. and L. Paul  Locality: Eagle Bay	While we welcome measured development of Lot 301, we have serious and valid concerns about the proposed development and object to the following points: <b>Proposed Short Stay Accommodation Use</b>  We do NOT wish to have "Bed and Breakfast Use Accommodation / four guests" so close to our home. My husband and I were under the impression that the existing holiday home single dwelling was the only construction applicable. This home was also built outside the building envelope (Balcony) which we did	<b>Short Stay Accommodation Use on the Site</b>  The small house has already been approved as a Holiday Home. This was done on our second DA to council in July 2022. It was designed for holiday use, with a main home outlined on the plans and always part of the overall plan for Lot 301. Nothing has changed.  Martin Bennett is a long-time good friend and associate of my family, who also own	<b>Short Stay Accommodation Use on the Site</b>  Holiday Home (Single House) 4 People Short stay use of the site has already approved under DA21/0250.  Changing this use to Bed and Breakfast use of the site reduces the scale of the short-stay accommodation use considering the Bed and Breakfast use can only operate while residents are occupying the Single House, in comparison to the

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No.	NAME	NATURE OF SUBMISSION	APPLICANT COMMENT	OFFICER COMMENT
		<p>not oppose as we thought it was a sufficient distance from our residence.</p> <p>My husband and I vigorously oppose this application and will seek legal advice from Mr Martin Bennett (Bennett and Co) to prevent such an approval proceeding. This is an overdevelopment in regards to the proposed plot ratio in a special control area and is unacceptable. We did not build our new residence in Eagle Bay to adjoin a Bed and Breakfast facility.</p>	<p>houses at 30 Eagle Cres and 11 Eagle Cres Eagle Bay. This would be a conflict of interest for Mr Bennett.</p> <p>They are not 'adjoining' a Bed and Breakfast. Our house in this DA application will be in between their house and the Ancillary. We will be living in the Ancillary ourselves once it is completed construction early 2024 and while the big house is being built. We see elderly parents and our grown children staying and we will make it available as holiday accommodation the rest of the time.</p> <p>Once our main house, in this DA application, is completed we will be providing much needed additional holiday accommodation for the area with the Ancillary. Accommodation that is a different experience to what you would get from staying in town. Accommodation that will also support the Eagle Bay community when their homes are too full over the holiday periods during the year and they need friends and family to stay nearby. Eagle Bay is a holiday destination and this helps to support tourism and local businesses in the region.</p>	<p>Holiday Home (Single House) use which is unhosted.</p> <p>Bed and Breakfast is a 'D'- discretionary land use on the site pursuant to cl. 3.3 Table 1- Zoning Table. The proposed Bed and Breakfast use is compliant with the relevant development standards.</p>

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		<p><b>Building Envelope &amp; Setbacks</b></p> <p>It is our understanding from when we built that there was a building exclusion plan which is outdated.</p>	<p>As an example, across the road from both of our properties at No 28 Carnarvon Castle Drive is an approved and recently operating holiday house accommodation rental. There are many more dotted throughout Eagle Bay. We should not be disadvantaged when our neighbours enjoy this use of their properties.</p> <p>The change from Holiday House to Bed and Breakfast was as per council request. We can confirm that we will be living on the property. I would also note that this is believed to be a less impactful use than a holiday house as we will be permanent residents of the main dwelling, and will be there to monitor any potential occupants of the additional accommodation. Therefore the use is believed to have less impact on the surrounding residents than that of a holiday home like the one located across the road at 28 Carnarvon Castle Drive.</p> <p><b>Building Envelope and Setbacks</b></p> <p>Our setbacks have always been: 10m North and South 5m East and West</p>	<p><b>Building Envelope and Setbacks</b></p> <p>The proposal is compliant with the setbacks provided by the DGP and associated endorsed</p>

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		<p>The setbacks in this related to the original subdivision. The standard building setbacks for rural residential lots are as follows: Side = 15m, Front = 20m, Rear = 20m however a building envelope of 1500m<sup>2</sup> was required by us. Lot 301 has a proposed setback of 5m to the shed. This is with the driveway boundary &amp; should be considered the front setback and is far too close and impactful to the road. It is not in keeping with the surrounding lots and unacceptable to us. We would also require a building envelope to contain the development on the site. We feel it is over developed with 2 dwellings, a shed and water tank.</p> <p><b>Height</b></p> <p>We feel that the first floor ceiling height of 3m is not essential. A 2.7m ceiling is more than adequate. All surrounding buildings have in some way cut into the lot to work to the height limits as best they can. This lot appears to not have any cut to</p>	<p>Not what is suggested here.</p> <p>The ancillary currently under construction was assessed under section 4 and the main house should be considered the same.</p> <p><b>Height</b></p> <p>We are not interested in how the submitter's 'feel' regarding our first-floor ceiling height.</p>	<p>Building Envelope Plan. Refer to the response to the submission 1 regarding compliance with the setback standards and which setback standards apply.</p> <p><b>Height</b></p> <p>Earthworks are required for construction on the site due to the slope.</p>

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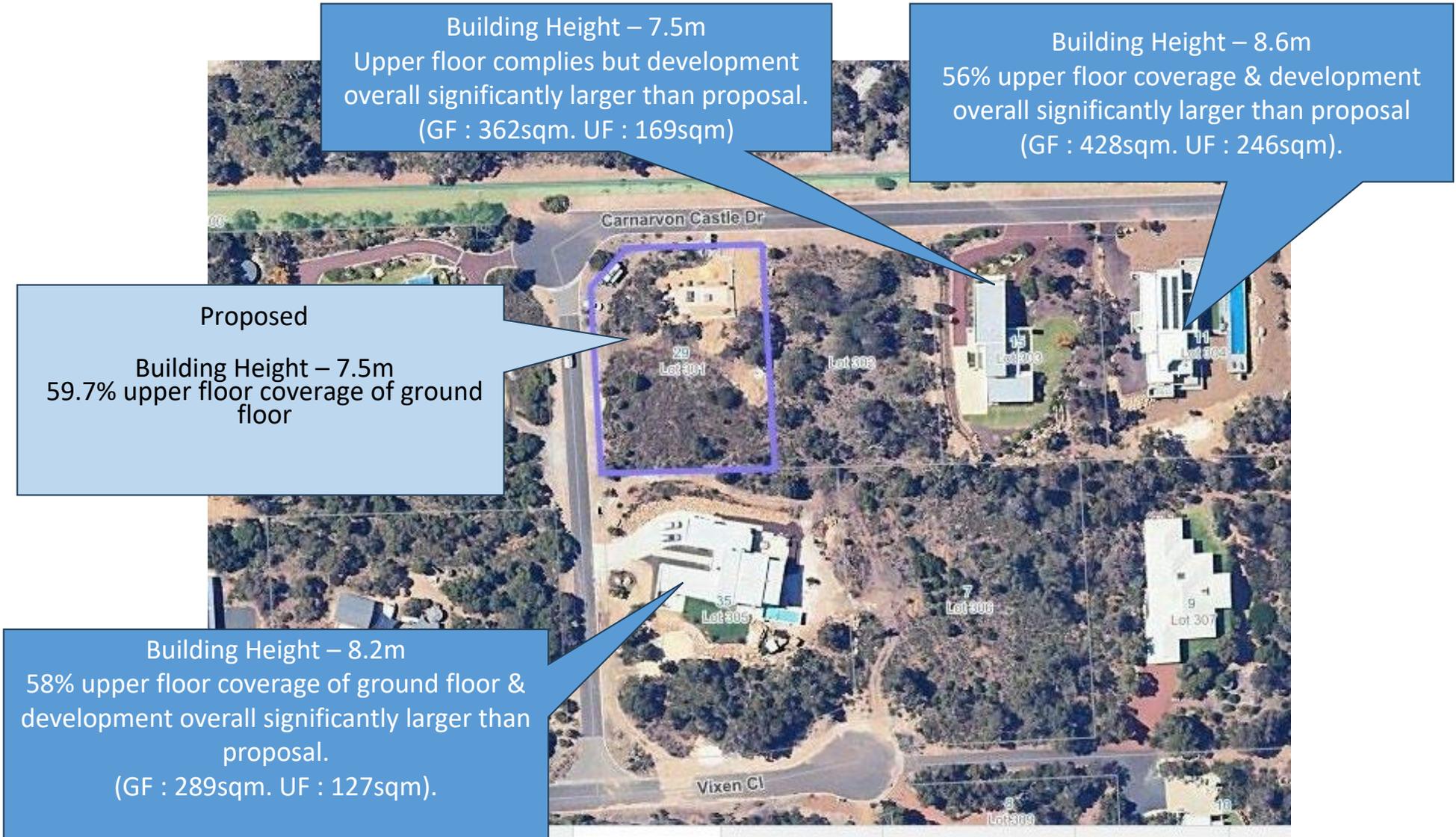
No.	NAME	NATURE OF SUBMISSION	APPLICANT COMMENT	OFFICER COMMENT
		<p>the ground floor and as such we feel the Height limit could be reduced.</p> <p><b>Ancillary Dwelling</b></p> <p>This was never approved as short stay accommodation.</p> <p>We feel that the oversized nature of this opens up potential to higher traffic, noise and reduced amenity to the surrounding areas.</p> <p>What will be the access and egress to the short stay area? How will noise and light pollution be managed?</p> <p>We are also concerned about uneducated visitors and fire pits. Our lot was rated BAL 29 and we</p>	<p><b>Ancillary Dwelling</b></p> <p>There will be no more traffic than a neighbour having a friend over to stay.</p> <p>The statement re the ancillary dwelling having never been approved as short stay accommodation, could not be further from correct, with consciousness. The smaller house has absolutely been approved as a holiday house as detailed within this reply.</p> <p>There will be no light or noise any different to any other home in the area with short stay accommodation. We will be living in the smaller house ourselves while the</p>	<p>The applicant is permitted to apply to vary the 7.5m height standard provided by the Structure Plan.</p> <p>The proposed building height has been assessed. It has been determined that no detrimental impact to the character or amenity of the area will result as a consequence of the proposed building height.</p> <p><b>Ancillary Dwelling</b></p> <p>Holiday Home (Single House) 4 People short stay use is already approved on the site. A change of use to Ancillary Dwelling with a Bed and Breakfast use of the subject building is now proposed. Bed and Breakfast is a discretionary land use. It is considered to be a lower scale of short-stay accommodation use given it is only permitted to operate while the Single House on the site is occupied by residents.</p> <p>In relation to traffic and light, the Bed and Breakfast use is considered to be commensurate with a residential use.</p>

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		<p>needed a BMP that was costly and with one exit the area is very dangerous.</p> <p>The Ancillary dwelling at 82m<sup>2</sup> is fully detached which essentially provides 2 residences onsite that we do not support, especially for short stay. Parking is also an issue.</p> <p style="text-align: center;"><b>Vegetation &amp; Landscaping</b></p>	<p>bigger house is being built and that house will be between the submitter's house and the ancillary.</p> <p>The submitter's own comment made above on the distance between the Ancillary and their house, they said our Ancillary "was a sufficient distance from our residence." And we agree it is.</p> <p>We have had three BAL reports prepared by Envision for our block to date and are fully aware of our BAL and BMP. There is no fire pit shown for guests use.</p> <p>We used and were assessed using Local Planning Scheme No. 21 - Clause 4.32 Ancillary Accommodation in Rural Areas: which allow 80m<sup>2</sup>.</p> <p>It must be physically linked by form of roof cover to the single house, with the max. separation not exceeding 10m; of which we have already demonstrated on our plans.</p> <p style="text-align: center;"><b>Vegetation and Landscaping</b></p>	<p>Noise from the Bed and Breakfast is also considered commensurate with a residential use, and is subject to the Environmental Protection (Noise) Regulations 1997. On site management is required for Bed and Breakfast use by occupiers of the house.</p> <p>Subject to compliance with the City's Firebreak and Fuel Hazard Reduction Notice, residents and guests are permitted to have a fire pit. The proposal also demonstrates compliance with State Planning Policy 3.7 Planning in Bushfire Prone Areas.</p> <p>The site is permitted to be developed with an Ancillary Dwelling. A variation to the R-codes deemed-to-comply limit 70m<sup>2</sup> of plot ratio area is proposed. Refer to the R-Codes design principles assessment in Attachment 3. Cl. 4.33 of the Scheme is not applicable.</p> <p>Parking is compliant with the R-Codes and LPP 2.1 standards.</p> <p style="text-align: center;"><b>Vegetation and Landscaping</b></p>

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		<p>With such a high segment of the lot being developed, we are concerned about the limited landscaping that can be integrated into the lot, specifically to the street setbacks but also to the south which has a view to our living areas. All these seem to be setbacks reduced from the scheme. We would urge the owners to develop the lot to the Local planning scheme.</p>	<p>Landscaping will be completed within the council's guidelines and plant specifications.</p> <p>We own a corner block the whole corner will need to be landscaped.</p>	<p>A landscaping plan was required by a condition of approval DA21/0250.01 to screen the northern aspect of the approved dwelling now subject to change of use to Ancillary Dwelling from view from the street.</p> <p>No setback variations are proposed. The proposal is compliant with the deemed-to-comply standards of R-Codes cl. 5.4.1 Visual Privacy.</p> <p>The proposed removal of vegetation is limited to accommodating the building footprints and establishing an asset protection zone. This is permitted.</p>





Disclaimer: Every effort has been made to make the information displayed here as accurate as possible. This process is ongoing and the information is therefore ever changing and cannot be disseminated as accurate. Care must be taken not to use this information as correct or legally binding. To verify information contact the City of Busselton office.

**DA23/0327 - 6**  
**Seaview Rise**

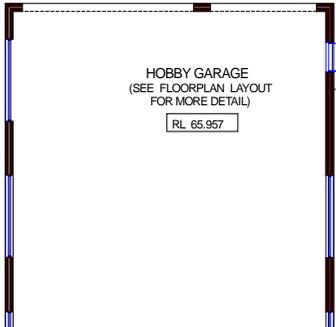
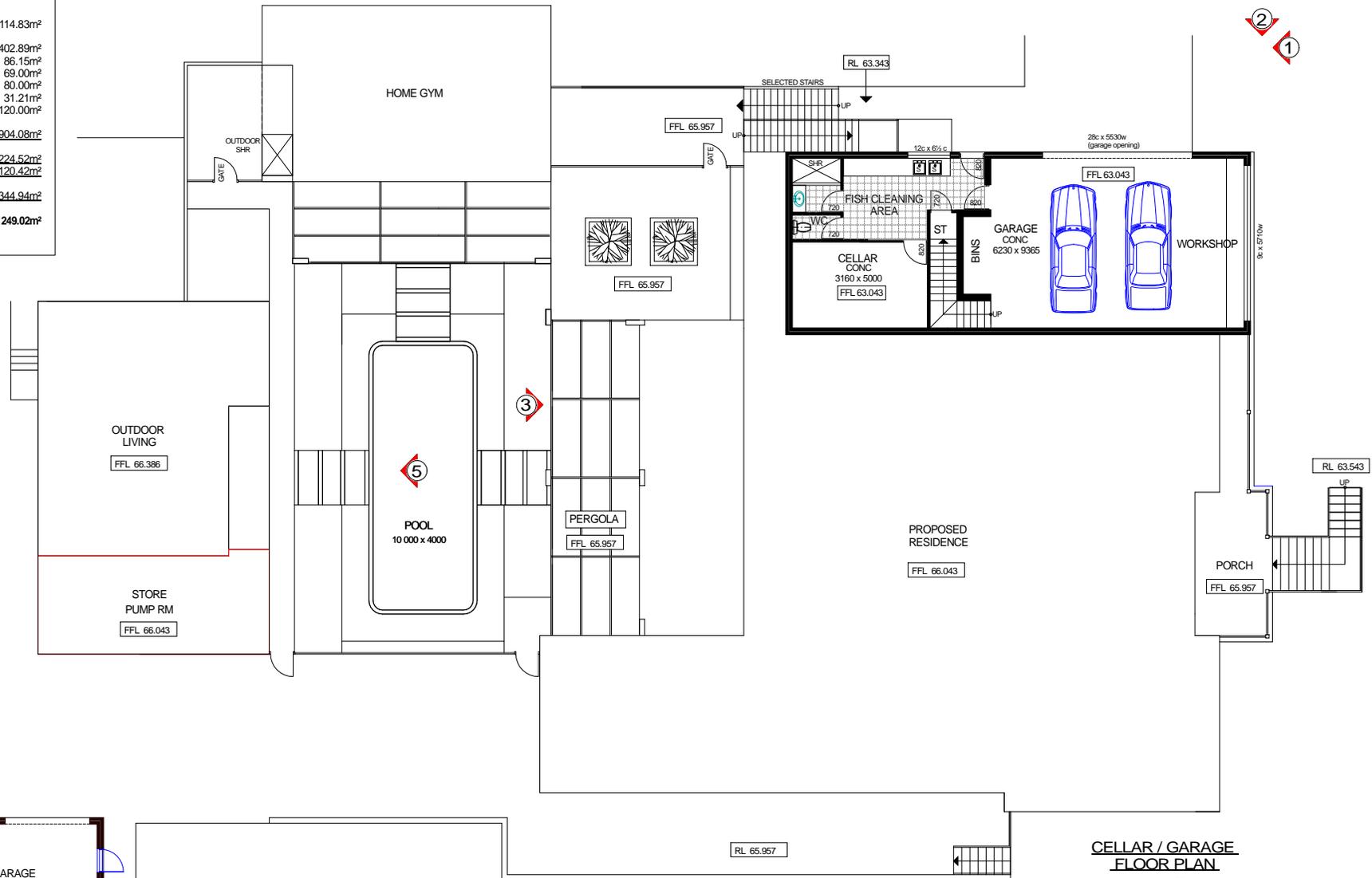
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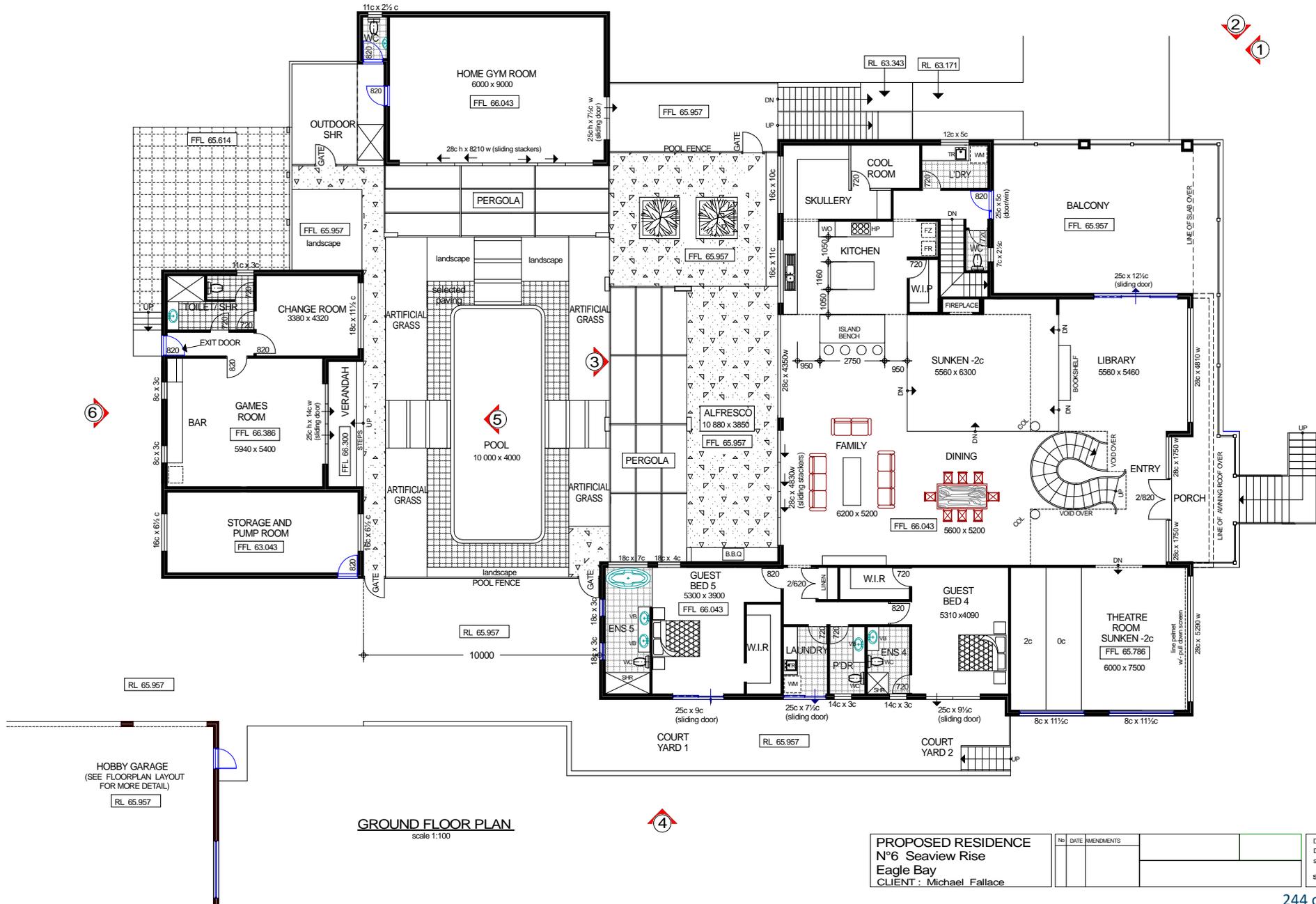


Areas	
Garage/cellar	114.83m <sup>2</sup>
<b>Ground Floor</b>	<b>402.89m<sup>2</sup></b>
Porch/balcony	86.15m <sup>2</sup>
Home Gym	69.00m <sup>2</sup>
Outdoor Rec room	80.00m <sup>2</sup>
Store/Pump rm	31.21m <sup>2</sup>
Hobby Garage	120.00m <sup>2</sup>
<b>Total Ground Floor/Cellar</b>	<b>904.08m<sup>2</sup></b>
<b>Upper Floor</b>	<b>224.52m<sup>2</sup></b>
<b>Top Deck</b>	<b>120.42m<sup>2</sup></b>
<b>Total Upper Floor</b>	<b>344.94m<sup>2</sup></b>
<b>Total Area</b>	<b>1249.02m<sup>2</sup></b>



**CELLAR / GARAGE FLOOR PLAN**  
scale 1:100

<b>PROPOSED RESIDENCE</b> N°6 Seaview Rise Eagle Bay CLIENT : Michael Fallace	No.	DATE	AMENDMENTS				
						DATE: Jan 2024 Drawn: MA scale: 1:100 sheet 2 of 8	



**GROUND FLOOR PLAN**  
scale 1:100

<b>PROPOSED RESIDENCE</b> N°6 Seaview Rise Eagle Bay CLIENT: Michael Fallace		No.   DATE   AMENDMENTS	DATE: Jan 2024 Drawn: MA scale: 1:100 sheet 3 of 8
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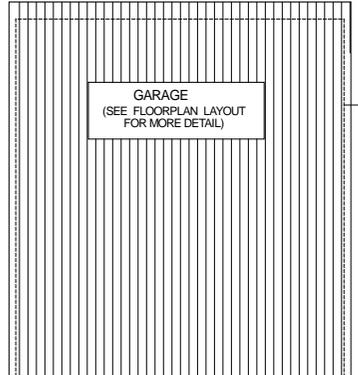
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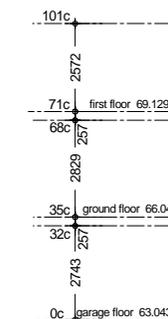
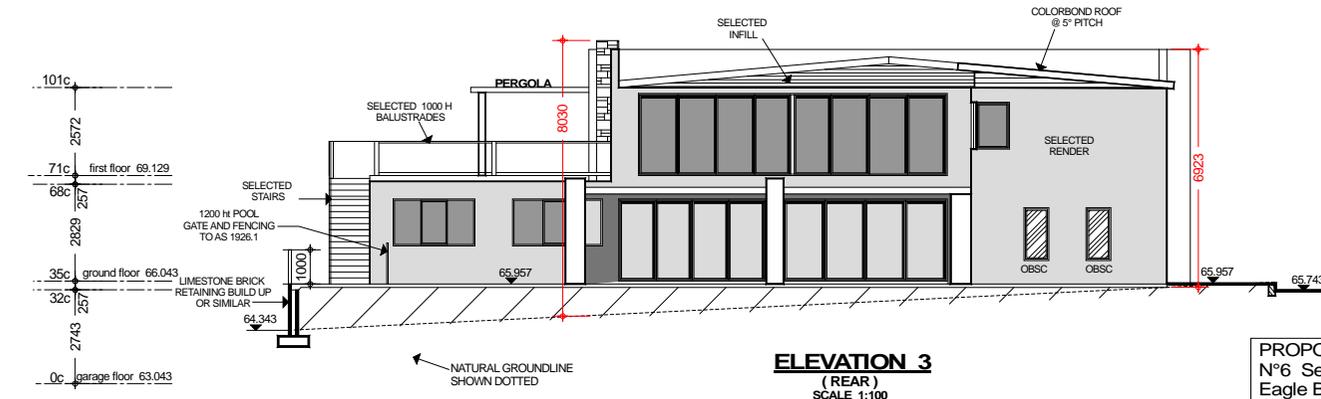
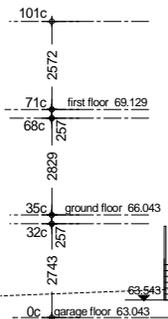
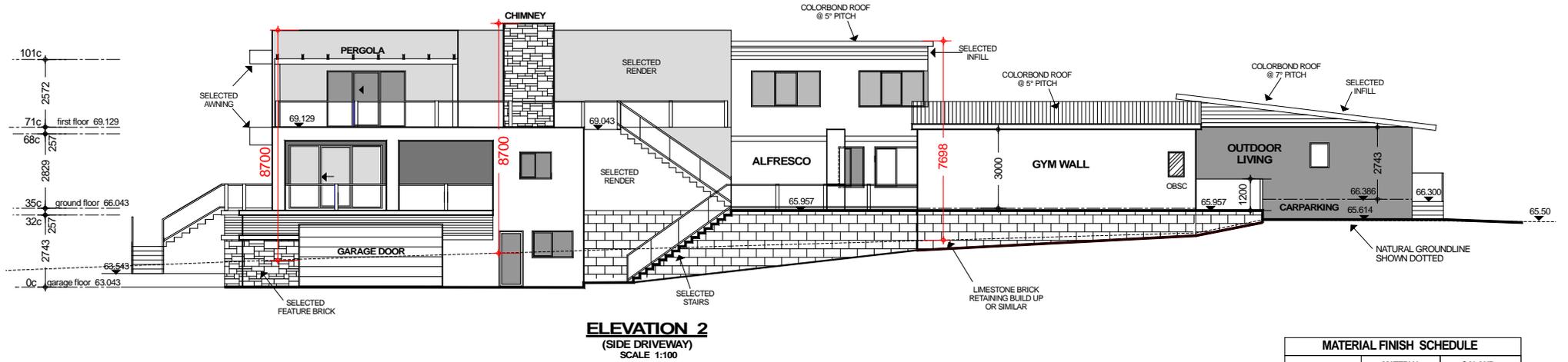
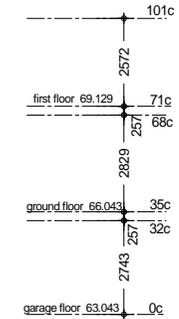
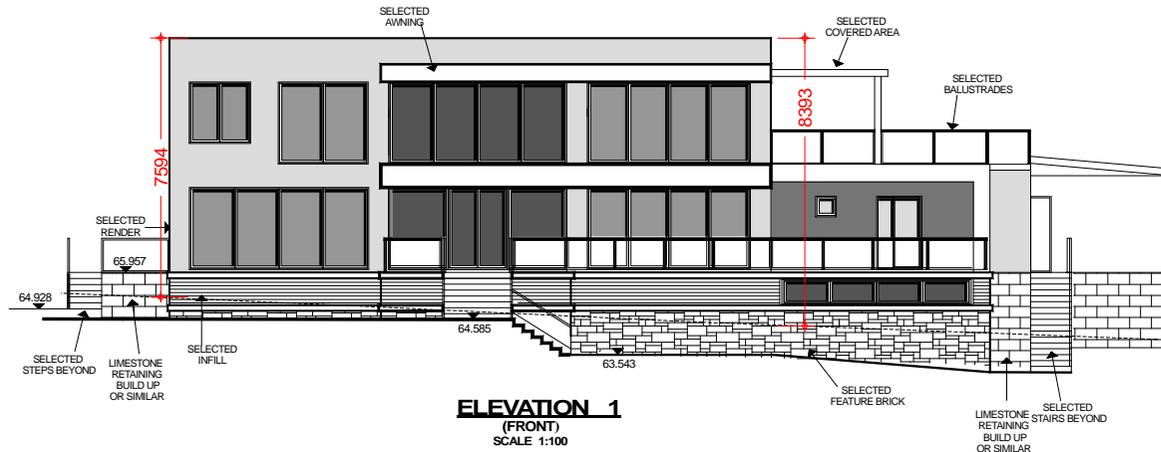
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GARAGE  
(SEE FLOORPLAN LAYOUT  
FOR MORE DETAIL)

FIRST FLOOR PLAN  
scale 1:100

PROPOSED RESIDENCE N°6 Seaview Rise Eagle Bay CLIENT : Michael Fallace	NO	DATE	AMENDMENTS	
DATE: Jan 2024 Drawn: MA scale: 1:100 sheet 4 of 8				

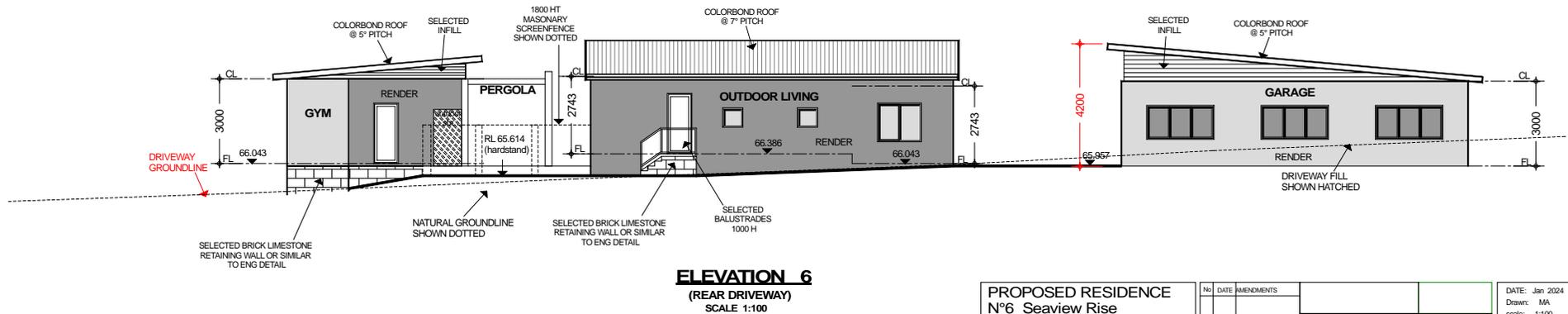
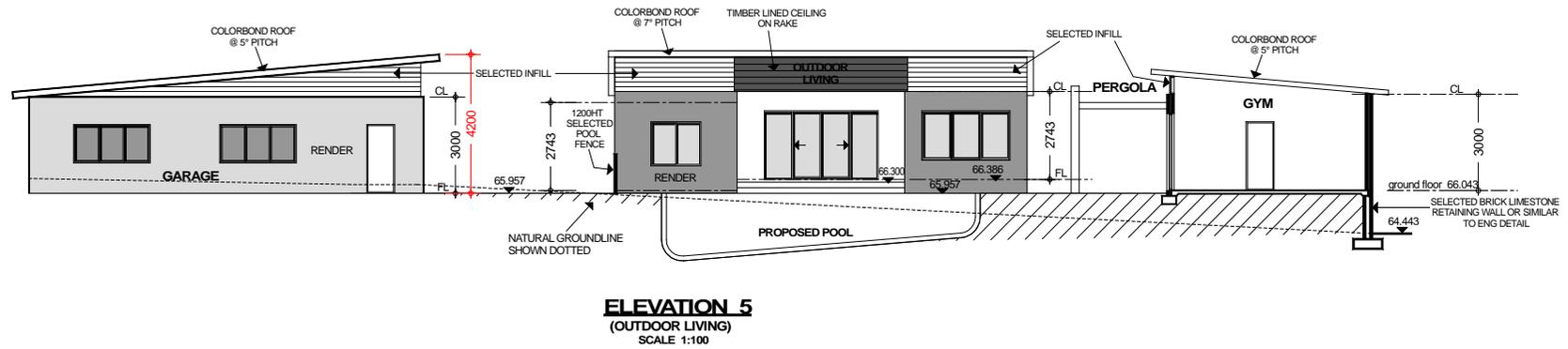
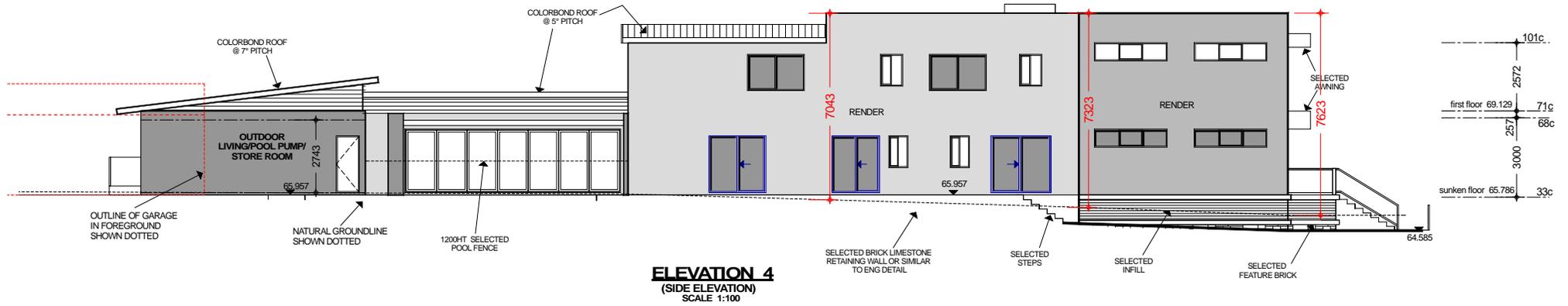


MATERIAL FINISH SCHEDULE		
	MATERIAL	CLOUR
WALLS	BRICK MASONRY RENDER	DIESKAU
FEATURE WALL	FEATUREBRICK	NEUTRAL
ROOF	COLORBOND STEEL	MONUMENT
GUTTERS & DP	COLORBOND STEEL	MONUMENT
WINDOW FRAMES	ALUMINIUM	BLACK
ROOF DECK COLUMNS	STEEL SHS	MONUMENT
GARAGE DOORS	TIMBER LOOK	
BALCONY CEILING LINING	TIMBER LOOK	
BALCONY ROOF FASCIA	COLORBOND STEEL	MONUMENT
DRIVEWAY & CROSSOVER PAVING	CONCRETE PAVE	NEUTRAL

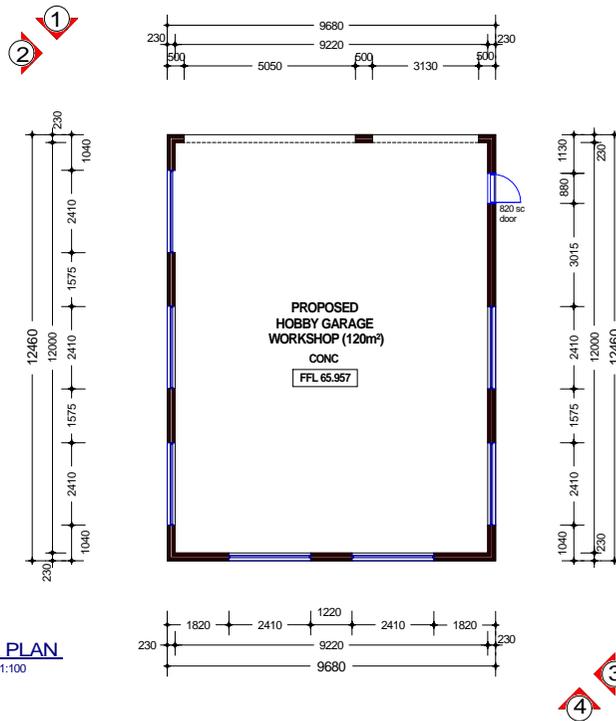
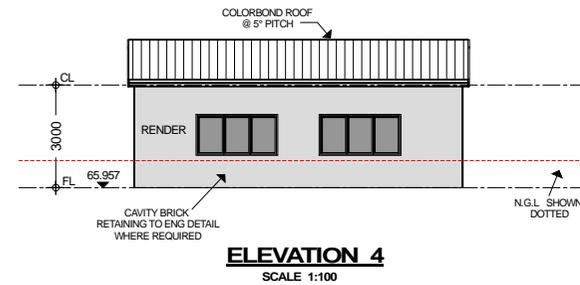
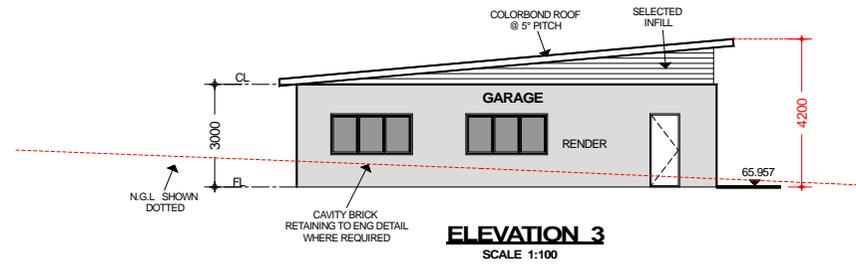
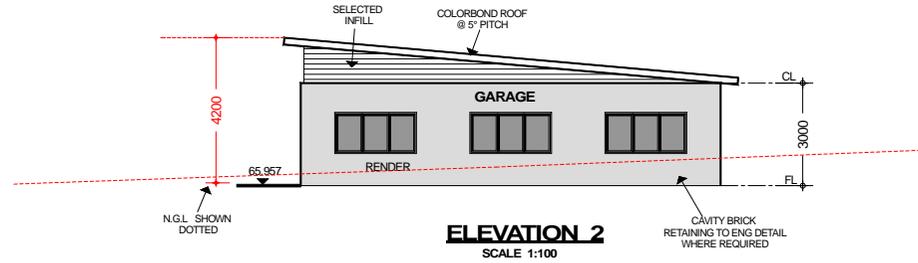
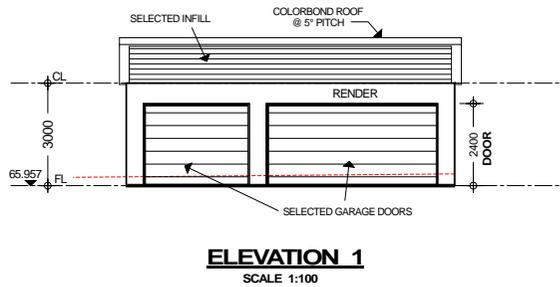
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N°6 Seaview Rise  
Eagle Bay  
CLIENT: Michael Fallace

No	DATE	AMENDMENTS

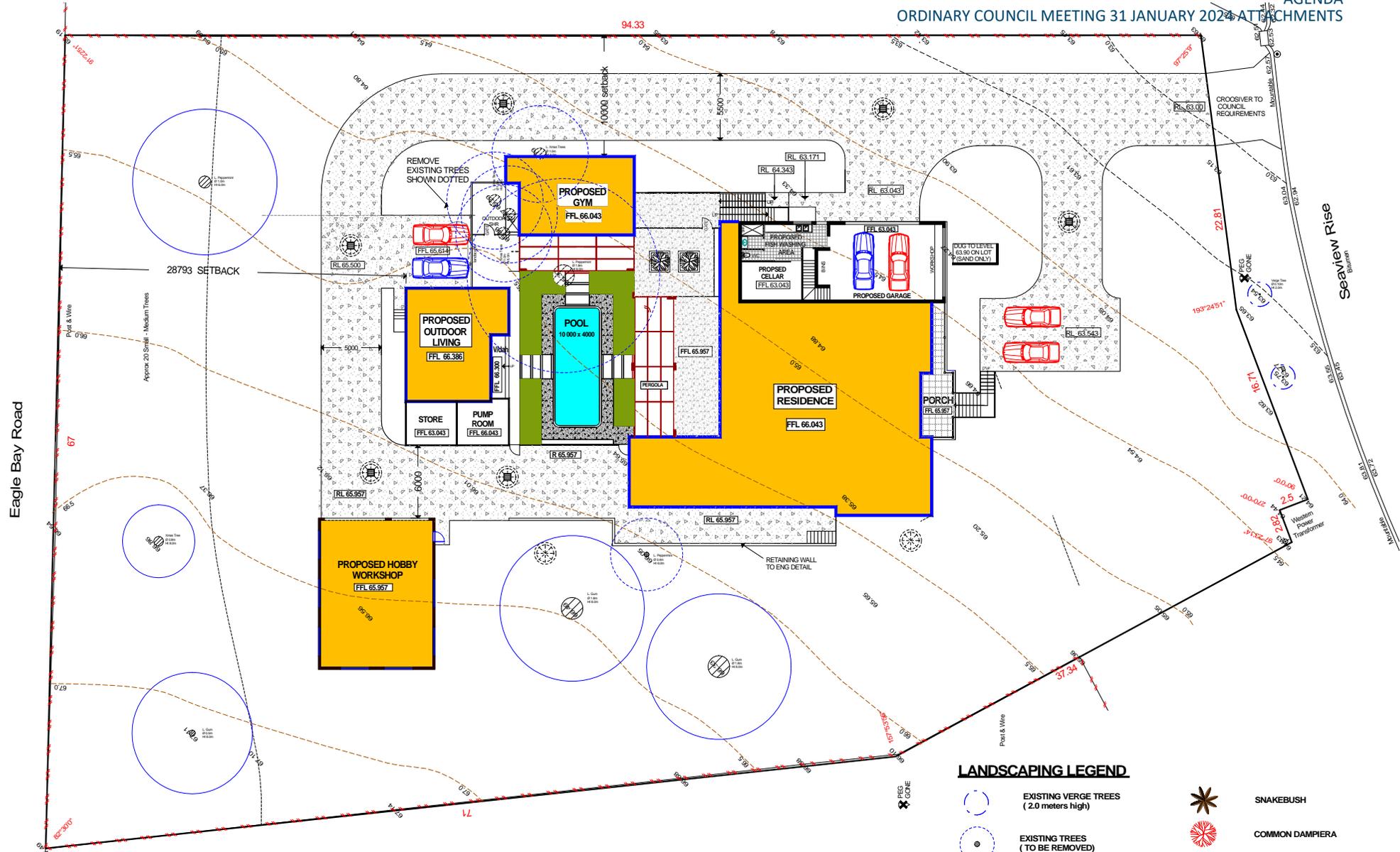
DATE: Jan 2024  
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sheet 5 of 8



PROPOSED RESIDENCE N°6 Seaview Rise Eagle Bay CLIENT: Michael Fallace	No	DATE	AMENDMENTS	DATE: Jan 2024 Drawn: MA scale: 1:100 sheet 6 of 8



PROPOSED RESIDENCE N°6 Seaview Rise Eagle Bay CLIENT: Michael Fallace	No.	DATE	AMENDMENTS				
							DATE: Jan 2024 Drawn: MA scale: 1:100 sheet 7 of 8



LANDSCAPE SITEPLAN  
scale 1:200



SOAK WELL LEGEND

- 1500 DIA x 1200 EFD SOAKWELL
- 1500 DIA x 1200 EFD SOAKWELL w/ TRAFFICABLE GRATE

LANDSCAPING LEGEND

- EXISTING VERGE TREES (2.0 meters high)
- EXISTING TREES (TO BE REMOVED)
- EXISTING EUCALPTUS TREES (min 6 metres high and over)
- SNAKEBUSH
- COMMON DAMIERA
- SHORT SPIKE GREVILLEA
- NEW GRASS COVER
- NEW CONCRETE PAVE DRIVEWAY

PROPOSED RESIDENCE N°6 Seaview Rise Eagle Bay CLIENT : Michael Fallace	NO	DATE	AMENDMENTS		DATE: Jan 2024 Drawn: MA scale: 1:100 sheet 8 of 8

LANDSCAPE DESIGN RATIONALE

Initial site investigations reveal a cleared 1520m<sup>2</sup> lot with naturally-recruited, juvenile Peppermint trees (*Agonis flexuosa*) on the Seaview Rise verge and no formal paved footpath or crossovers. The streetscape is typically yellow sand with occasional service pits and a transformer to the north-east corner of the lot.

The streetscape in this location would benefit from additional street tree planting to match the existing street tree selections with species size and positioning considerations for the views from the proposed residence.

The internal landscape character is proposed to be enhanced to match the vernacular materials and finishes common to Eagle Bay with a variety of locally indigenous street trees to make a colourful statement as a welcoming identity and extension of local environmental values of the precinct.

No turf will be used on street verges but rather planted and drip-irrigated to soften the presentation to both Eagle Bay Road at the back of the lot and on Seaview Rise

frontage. The landscape design for this project will consist of plants and materials sourced locally. The softworks designs are inspired by local vegetation complexes punctuated with small and medium-sized ornamental trees to create a flowering, edible and scented, native garden setting to the residence.

Internal courtyards around the pool area will incorporate a drought-tolerant, succulent planting rationale contained by pavements on all sides to minimise any risks of garden escapes or weed egress. In addition to technical responses to maintaining fire separation, native planting, sustainable irrigation and drainage, a strong greening rationale is incorporated to achieve a verdant, shaded environment that will complement the presentation of the buildings to the street and neighbouring lots. Larger native Eucalypts, Banksias and Peppermints will be incorporated to tie in with local habitat corridors and promote shade to alfresco areas and facades of buildings generally.

LEGEND

- DECORATIVE CONCRETE PAVING**  
exposed aggregate | saw cuts | acid etched
- CONCRETE PAVING**  
reinforced + thickened | exposed aggregate
- ASPHALT TRAFFICABLE PAVEMENT**  
red asphalt | flush concrete kerbs + beams
- TIMBER DECK | Hardwood or Composite**  
5mm joints | treated softwood substructure
- GRANULAR PAVING | Permeable**  
75 cracked pea gravel | compacted insitu
- FIRE BREAK | DBCA standards**  
4m natural earth | 6m slashed seasonally
- TREES TO BE RETAINED**  
to be protected to AS 4970
- TREE TO BE REMOVED**  
stumps + roots to be removed
- PROPOSED TREE**  
refer schedule for selections & sizes
- MASS PLANTING**  
75mm organic mulch | 3 plants/2sqm | 130mm pots
- FIRE SEPARATION ZONE** | 6m bdg perimeter  
no trees permitted
- FEATURE PLANTING**  
accent species | succulent or vertical accent

IRRIGATION RATIONALE

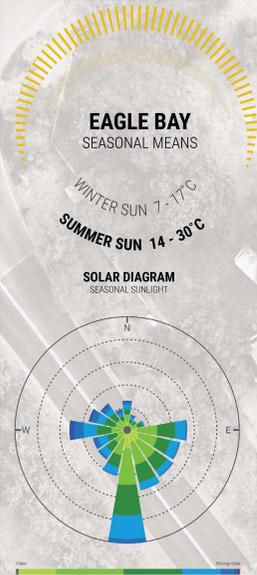
Irrigation will be a series of smart, automatic irrigation systems that will provide drip irrigation to individual trees and mass planting areas.

- DRIP IRRIGATION THROUGHOUT**  
in-line drip system | subsurface
- BUBBLERS TO TREES**  
1-3 bubblers per tree dependent on size
- NO OVERHEAD SPRAY SPRINKLERS**  
minimise water use, over spray + evaporation
- HYDROZONING**  
designed with water demand calibrated stations
- SOIL MOISTURE SENSORS**  
smart system to respond to soil moisture levels
- RAIN SENSORS**  
smart system to respond to prevailing weather
- PASSIVE IRRIGATION**  
direct storm water to gardens before drains

BUSHFIRE MITIGATION RATIONALE

Bushfire mitigation measures involve vegetation modification or removal, in accordance with State Planning Policy 3.7 Planning in Bushfire Prone Areas. This includes implementing the vegetation management measures set out in Schedule 1 of the Guidelines for Planning in Bushfire Prone Areas 2021.

- USE FIREWISE PLANT SPECIES**  
succulent, glossy leaf, sparse canopy species
- MAINTAIN LANDSCAPE TO REDUCE FUEL LOADS**  
regularly remove leaf litter and overhanging branches
- USE SMALL TREES**  
minimise potential for tree canopy to grow into a risk area
- ISOLATE CLUMPS OF VEGETATION**  
separate defendable areas of vegetation with non flammable surfaces i.e. paving
- USE NON-FLAMMABLE MATERIALS**  
avoid plastics and fabrics that easily ignite



WIND DIRECTION

- M1**  
*Xanthorrhoea preissii* Balga  
*Pimelea ferruginea* Rice Flower  
*Eremophila glabra* Kalbarri Carpet
- M2**  
*Amigodanthus mangrovei* Red + Green Kangaroo Paw  
*Conostyche canadensis* Grey Cotton Heads  
*Mitrasacme purpurilium* Creeping Boobialla
- M3**  
*Hibbertia scandens* Snake Vine  
*Westringia dampiera* Coastal Rosemary  
*Casuarina "Cousin It"* Cousin It  
*Carpodrotus virens* Pig Face
- M4**  
*Billardiera heterophylla* WA Blue Bell  
*Leucophyta brownii* Pincushion Bush  
*Lepidosperma gladiatum* Coastal Sword Sedge  
*Hemiantra pungens* Snake Bush
- M5**  
*Agave attenuata* Century Plant  
*Aeonium arborum* Irish Rose  
*Aloe ferox* Bitter Aloe  
*Crinum pedunculatum* Spider Lily  
*Sansevieria trifasciata* Mother in Law Tongue  
*Senecio serpens* Blue Chalk



- Cv** *Callistemon citrinus* Kings Park Special 100L
- Cf** *Citrus latifolia* Lime 200L
- Dt** *Diracena marginata* Dragon Tree 100L
- Af** *Agonis flexuosa* WA Peppermint 45L
- Bg** *Banksia grandis* Bull Banksia 45L
- Cf** *Corymbia ficifolia* Red Flowering Gum 100L
- Ed** *Eucalyptus decipiens* Red Heart Moit 30L

TYPICAL UNDERSTOREY PLANTING PALETTE

TYPICAL TREE PLANTING PALETTE

propagule



Disclaimer: Every effort has been made to make the information displayed here as accurate as possible. This process is ongoing and the information is therefore ever changing and cannot be disseminated as accurate. Care must be taken not to use this information as correct or legally binding. To verify information contact the City of Busselton office.

**Attachment 3 - Aerial indicating building setback line from Eagle Bay Road**

10/01/2024

1:1832 @ A4L





**SPECIAL CONTROL AREA ASSESSMENT SHEET**

APPLICATION DETAILS	
<b>APPLICATION No.</b>	DA23/0327
<b>PROPOSAL</b>	Single House (Swimming Pool, Gym and Outbuilding) - Special Control Area
<b>LOCATION</b>	Lot 16 No 6 Seaview Rise EAGLE BAY WA 6281
<b>APPLICANT</b>	Joe Ferrant
<b>OWNER</b>	C Leo, K Fallace, L Fallace
<b>DISCRIPTION OF PROPOSAL</b>	The proposal comprises a mainly two storey dwelling with a three storey section (where the garage in partiality below ground level on the northern elevation), as well as a separate single storey habitable building proposed to be used as a “gym” and another separate single storey games and change room, with pump room enclosed. The development also includes a below ground swimming pool and outbuilding.
<b>DISCRETIONS</b>	<p>The proposal complies with the requirement of LPS 21, Schedule 4 - Special Character Area as they apply to Eagle Bay.</p> <p>The site is subject to LPS 21, Special Provision 6 (SP6) and the Eagle Bay Structure Plan (2007). The development does not meet the following provisions of SP6 and the Eagle Bay Structure Plan.</p> <p><b><u>Setbacks</u></b></p> <p>The Structure Plan specifies a setbacks to Eagle Bay Road of 40 m. The development proposes a setback to Eagle Bay Road of 22m. It is noted that under Schedule 4 of the Scheme a rear setback of 6m is applicable.</p> <p><b><u>Building Height</u></b></p> <p>The Structure Plan and LPS 21 Special Provisions 6 specifies a maximum height limit at 7.5m. The maximum height of the proposal at its highest point is 8.7m and only on the northern elevation.</p>



**Views from Seaview Rise**

- **View of proposed site**



- **View of adjoining site - Lot 38 (4) Seaview Rise**



- **View of No. 9 Seaview Rise on the other side of the road from the site.**

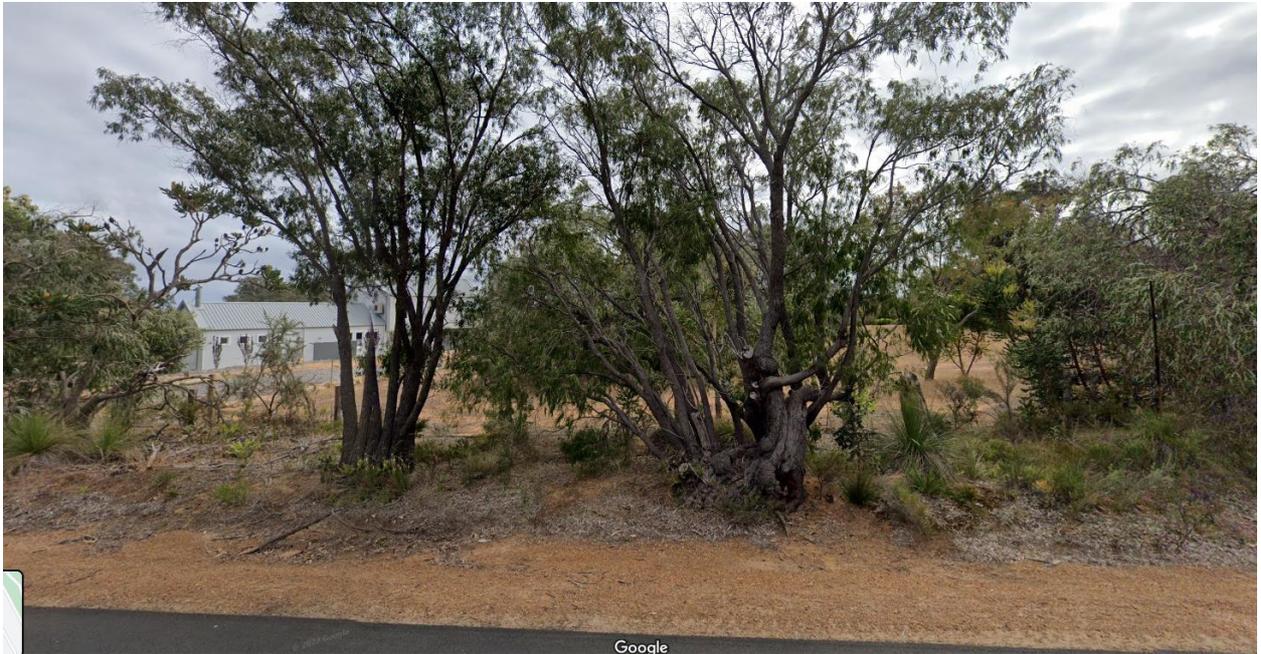


**Views from Eagle Bay Road**

- View of the site from Eagle Bay Road.



- View of site adjacent to Lot 38 (No.4) from Eagle Bay Road



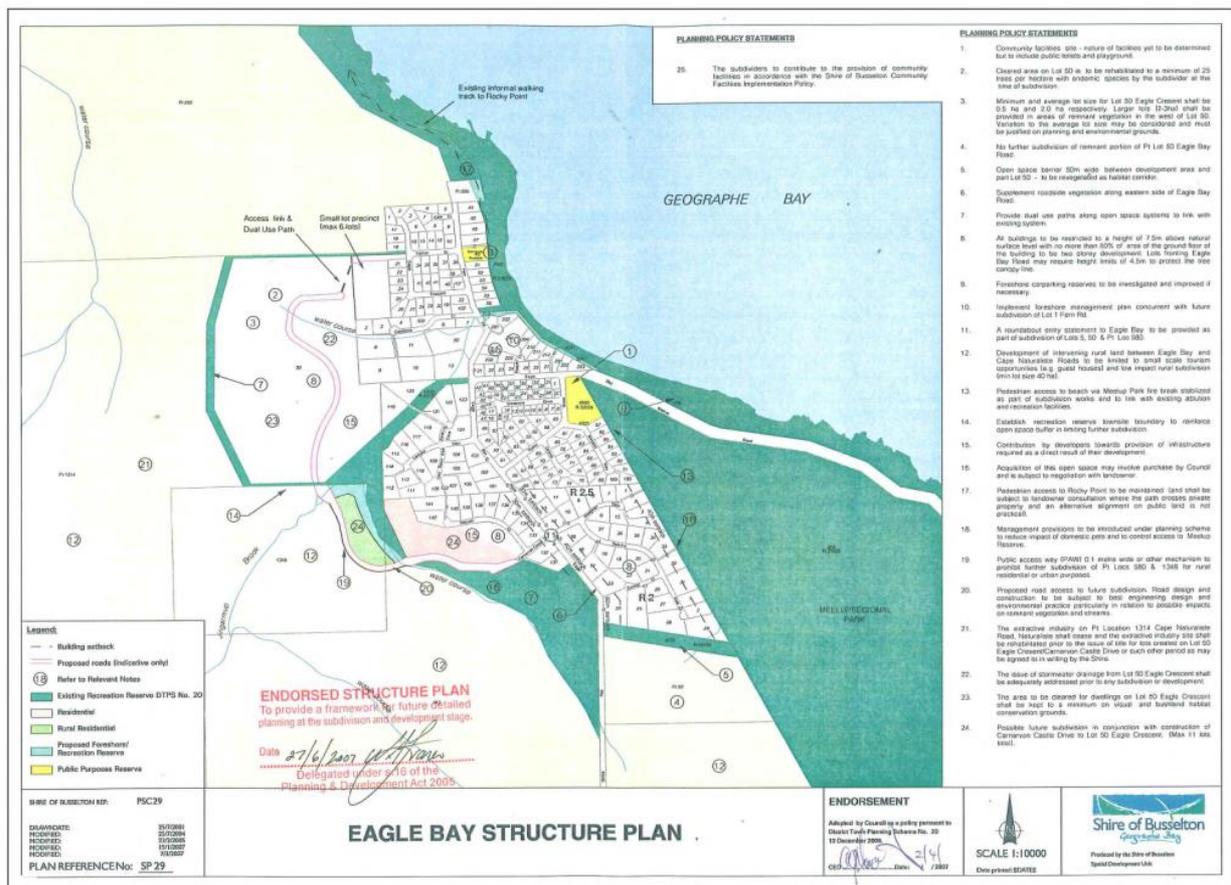
- View of lot adjacent to Lot 18 from Eagle Bay Road



<b>PROPERTY / DEVELOPMENT HISTORY</b>	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No - No relevant history on this site. Relevant site history relating to surrounding sites regarding setbacks is set out below
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SCHEME ASSESSMENT – ENSURE RELEVANT DISCUSSION PROVIDED WHERE ANSWERING “YES”			
<b>Zoning</b>	Residential	<b>Land use permissibility</b>	Single House - P
<b>Density</b>	R2.5	<b>Existing land use</b>	Vacant Lot
<b>Lot Area</b>	5,907m <sup>2</sup>	<b>Required FFL?</b>	None
<b>Does the site contain a building envelope?</b>		<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
<b>Is the site located within a Structure Plan/DGP/LDP area?</b>		<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No - Eagle Bay Structure Plan – See assessment under the Structure Plan below:	

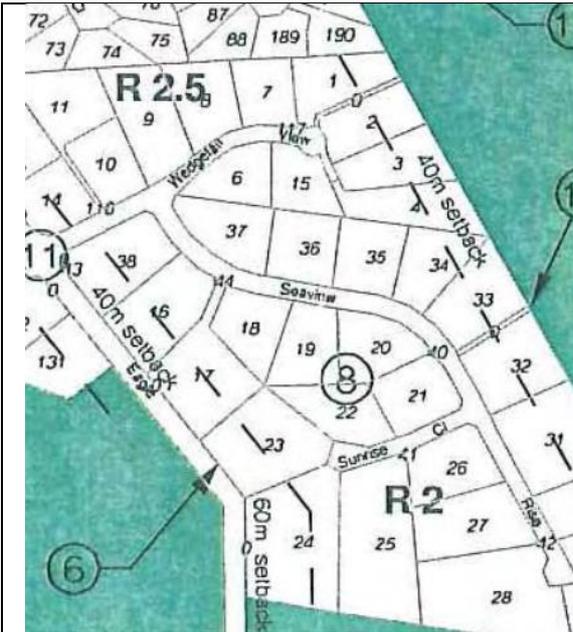
**Eagle Bay Structure Plan: 2007**



In relation to Building Height - Provision 8 of the Structure Plan states as follows:

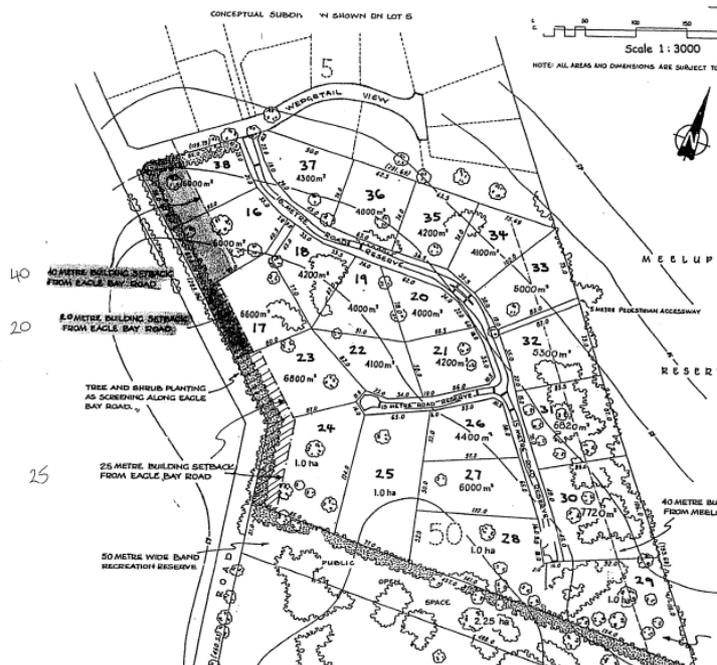
8. All buildings to be restricted to a height of 7.5m above natural surface level with no more than 50% of area of the ground floor of the building to be two storey development. Lots fronting Eagle Bay Road may require height limits of 4.5m to protect the tree canopy line.

In relation to setbacks, the structure plan map depicts a 40m setback from Eagle Bay Road for the four lots marked as 38, 16, 17 and 23.



The structure plan is a “due regard” planning instrument, therefore discretion to vary setback to 22m.

Previously endorsed Structure Plan (DGP 20) Seaview R:2003



DGP 20 shows variable setbacks for several lots adjacent to Eagle Bay Road (ie 20m, 25m, and 40m); the more recent structure plan has a uniform 40m setback. This has resulted in anomalies in the streetscape, with existing buildings having a <40m setback. Properties with <40m setbacks have implemented a landscaping buffer. A Landscaping plan has been provided and it provides an acceptable buffer.

## Officer Assessment in relation to setbacks to Eagle Bay Road

Aerial image below shows that the proposed setback (22m) is consistent with the existing streetscape



### SETBACK HISTORY ALONG EAGLE BAY ROAD.

Lot 38 (No.4) Seaview Rise adjoining the northwest of the proposed dwelling has reduced setbacks of 22m. A dwelling was approved under **DA18/0487** (single dwelling reduced setbacks) DGP 20 showed this Lot as requiring a 40m setback.

This setback line was adjusted when the single dwelling was approved under **DA18/0487** and a 22 metre setback agreed. Justified on the basis that the proposed setback was consistent with the existing streetscape. On this same site **DA21/0741** for garage studio and carport (reduced setbacks to 21.44 and 22.9) was approved. (Both additions are setback 22 metres from Eagle Bay Road.)

**PA18/0016** provided advice on reduced setbacks for Lot 38 (No.4). The advice states the following and is still relevant for this application:

*“should the building address Eagle Bay Road then a variation to the 40m front setback will be considered within the context of the prevailing streetscape, and other planning instruments in force. All other setback requirements comply. Should the building address Seaview Rise, a variation to the 40m secondary street setback will be considered within the context of the prevailing streetscape, and other planning instruments in force. All other setback requirements comply. Should the current plan be presented for planning approval, given the proposed setback variation it will be referred to adjoining landowners for comment. While it is noted that the south western boundary of the lot is well screened from Eagle Bay Road, the applicant is advised to provide a landscaping plan to support their application for a reduced setback”*

Lot 17 (No.8) Seaview Rise to the south east has a 22m setback - Approved - 2002

Lot 23 (6) Seaview Rise, located to the south of Lot 17 have tennis court and a water tank with reduced setback of 22 - 28m. Approved as an over height dwelling, 2004 **P3438**

Landscaping plan provided – Officer satisfied that the landscaping plan addresses the reduced setback with a mix of trees and plants endemic to SW of WA and will grow to heights sufficient for effective screening.

<b>What is the prescribed building height under the Structure Plan</b>	7.5m
--	------

**The Local Planning Scheme states:**

4.8.1 A person must not erect any building that -

- (a) contains more than two storeys or exceeds a height of 9 metres above natural ground level, where land is within 150 metres of the mean high water mark; or
- (b) contains more than three storeys or exceeds a height of 12 metres above natural ground level, where land is more than 150 metres from the mean high water mark, except where otherwise provided for in the Scheme.  
**(Amendment No. 50 – GG. 22 Jul 2022)**

[Refer Eagle Bay Structure Plan](#)

- 8. All buildings to be restricted to a height of 7.5m above natural surface level with no more than 50% of area of the ground floor of the building to be two storey development. Lots fronting Eagle Bay Road may require height limits of 4.5m to protect the tree canopy line.

[Refer Planning and Development \(Local Planning Schemes\) Regulations 2015](#)

In accordance with the *Planning and Development (Local Planning Schemes) Regulations 2015, Schedule 2, clause.67 (Consideration of application by local government)* and *clause 27(1) of the Deemed Provisions*: the structure plan is a “due regard” planning instrument only, therefore there is discretion to vary any provisions contained within the structure plan.

## **Officer Assessment in relation to building height**

The development proposes a maximum height in accordance with the requirements of Schedule 4 of the Scheme, 3.3 Eagle Bay SCA. While Schedule 4 makes reference to the Eagle Bay structure, in accordance with the above a structure plan is a matter for due regard only and therefore there is discretion to vary the provisions contained within the structure plan.

The site naturally slopes 4m up from the front boundary, at Seaview Rise, up to the rear boundary at Eagle Bay Road (from the that north of the site to the south of the site). The development is cut into the site with approximately 1m of the garage/lower floor proposed to be below natural ground level. As a result, from the primary street, the dwelling will present as a two storey façade only. The only elevation that will present as a three storey is the northern elevation and the applicant has provided a landscaping plan which includes trees along the northern lot boundary which will soften the visual impact of the dwelling from this neighbouring property.

It is important to note that only a section of the northern elevation will be 8.7m high, the rest of the building height is below 8.7m with heights ranging from 4.2m, 6.5m, 7.1m, 7.3, 7.5, 7.7m, 8.2m and 8.3m. Therefore the proposal is addressing the sites natural slope, and for the most part is a 2 storey dwelling on most elevations and is only considered 3 storey where the garage is located.

It is considered that due to the design, layout, sloping site and extensive landscaping plan proposed the height of the building is acceptable. The highest point of the dwelling is set back from neighbours to the north by 15.4m which is considered an adequate distance and will not result in overlooking or over shadowing. The height points of the dwelling (elevations over 7.5m) are setback from Eagle Bay Road by approximately 54m and setback from Seaview Rise by approximately 22m-37m.

It is considered that a 7.5m high limit is not required on this site, the design, location and landscaping sustainably integrates the proposed dwelling on to this site without detrimental impact on the character of the area or residential amenity, which are considered to be the key outcome to be achieved via the clauses set out in the Eagle Bay Structure Plan.

It is demonstrated below that, in line with the Structure plan, no more than 50% of area of ground floor of the building to be two storey development.

It is considered that due regard has been given to the Eagle Bay Structure Plan and as such the propped building height and setbacks to Eagle Bay Road, on this occasion, are acceptable and will acceptably protect the prevailing amenity of the locality.

See detailed height assessment below:

The lot levels taken from the site survey and as such are used for all building height calculations as per the definition of *Natural Ground Level* from the R-Codes. (See table below for height calculations)

Relevant information:

**“Natural ground level”**

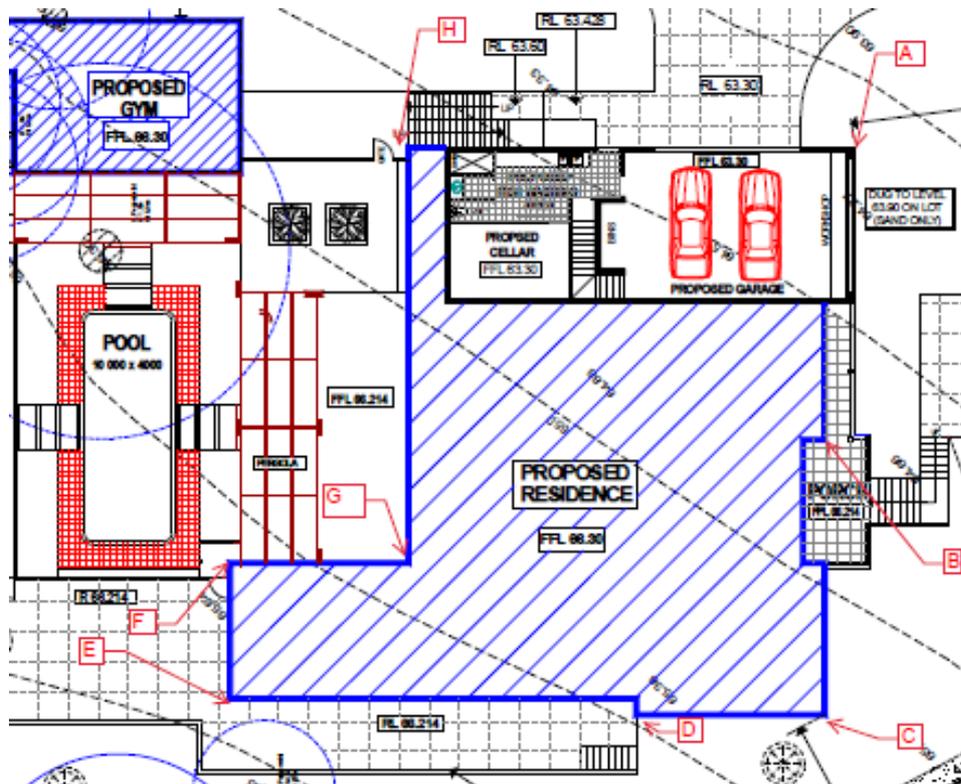
The levels on a site which precede the proposed **development**, excluding any site works unless approved by the **decision-maker** or established as part of subdivision of the land preceding development.”

The site is over 150 metres (Approx. 730m) from the MHWM. The MHWA is defined by the scheme as follows:

**“Mean High Water Mark”** means the demarcation line shown on the Scheme map as provided by Landgate on the day of 22 June 2015, that identifies the interface of the ocean and land, and shall exclude any demarcation of natural inland water systems or man-made harbours/canals.

(Amendment No. 1 – GG. 4 Aug 2017)

**Survey Plan**



**Extract from Plans – Annotated Building Height**

**Table of Building Height Calculations**

POINT	NGL	FIRST FLOOR FFL	ROOF RL	MAXIMUM HEIGHT PROPOSED	COMPLIES
A	63.9m	69.129m	72.6m	8.7m	Yes
B	64.5m	69.129m	72.65m	8.15m	Yes
C	65.2m	69.129m	72.82m	7.62m	Yes

D	65.5m	69.129m	75.52m	7.32m	Yes
E	66.0m	69.129m	72.5m	6.5m	Yes
F	65.5m	69.129m	72.3m	6.5m	Yes
G	65.5m	69.129m	72.5m	7.4m	Yes
H	64.5m	69.129m	72.8m	8.3m	Yes

The entire lot is subject to the 8.7m building height limit and as demonstrated above is compliant.

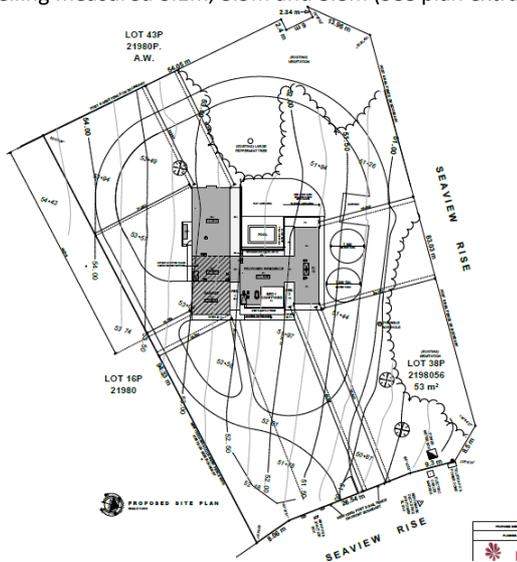
The Structure Plan states that no more than 50% of area of the ground floor of the building to be two storey. The habitable building consist of a main residences outdoor living area/games room and gym. The areas are broken down as follows:

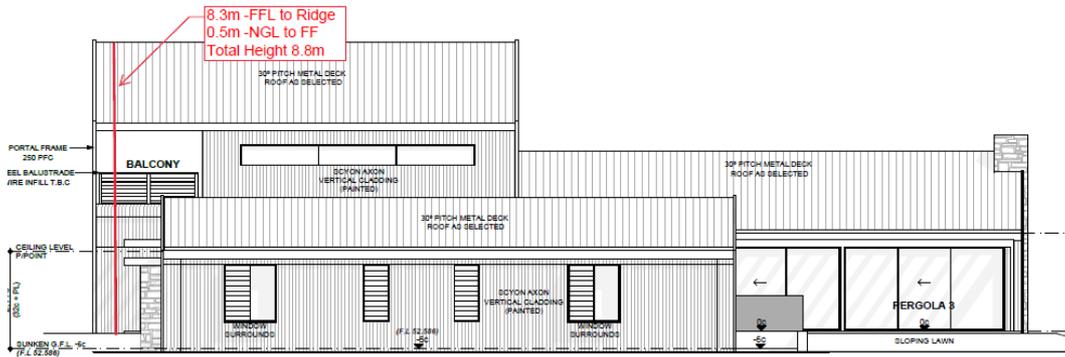
Areas	
Garage/cellar	114.83m <sup>2</sup>
<b>Ground Floor</b>	<b>402.89m<sup>2</sup></b>
Porch/balcony	86.15m <sup>2</sup>
Gym	69.00m <sup>2</sup>
Outdoor House	80.00m <sup>2</sup>
Stores/Pump rm	31.21m <sup>2</sup>
Outdoor Garage	227.54m <sup>2</sup>
<u>Total Ground Floor/</u>	<u>1011.62m<sup>2</sup></u>
Cellar	
<b>Upper Floor</b>	<b>224.52m<sup>2</sup></b>
Top Deck	120.42m <sup>2</sup>
<u>Total Upper Floor</u>	<u>344.94m<sup>2</sup></u>
<b>Total Area</b>	<b>1356.56m<sup>2</sup></b>

As such it is considered that the two storey/upper floor area of 224m<sup>2</sup> is not more than 50% of the ground floor of this property.

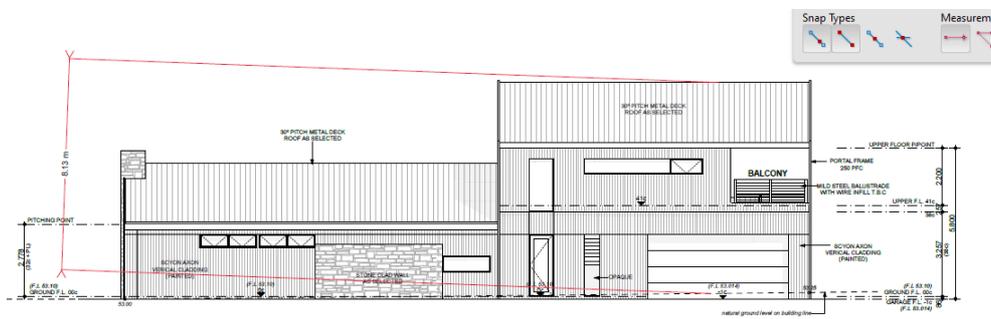
**Examples of dwelling approved in Eagle Bay over 7.5m**

- A. Lot 38 (No.4 Seaview Rise), adjoining the north/ northwest boundary of the Site has a varied building height. A dwelling was approved under DA18/0487 (single dwelling reduced setbacks & over 7.5m) at its highest points the dwelling measured 8.1m, 8.5m and 8.8m (See plan extract below).

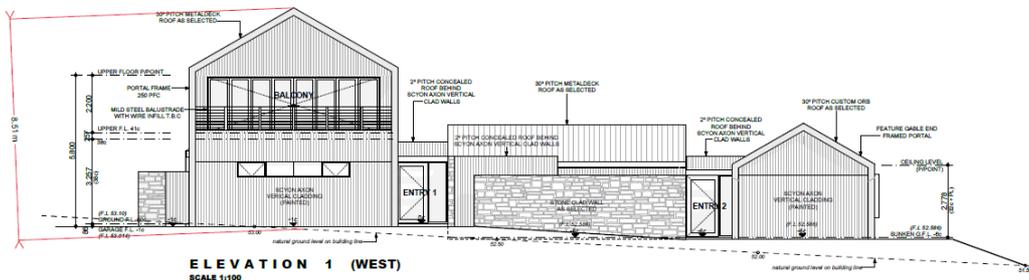




**ELEVATION 4 (NORTH)**  
 SCALE 1:100

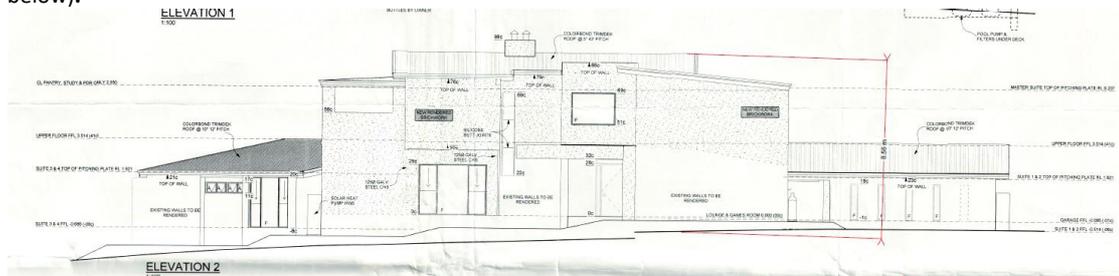


**ELEVATION 2 (SOUTH)**  
 SCALE 1:100

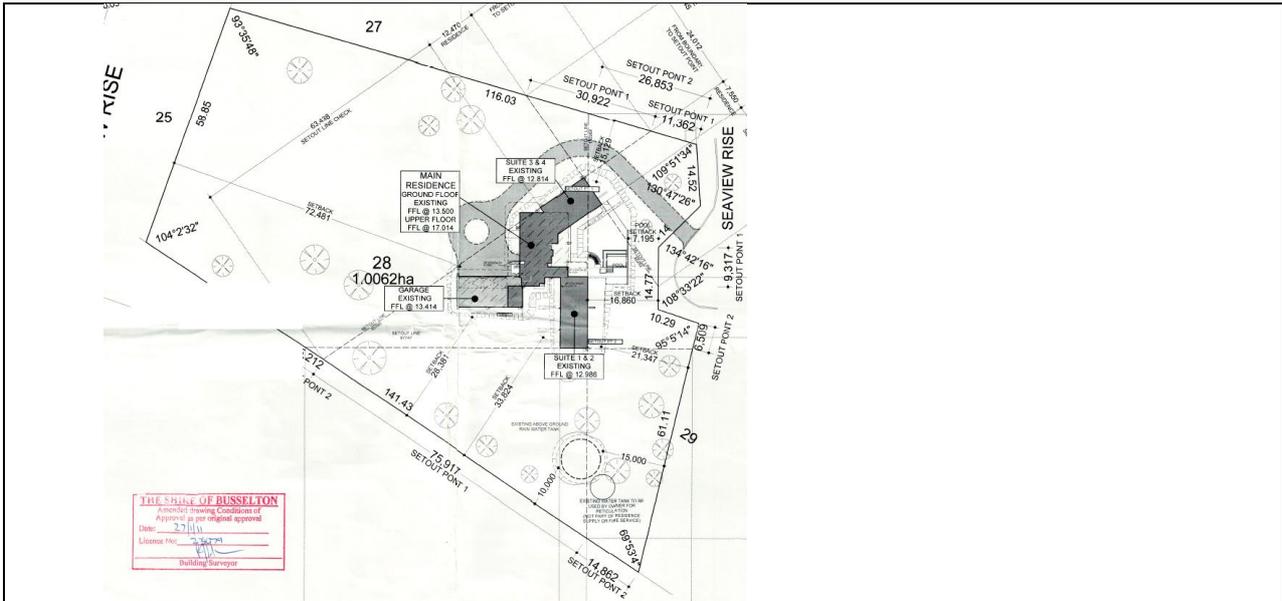


**ELEVATION 1 (WEST)**  
 SCALE 1:100

- B. Lot 28 (No. 22 Seaview Rise) has been approved with a varied building height. A dwelling was approved 2007 under P1211, later amended in 2011. At its highest point the dwelling measures 8.6m approximately (see plan extract below).



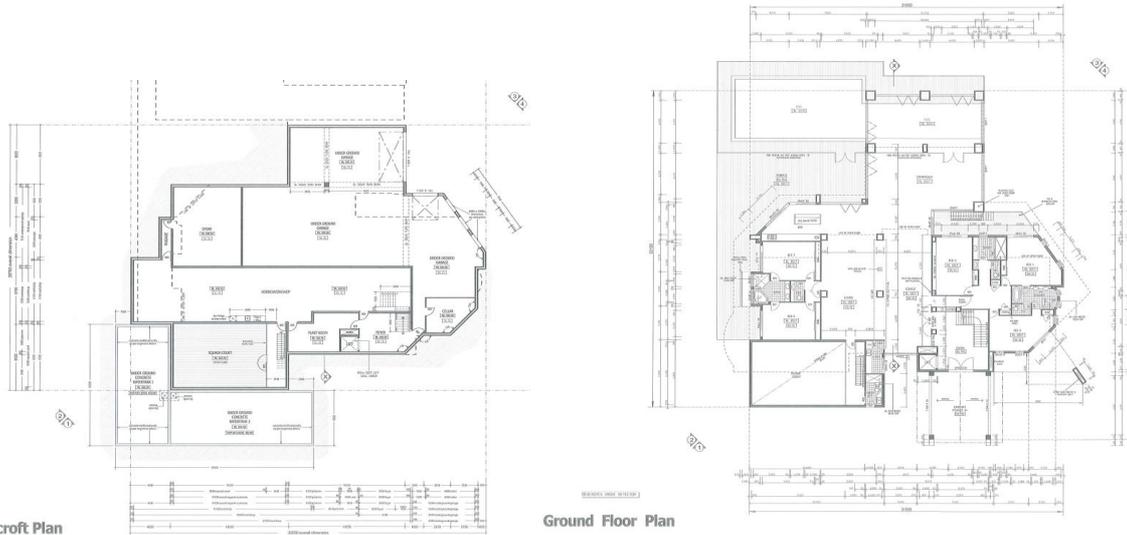
THE SHIRE OF RUSSELLTON  
 Approved Drawing, Council of  
 Date: 27/1/11  
 Licence No: 254675  
 Building Supervisor

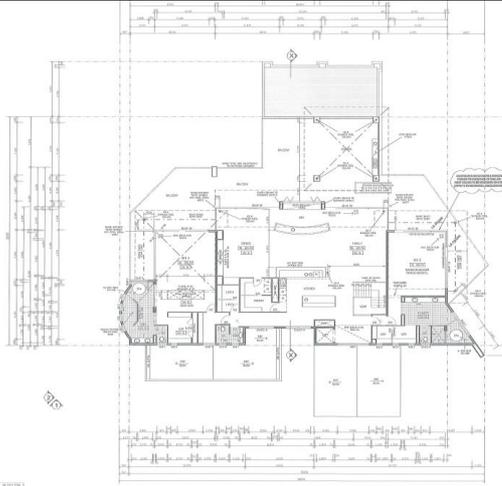


Furthermore, it is noted that three storey dwellings are not uncommon in the Eagle Bay SCA

**Examples of three storey dwellings in Eagle Bay SCA**

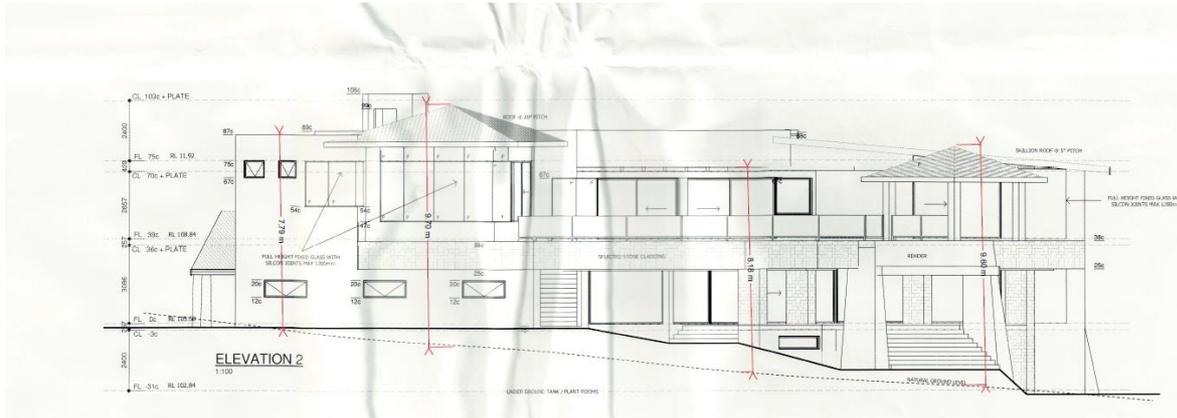
- A. Lot 15 (No. 4) Wedgetail view. The three storey dwelling was approved in 2003, due to the slope of the site basement level was largely cut in to the site (see plan extract below).

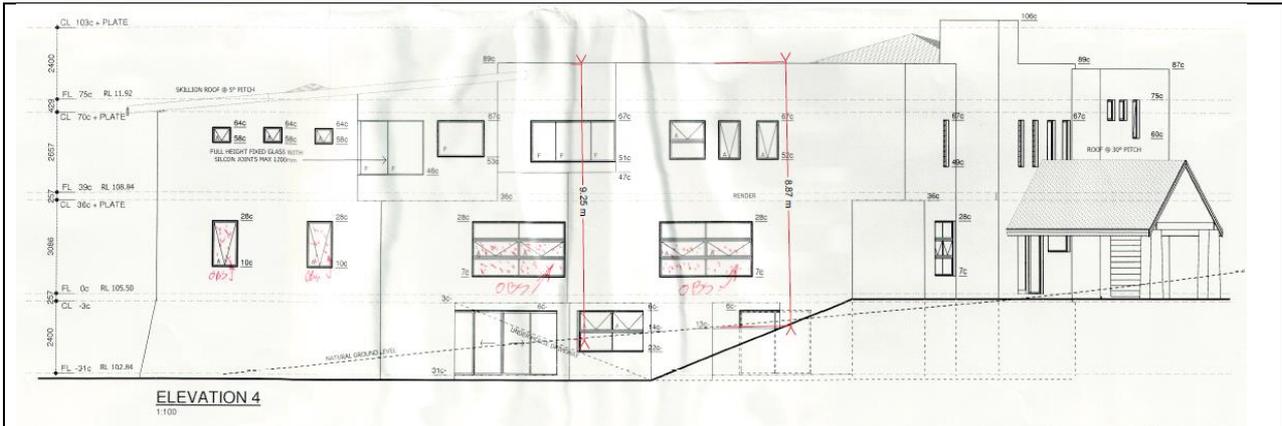




Upper Floor Plan

B. Lot 67 (No.16) Gypsy Street is a three storey dwelling that was approved in 2008, and due to the slope of the site it has been design to address the natural contours of the site (the site slope from the north to the south, 101 AHD to 107AHD). Due to the contours of the site some section of the elevations are greater than 7.5m and some section of elevation are in excess of 9m high (see plan extract below).





C. Lot 69 (No.1) Otranto Close is a three storey dwelling that was approved in 2007, this dwelling has also been design to address the contours of the site. See photo below.



<p><b>Is the site a designated contaminated site?</b></p>	<p><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p>
<p><b>Is the site in a Special Control Area?</b></p>	<p><input checked="" type="checkbox"/> Yes <input type="checkbox"/> No Special Provision Area 6 Eagle Bay Special Character Area</p>

	Landscape Value Area
Is the site subject to Easements	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
Is the site within the BOB/Vasse Dunsborough Alignment	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No

**LOCAL PLANNING SCHEME**

Relevant Clauses	Complies (YES/NO) & Comments
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**3.2 ZONE OBJECTIVES**

<p>The objectives of each zone are as follows –</p> <p><b>Residential</b></p> <p>a. To provide for a range of housing and a choice of residential densities to meet the needs of the community.</p> <p>b. To facilitate and encourage high quality design, built form and streetscapes throughout residential areas.</p> <p>c. To provide for a range of non-residential uses, which are compatible with and complementary to residential development.</p>	<p>Satisfied.</p> <p>Proposed residential dwelling consistent with the objectives of the residential zone.</p>
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**4.3 Modification of R-Codes**

<p>(g) On land coded R2, R2.5 or R5, all building development is to be contained within a building envelope area which does not exceed 1,000m<sup>2</sup>, or as otherwise required by a plan adopted pursuant to Part 4 of the Deemed Provisions.</p>	<p>The main intent of Clause 4.3 (g) relates to the development of land prior the subdivision of land.</p> <p>In this instance the land has been subdivided and the plan adopted pursuant of Part 4 Deemed Provisions is the Eagle Bay Structure Plan.</p> <p>As explained earlier in this report the officers must give due regard to the Structure Plan, but do have discretion to vary it. As such we have reviewed the structure plan and as set out above. The structure plan has allocated no Building Envelop (BE) for this property. The 2003 and 2007 Structure Plans has no BE allocated for this property.</p> <p>Therefore in order to assess the appropriateness of this building we have assessed the area of building development to be 1194m<sup>2</sup>. Although 194m<sup>2</sup> over the 1000m<sup>2</sup> we have considered, following the R-Code assessment, that it is appropriate to allow discretion in this instance for the following reasons:</p> <ul style="list-style-type: none"> <li>• The R-Codes requires 80% open space, the proposal provides 86%. A large area of open space will remain undeveloped on this site.</li> <li>• The setback to neighbouring properties area as follows:  <u>Side (North)</u>  DTC - 7.5m  <b>Proposed – 10m</b>  <b>Complies</b></li> <li>• <u>Side (South)</u>  DTC – 7.5M  <b>Proposed – 16m</b>  <b>Complies</b></li> <li>• This is considered appropriate as all other aspect, including open space and setbacks, comply with the deemed-to-comply criteria of the R-codes.</li> </ul>
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	<p>Following the above it has been demonstrated that the proposal fully complies with the intent of Clause 4.3 (g), even though the Structure Plan has demonstrated that no Building Envelop was required.</p>
<b>4.12 EFFLUENT DISPOSAL</b>	
<p>4.12.1 If no reticulated sewerage system is available, development approval shall not be granted for development in excess of a single house unless -</p> <ul style="list-style-type: none"> <li>(a) adequate and appropriate provision can be made for the on-site disposal of effluent, having particular regard to the comments or requirements specified by the Department of Health; or</li> <li>(b) the development replaces an existing lawful development and redevelopment does not represent an increase of more than 10% over and above the density of the existing development.</li> </ul> <p>4.12.2 If no reticulated sewerage system is available, liquid and/or solid wastes shall be disposed of on-site by means of an effluent disposal system approved by the local government. No house shall be occupied without the prior approval and installation of such a disposal system.</p> <p>4.12.3 In the Rural Landscape zone effluent disposal areas shall be set back a minimum of 30 metres from any watercourse or dam or as otherwise approved by the local government.</p> <p>4.12.4 In the Conservation and Bushland Protection zones effluent disposal areas shall be set back a minimum of 50 metres from any watercourse or dam or as otherwise approved by local government.</p>	<p>Satisfied.</p> <p>Reticulated sewer not available. Septic system proposed</p> <p>Refer EHO comments below.</p> <p>Noted</p> <p>Noted</p> <p>Noted</p>
<b>4.31 CLEARING OF LAND IN RURAL AREAS OR LAND CODED R2, R2.5 OR R5</b>	
<p>4.31.1 On land coded R2, R2.5 or R5 or located in the Rural Residential, Rural Landscape or Bushland Protection zones, vegetation may only be felled, removed or damaged when –</p> <ul style="list-style-type: none"> <li>(a) associated with implementing approved development;</li> <li>(b) essential for achieving adequate fire protection; or</li> <li>(c) the vegetation is dead, dying or dangerous.</li> </ul>	<p>Satisfied.</p> <ul style="list-style-type: none"> <li>(a) Approximately 5 trees will have to be removed for the development of this dwelling. Given the setback to Eagle Bay Road will be reduced, a landscaping plan has been requested along the rear boundary. The planting proposed in the landscaping will adequately compensate the removal of trees from the site.</li> <li>(b) Ongoing modification of vegetation is essential in accordance with the City’s Firebreak &amp; Fuel Hazard Reduction Notice (Category 2).</li> <li>(c) N/A</li> </ul>
<b>4.8 HEIGHT OF BUILDINGS</b>	
<p><b>4.8 HEIGHT OF BUILDINGS</b></p> <p>4.8.1 A person must not erect any building that –</p> <ul style="list-style-type: none"> <li>(a) contains more than two storeys or exceeds a height of 9 metres where land is within 150 metres of the mean high water mark; or</li> <li>(b) contains more than three storeys or exceeds a height of 12 metres where land is more than 150 metres from the mean high water mark, except where otherwise provided for in the Scheme.</li> </ul>	<p>Refer previous assessment above.</p>

<p>4.8.2 Notwithstanding clause 4.8.1, the local government may grant approval for the development of a building containing more than two or three storeys, as the case may be, provided that the additional storey or storeys are of the nature of a basement or similar structure and that they do not protrude more than 1 metre above finished ground level at the perimeter of the building.</p> <p>4.8.3 In respect to clauses 4.8.1 and 4.8.2 above, the local government, upon receipt of an application for development approval, may approve building heights which exceed those maximum height limitations as specified, subject to the local government being satisfied that the building height is consistent with the relevant assessment criteria specified under clause 67 of the Deemed Provisions and the performance criteria specified under 5.1.6 (P6) and 6.1.2. (P2) of the R-Codes.</p>	
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<b>5.4 LANDSCAPE VALUE AREA</b>
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<p>5.4.2 The local government shall not grant development approval for the clearing or development of any land identified within a Landscape Value area on the Scheme map, unless it has considered –</p> <p>(a) whether the development will be compatible with the maintenance and enhancement, as far as is practicable, of the existing rural and scenic character of the locality;</p> <p>(b) whether the development will materially affect any wildlife refuge, significant wetland, coastal environment or any identified site containing Aboriginal archaeological relics; and</p> <p>(c) disturbance to the natural environment, including -</p> <p style="margin-left: 20px;">(i) visual effects of clearing for development;</p> <p style="margin-left: 20px;">(ii) maintenance of rural character; and</p> <p style="margin-left: 20px;">(iii) habitat disturbance.</p>	<p>Satisfied.</p> <p>(a) The proposed dwelling design, scale and appearance is considered compatible with the scenic character of the locality. The character of this part of Eagle Bay on the newer lots is mixed with modern style architecture of a range of single storey and two storey designs. It is consistent with other designs and built form in the area.</p> <p>(b) No impact to wildlife, environment etc...</p> <p>(c) This is a cleared site with the most vegetation along Eagle Bay Road and the boundary with numbers 8 &amp; 10 to the south. Some clearing will be required to accommodate the proposed dwelling which is permissible in this zone.</p> <p>A detailed landscaping plan has been provided this will provide adequate screening between the built form and Eagle Bay Road.</p> <p>A balance has been achieved between maintaining vegetation within the lot whilst considering the requirements of planning in bushfire prone areas. In a letter dated 12 October 2023, the bushfire consultant has confirmed the following:</p> <p><i>“This is to certify that the Landscape management plan dated 11/10/2023, for Lot 16, #6 Seaview Rise, will not impact on the current BAL rating of BAL – 19. The plan complies with State Planning Policy 3.7 – Planning in Bushfire Prone Areas.</i></p> <p><i>No trees are located within 6m of the residence and no limbs overhang the structure. The firebreak is maintained around the site, complying with the City rating of “Category 2”.</i></p> <p><i>The plan indicates ongoing maintenance of the firebreak.</i></p> <p><i>Proposed plantings appear to be “Firewise” species.”</i></p>
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	<div style="display: flex; justify-content: space-between; align-items: center;"> <div style="font-size: small;"> <p>Bushfire Works A.B.N. 945 23123 843 PO Box 1249, Bibra Lake DC WA 6965 E: david.deeley@bushfireworks.com.au www.bushfireworks.com.au</p> </div> <div style="text-align: right;">  </div> </div> <p style="text-align: center;"><b>BAL Rating Confirmation</b> <b>DA23 / 0327 Landscape Management Plan</b></p> <p>This is to certify that the Landscape management plan dated 11/10/2023, for <b>Lot 16, #6 Seaview Rise</b>, will not impact on the current BAL rating of BAL – 19. The plan complies with State Planning Policy 3.7 – <i>Planning in Bushfire Prone Areas</i>.</p> <p>No trees are located within 6m of the residence and no limbs overhang the structure.</p> <p>The firebreak is maintained around the site, complying with the City rating of “Category 2”. The plan indicates ongoing maintenance of the firebreak.</p> <p>Proposed plantings appear to be “Firewise” species.</p> <div style="text-align: center;">  </div> <p style="text-align: center;">Dr David Deeley 12<sup>th</sup> October 2023</p>
<p>5.4.3 The local government shall not grant development approval for the carrying out of development on land within the Landscape Value area or on land on or near any ridgelines where, in the opinion of the local government, that development is likely to substantially detract from the visual amenity of the area, having regard to, among other things, the cumulative visual effect of the development related to other development that may be anticipated in the locality and in the area generally.</p>	<p>Satisfied.</p> <p>Site rises from the north to the south, as shown on the survey plan (from 6.3 AHD to 6.7 AHD).</p> <p>The proposed dwelling is not on or near any ridgeline and the requisite setback distance from Eagle Bay Road and Seaview Rise.</p> <p>It is considered the design, siting and scale of the proposed development will not detrimentally impact the visual amenity of the area.</p> <p>The landscaping plan will help also to integrate the proposal in to the area further protecting visual amenity.</p>
<p>5.4.4 Before granting development approval for the erection of a building on land within the Landscape Value area, the local government shall make an assessment as to whether it should impose conditions relating to –</p> <ul style="list-style-type: none"> <li>(a) the siting of the proposed building;</li> <li>(b) the use of prescribed materials on the external surfaces of the building; and</li> <li>(c) the number, type and location of existing trees and shrubs which are to be retained and the extent of landscaping to be carried out on the site.</li> </ul>	<p>Satisfied.</p> <ul style="list-style-type: none"> <li>a) The siting of the proposed development is considered appropriate to achieve the required setbacks as per Schedule 4. (see assessment below)</li> <li>b) Nominated materials and colours on external surfaces are acceptable. They are in keeping with that which is already existing in this area.</li> <li>c) Landscaping plan complies provides adequate screen while maintain the BAL as assessed.</li> </ul>
<b>SCHEDULE 4 – SPECIAL CHARACTER AREAS - EAGLE BAY</b>	
<p>The following provisions shall apply to subdivision and development within the Eagle Bay Special Character Area as identified on the Scheme map –</p> <ul style="list-style-type: none"> <li>(a) Other than provided for in a Structure Plan, Approved Building Envelope Plan or Structure Plan, development within the various planning sectors, as outlined in the Eagle Bay Special Character Area Local Planning Policy, shall comply with the following setback requirements:</li> </ul>	<p>Satisfied.</p> <p><b>EBSCAP Sector 3, R2.5 Density.</b></p> <p><b>Front</b> Required = 20.0m Provided = 22.8m</p> <p><b>Rear</b> Required = 6m Provided = 22m (However, this does not accord with the 40m setback required in the structure plan. See assessment above where a reduction to a 22m setback has been deemed acceptable in this location)</p>

Table 1 - Setbacks by Sector	Density	Minimum Setbacks for Development (in metres)		
		Front	Rear	Secondary Street
Sector 1	R5 Density	9.0*	6.0	4.5
Sector 2	R5 Density	15.0	6.0	6.0
	R2 Density	15.0	10.0	10.0
Sector 3	R2.5 Density	20.0	6.0	12.0
	R2 Density	20.0	10.0	12.0
Sector 4	Rural Residential	As per Structure Plan	As per Structure Plan	

*\*In determining the appropriate front setback, consideration shall be given to the setback of dwellings on neighbouring/nearby properties and the impact a 9.0 m front setback may have on the existing streetscape.*

(b) Incidental development shall be setback from front and neighbouring boundaries, including boundaries which interface with coastal foreshore and other public reserves, to protect the prevailing amenity of the locality in accordance with Table 1.	Complies - Septics and front steps where moved to site behind the 20m setback line
(c) Where a lot abuts a foreshore or public open space reserve, the setback applicable to development shall be no less than the required front setback distance in order to retain the natural amenity of the Reserve and avoid the encroachment of development on the reserve area.	N/A
(d) The applicable height limits will be 7.5 metres within 150 metres of the mean high water mark and 9.0 metres for all other areas unless otherwise determined by zoning provisions, a Structure Plan.	Complies Refer previous assessment above.

SPECIAL PROVISION 6 – LOT 5 & PORTION OF LOT 50 EAGLE BAY RD, EAGLE BAY			
Provision			Complies (YES/NO) & Comments
SP6	Lot 5 & Portion of Lot 50 Eagle Bay Rd, Eagle Bay	Residential (R2 and R2.5)	<p>1. Subdivision and development of the lots shall generally be in accordance with the Structure Plans adopted by the local government and approved by the WA Planning Commission.</p> <p>2. No further subdivision of lots will be supported by the local government.</p> <p>3. The standard minimum building setbacks shall be 15 metres from the front boundary and 10 metres from all other boundaries where setbacks are not specified on the Structure Plan.</p> <p>4. No building shall be constructed closer to Eagle Bay Road than the setback line shown on the Structure Plan, with the exception of Lots 12, 13 and 14 which shall have a minimum setback of 30 metres from Eagle Bay Road.</p> <p>5. No effluent disposal area or buildings on lots abutting Meelup Reserve shall be set back less than 40 metres from the common boundary within Meelup Reserve.</p> <p>6. All outbuildings are to be contiguous with the main dwelling in terms of building materials, colour and form and shall be located within the building envelope indicated for each lot on the Subdivision Guide Plan and shall be limited to single storey development.</p> <p>7. Unless otherwise approved by the local government, each dwelling shall be connected to a water storage tank of not less than 135,000 litre capacity. The total catchment for the tank shall be not less than 150 m<sup>2</sup>. Water tanks shall be fitted with couplings compatible with Department of Fire and Emergency Services requirements so that the total tank capacity is available for fire fighting purposes at any given time.</p> <p>8. Rain water storage tanks are to be sited within the building envelope and form part of the dwelling and outbuilding group. Such tanks shall be painted mist green or similar and screened with vegetation to the satisfaction of the local government.</p> <p>9. The local government shall require each application for a building licence to be accompanied by a landscaping plan for the lot unless</p>
			<p><input checked="" type="checkbox"/> Yes <input type="checkbox"/> No</p> <p>1. N/A – Existing established lot.</p> <p>2. N/A – No subdivision proposed by this DA.</p> <p>3. Schedule 4 takes precedent for the front and rear setbacks. Side (north) setback: 10m required, 10m provided Side (south) setback: 10m required, 11m provided</p> <p>4. <a href="#">The Eagle Bay Road Setback has been reduced to 22m – see justification in sections above.</a></p> <p>5. N/A – Lot not located on or within 40m of Meelup Reserve.</p> <p>6. Complies.</p> <p>7. Lot is located on reticulated water, as such it is not considered necessary to install a water tank.</p> <p>8. Refer 7.</p> <p>9. Noted.</p> <p>10. N/A – New build</p> <p>11. The Structure Plan and LPS 21 Special Provisions 6 specifies a maximum height limit at 7.5m. The maximum</p>

			<p>such lot is substantially vegetated, indicating the areas to be subject to tree planting and revegetation in accordance with the Structure Plan.</p> <p>10. Pre-fabricated kit homes, relocated and transportable dwellings are not permitted.</p> <p>11. Dwelling houses must be limited to a maximum height of 7.5m above the natural ground level at any point, with any second storey (first floor) to comprise a gross floor area of not more than 50% of the gross ground floor area. On Lots 12, 13 and 14 the maximum dwelling height shall be limited to the general tree canopy height on the lot and in any event must not exceed 7.5 m.</p>	<p>height of the proposal at its highest point is 8.7m and only on the northern elevation, 0.1m lower than the neighbouring property. Impacts of the variation have been assessed using the planning framework. The variation is considered acceptable provided the design, location and landscaping sustainably integrate the proposed dwelling on to this site without detrimental impact on the character of the area or residential amenity, which are considered to be the key outcome in the Eagle Bay Structure Plan. Furthermore, it is noted that three storey dwellings are not uncommon in the Eagle Bay SCA. Complies with LPS 21, Clause 4.5. <i>(Clause 4.5 of the Scheme provides discretion to vary site and development standard and requirements within the Scheme and it is considered that the provisions contained within Special Provision Special Control Areas are therefore able to varied).</i></p>
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**Officer Comments:** It is considered that the variation of the special provision (Clause 4) is acceptable to achieve appropriate siting of the proposed development. Refer to Clause 67 matter to be considered below in this report.

The proposed location of the development is consistent with Schedule 4 and the variation to the special provision is considered acceptable.

**LOCAL PLANNING POLICY**

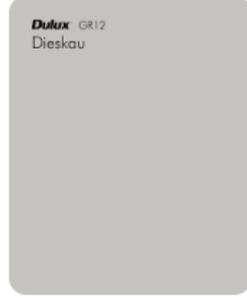
Local Planning Policy applicable?	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No
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Relevant Policies	<b>Complies (YES/NO) &amp; Comments</b>
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**LOCAL PLANNING POLICY – 3.1 REFLECTIVE BUILDING MATERIALS**

No Zinalume permitted within Residential, Tourist, Special Purpose or Rural Residential zone (includes residential components of the Vasse Development Zone, Yalyalup Development zone and other Development Zones) without an application.	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A Complies zinalume not proposed
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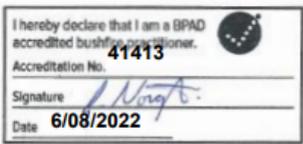
Reflective materials are not permitted within the Landscape Value Area and the Travel Route Corridors without an application.  <i>Note: Please make sure colours are nominated before approval is issued</i>	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A Nominated colours:  <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No <b>Materials:</b> Roof - Colorbond Steel Walls – Brick Masonry Render <b>Colours:</b> Roof – Monument
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	 <p>Dulux C29 Colorbond® Monument®</p> <p>Walls - Dieskau</p>  <p>Dulux GR12 Dieskau</p> <p>Feature wall – Brick/Neutral</p> <p>Materials and colours to be used comply with the requirements of this area and in keeping with the local Character.</p>
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**LPP 3.3 – EAGLE BAY SPECIAL CHARACTER AREA (PRECINCT 3)**

<p><b>Objectives for Eagle Bay</b></p> <p>The City’s overall aim is to ensure that any future development complements the positive character and attributes of the area. In particular the objectives are to:</p> <ol style="list-style-type: none"> <li>a) Conserve the coastal/rural setting of Eagle Bay.</li> <li>b) Ensure that the immediate surrounds of Eagle Bay remain set in retained natural vegetation provided by reserves, partially cleared rural land and protected fringing dune system.</li> <li>c) Conserve the quality of the clean beaches, unpolluted ocean water, diversity of bird life and other native fauna.</li> <li>d) Contain the residential development within the natural amphitheatre setting.</li> <li>e) Retain the non-urban beachside character and intimate scale.</li> <li>f) Maintain the low overall density of development and the well-vegetated environment with natural bush running all the way to the beach.</li> <li>g) Retain and enhance the balance between natural landscape over and the built environment throughout Eagle Bay.</li> <li>h) Ensure that the low impact of built form and facilities (both private and public) have minimal impact on the natural environment.</li> <li>i) Improve pedestrian movement throughout the settlement and maintain low traffic volumes via a non-through road traffic environment.</li> <li>j) Provide for development of a low rise residential character in the foreshore areas to reduce the dominance of built form in the foreshore localities.</li> </ol>	<p>Satisfied.</p> <p>The design, siting and appearance of the proposed single house complements the established character and attributes of the area.</p>
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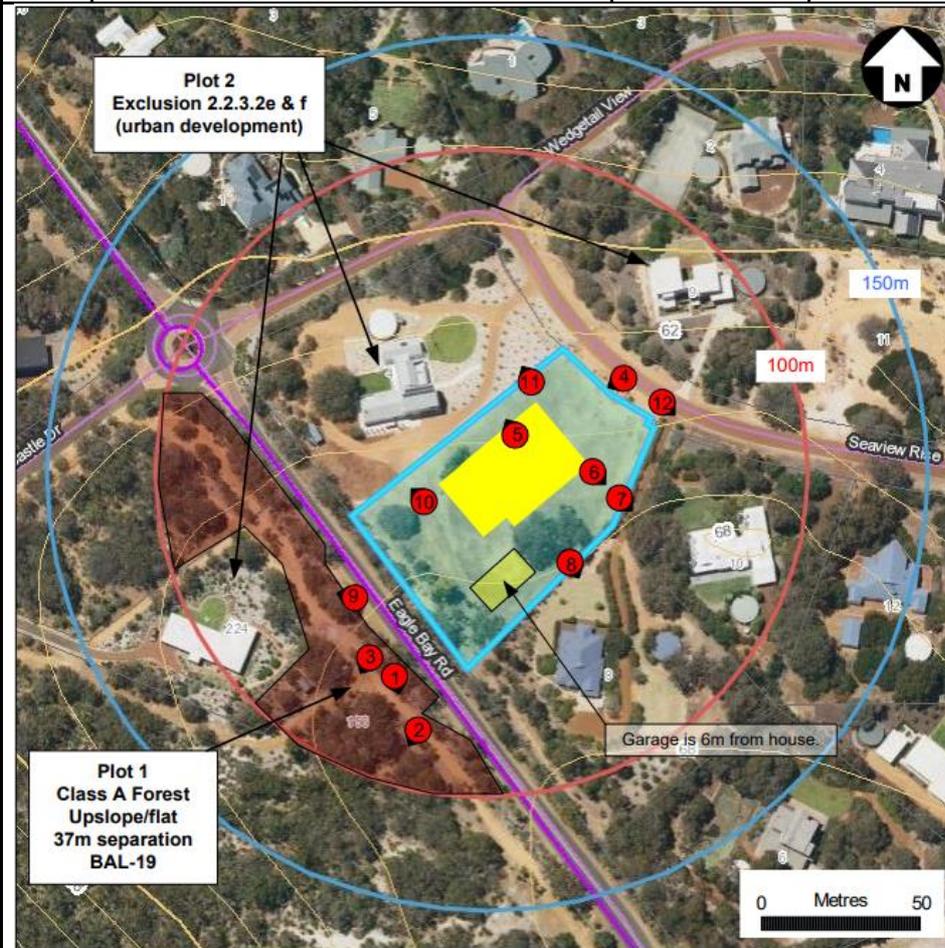
k) Ensure that the development proposals for each sector take account of the characteristics of the adjoining sector.	
<p><b>Precinct 3: Large Sized Lot Residential</b></p> <p>The third and final stage of residential development consists of the residential area to the south-east. This precinct is characterised by the larger lot sizes (typically around 2,000 to 10,000 square metres) and copses of remnant vegetation and again predominant uses of large single residential dwellings and open space areas.</p>	Noted.
<p><b>4.2 Design guidelines</b></p> <p>The provisions are addressed in the following design guidelines. Each Element of the design guidelines has been predicated on the basis of the foregoing assessment which identified the key elements of the character of the Eagle Bay Special Character Area and suggestions for retaining/preserving this identifiable character. The elements include the following:</p> <ul style="list-style-type: none"> <li>A Setback Variations</li> <li>B Protection of Views</li> <li>C Coastal Setbacks/ Lots Adjoining Public Open Space</li> <li>D Outbuildings/Incidental Development</li> <li>E Single dwelling design</li> <li>F Building height</li> <li>G Vegetation Protection/Landscaping</li> <li>H Vehicle Access Design</li> <li>I Site Works</li> </ul> <p>Building envelope plans shall still be given due regard in the assessment of development applications pursuant to the Scheme.</p>	<p>Satisfied.</p> <p>Design elements considered in this assessment. It is considered the proposed development compliments the existing character.</p> <p>There is no Building Envelop for this site.</p>
<b>Element A – Setback Variations</b>	N/A
<b>Element B – Protection of Views</b>	<p>Views cannot be “owned”.</p> <p>The provisions do not provide any control over the preservation of views. There is no guarantee the proposed development will not impact view(s) of adjoining properties to some extent.</p>
<b>Element C – Coastal Setbacks/Lot Adjoining Reserves</b>	N/A
<b>Element D – Outbuilding/Incidental Development</b>	Outbuilding and incidental development comply with the R-Codes and Local Planning Policy 4.10, see assessment below.
<b>Element E – Single Dwelling Design</b>	The proposal is for a single dwelling
<b>Element F – Building Height</b>	<p>Satisfied.</p> <p>Refer previous assessment.</p>
<b>Element G – Vegetation Protection/Landscaping</b>	Minimal impact to existing vegetation. Landscaping Plan provided to required standard.
<b>Element H – Vehicle Access Design</b>	The driveway and crossover will be constructed of Concrete Pave See Engineering referral below.
<b>Element I – Site Works</b>	Retaining walls are proposed to address the sloping site (67 -63 AHD in a south to north direction)
<b>LPP 4.2 Bushfire</b>	
	<p><input checked="" type="checkbox"/> Yes <input type="checkbox"/> No</p> <p>A BAL has been prepared by an appropriately accredited Bushfire Planning Practitioner (refer ‘Bushfire’ section below).</p>

BUSHFIRE			
<b>Is the site bushfire prone?</b>	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	The site is bushfire prone within the Residential Built Out Area  <input checked="" type="checkbox"/> Residential Built Out Areas 2020
<b>What is the BAL rating?</b>	<b>BAL-19</b>		
<b>Has the application been referred to DFES?</b>	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No	Residential Built Out Area and BAL 19 can be achieved.
<b>Is the City satisfied that the proposal complies with:</b>			
1. <b>SPP3.7 – Planning in Bushfire Prone Areas?</b>	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	
2. <b>Position Statement – Vulnerable Land Uses in a Bushfire Prone Area?</b>	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No	Residential Dwelling
3. <b>Guidelines for Planning in Bushfire Prone Areas has been achieved?</b>	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	
LOCAL PLANNING POLICY 4.2 - BUSHFIRE			
	POLICY STATEMENT	COMPLIES? YES/NO	COMMENTS
4.1	All supporting plans and documents relevant to consideration of bushfire risk have been endorsed by an Appropriately Accredited Professional.	<input checked="" type="checkbox"/> YES <input type="checkbox"/> NO	
Gregory Voigt	BPAD41413 1	31/08/2023 12:00:00 AM	MARGARET RIVER, WA BushFire Works
BPAD Accredited Practitioner Details			
<b>Name:</b> Greg Voigt 0409 272 666		<i>Authorised Practitioner Stamp</i>	
<b>Company Details</b>			
 <b>BushFire Works</b> ABN 945 23123 843 PO Box 1249, Bibra Lake DC, WA 6965 <a href="mailto:enquire@bushfireworks.com.au">enquire@bushfireworks.com.au</a> <a href="http://www.bushfireworks.com.au">www.bushfireworks.com.au</a>			
<i>Reliance on the assessment and determination of the Bushfire Attack Level contained in this report should not extend beyond a period of 12 months from the date of issue of the report. If this report was issued more than 12 months ago, it is recommended that the validity of the determination be confirmed with the Accredited Practitioner and where required an updated report issued.</i>			
4.2	The APZ proposed to be provided: 1. Is no greater than 25 metres from Relevant Buildings.  <i>Note: A larger APZ may be considered, but only to the extent necessary to allow the determined BAL requirement to be reduced to BAL29, and where there are considered to be no unacceptable</i>	<input checked="" type="checkbox"/> YES <input type="checkbox"/> NO <input type="checkbox"/> N/A	The site is Currently BAL 19, the site is maintained to APZ standard through enforcement of the firebreak notice. The ranger who last inspected it was Gordon Boettcher, the Rangers Department have confirmed this site has been compliant every year.

impacts on biodiversity, landscape values or visual amenity impacts.

*In respect of visual amenity, this shall generally require a demonstration that the larger APZ will not increase the visibility of the proposed development, when viewed from locations outside the subject lot (and, in cases where the potential impact is considered significant, a visual/landscape assessment may be required, prepared using the methodology set out in the Visual Landscape Planning Manual).*

Historical Aerials show this as a cleared site approx. 10 year ago.



re 1: Classified Vegetation

The site is Classified as a Firebreak Category 2



- Firebreak Category
- Category 0
- Category 1
- Category 2
- Category 3

Under the Firebreak Notice the following is stated:

<b>CATEGORY 2</b> URBAN RESIDENTIAL & INDUSTRIAL - COMMERCIAL Sections A, B, D and E1 Trees, apply to this category. Refer to section E - Interpretation and Additional Requirements (E1 Trees).	✓	✓	✓
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**Firebreak**

Category 2 - Urban Residential and Industrial-Commercial: Where the area of land exceeds 2024m<sup>2</sup> (½ acre) a mineral earth FIREBREAK shall be constructed and maintained at least 3 metres wide and within 6 metres of the inside of all external boundaries of the land. Where the area of land is 2024m<sup>2</sup> (½ acre) or less, hazardous material must be removed in accordance with section B - Fuel Reduction (refer to B1).

**Fuel Reduction**

Category 2 - Urban Residential and Industrial-Commercial: Where the area of land is 2024m<sup>2</sup> (½ acre) or less, ALL HAZARDOUS MATERIAL must be removed from the whole of the land except living trees. In the area remaining, vegetation is to be maintained to a height of no greater than 10 centimetres; this includes piles of timber, branches and other vegetation. Trees shall be pruned in accordance with section E – Interpretation and Additional Requirements (refer to E1).

It has been confirmed by the Rangers Department (18/07/23) that this site is inspected every year to enforce the firebreak notice and that this site has complied every year. Therefore the city can accepted that site being excluded from the vegetation classification map provided by the Bushfire consultant as set out above.

Plot 2 is also accepted as excluded given that they also have to adhere to the City’s Firebreak Notice and Eagle Bay is a priority area for the Rangers Department.

(b) Can be accommodated entirely within the subject lot or, where the APZ cannot be accommodated entirely within the subject lot, the land involved –  (i) is developed as a public road, path or is otherwise public land managed such that there is a reasonable expectation that fuel loads will be managed to the APZ standard on an ongoing basis; or  (ii) is part of an APZ or Firebreak already in place and/or required on an adjoining property; or (iii) is otherwise Low-Threat Vegetation or Non-Vegetated Area.	<input checked="" type="checkbox"/> YES <input type="checkbox"/> NO <input type="checkbox"/> N/A	
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	<p><i>Note: Where the APZ cannot be accommodated within the subject lot or in accordance with parts (i) – (iii) above, a Perpetual Agreement is required to be in place with the owner of the neighbouring property to allow and require the maintenance of the portion of the APZ that extends beyond the subject property.</i></p>		
	<p>(c) Can be created and maintained without need to obtain a clearing permit under the (State) <i>Environmental Protection (Clearing of Native Vegetation) Regulations 2004</i>, approval under the (State) <i>Biodiversity Conservation Act 2016</i>, and/or approval under the (Commonwealth) <i>Environmental Protection and Biodiversity Conservation Act 1999</i>.</p> <p><i>Note: Where a clearing permit is necessary, consideration of the following is required:</i></p> <ul style="list-style-type: none"> <li>• <i>Whether the necessary permit or approval (including a conditional permit or approval, where the application is consistent with those conditions) has been obtained; or</i></li> <li>• <i>Flora and fauna assessments have been prepared by appropriately qualified and experienced environmental professionals, and the City considers that the clearing is necessary and appropriate, from an environmental perspective, to accommodate the proposed development; or</i></li> <li>• <i>The applicant commits to seeking the necessary permit or approval prior to commencement of development (including a change of use), and the City considers that the environmental approval processes are unlikely to require significant changes to the proposal.</i></li> </ul>	<input checked="" type="checkbox"/> YES <input type="checkbox"/> NO <input type="checkbox"/> N/A	<p>Site has been predominantly cleared for over 10yrs</p>
<b>CHANGE OF USE OF EXISTING BUILDINGS TO A VULNERABLE LAND USE IN A BUSHFIRE PRONE AREA</b>			
<b>4.3</b>	<p>Where the Bushfire Management Plan prepared by an Appropriately Accredited Professional sets out, and the City agrees, that it is not practicable to upgrade the existing building to the determined BAL a larger APZ may be considered, having considered the application against the other requirements as listed in 4.1.2 above.</p>		<b>N/A – New dwelling BAL 19</b>
<p><b>Officers Comments.</b> A detailed landscaping plan has been provided and the bushfire consultant has confirmed that this will not impact on the BAL</p>			

Bushfire Works  
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PO Box 1249, Bibra Lake DC WA 6965  
E: david.deeley@bushfireworks.com.au  
www.bushfireworks.com.au



**BAL Rating Confirmation**  
**DA23 / 0327 Landscape Management Plan**

This is to certify that the Landscape management plan dated 11/10/2023, for **Lot 16, #6 Seaview Rise**, will not impact on the current BAL rating of BAL – 19. The plan complies with State Planning Policy 3.7 – *Planning in Bushfire Prone Areas*.

No trees are located within 6m of the residence and no limbs overhang the structure.

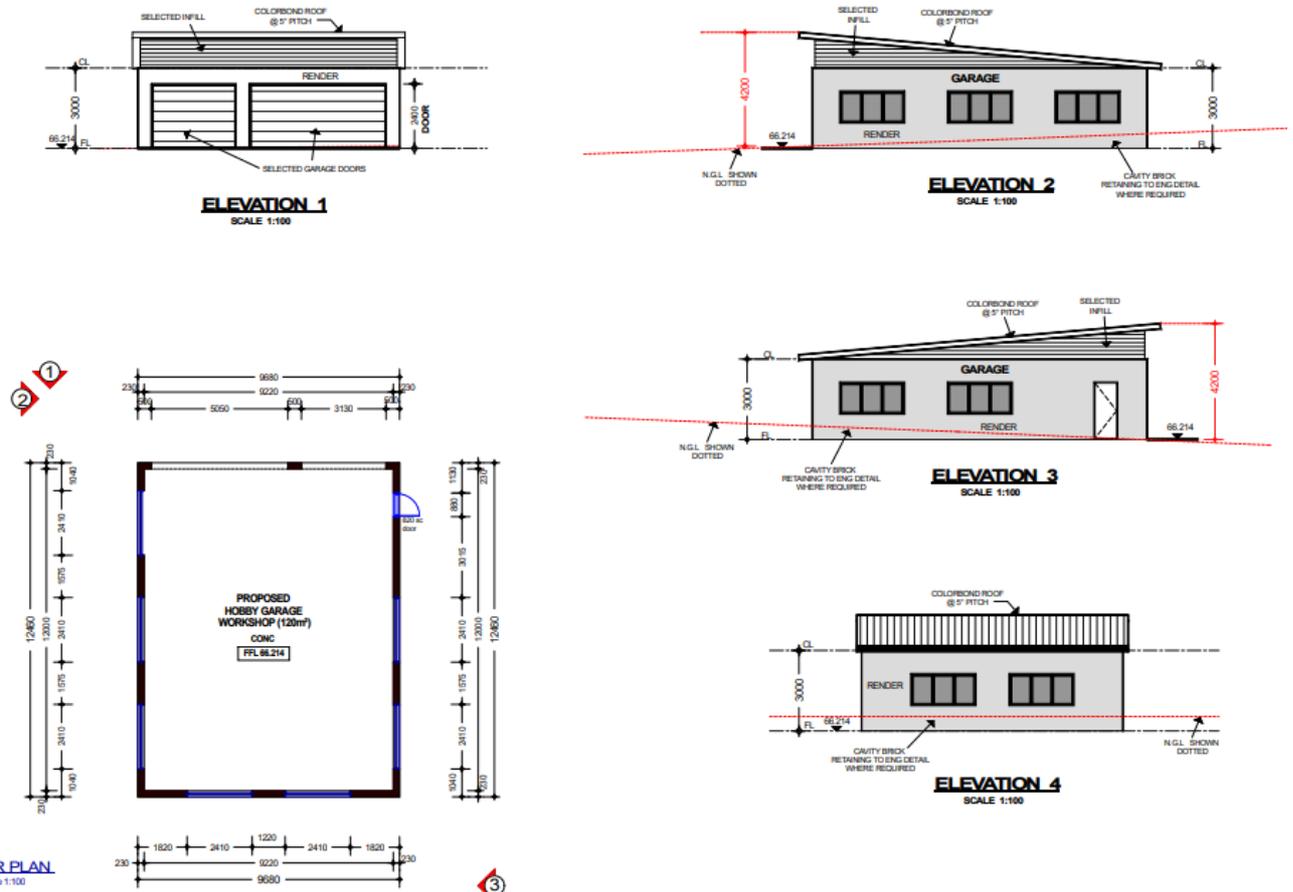
The firebreak is maintained around the site, complying with the City rating of “Category 2”. The plan indicates ongoing maintenance of the firebreak.

Proposed plantings appear to be “Firewise” species.

Dr David Deeley  
12<sup>th</sup> October 2023

**LPP 4.10 OUTBUILDINGS AND OTHER NON HABITABLE BUILDINGS**

**RESIDENTIAL ZONE (R2 – R5)**

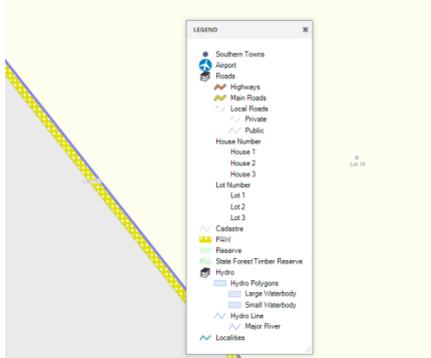


	POLICY REQUIREMENT	PROPOSED	COMPLIES?
Wall Height	3.1m	3m – Complies	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No
Ridge Height	4.5m	<4.5m – Complies	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No
Area	120m <sup>2</sup>	The previous proposal was 228m <sup>2</sup> Current Proposal is 120.6m <sup>2</sup>	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No A discretion 0.6m <sup>2</sup> is considered negligible

LPP 6.01 STORMWATER MANAGEMENT	
<p><b>Stormwater runoff is retained for use and/or infiltration within the lot at a rate of 1m<sup>3</sup> of storage per 65m<sup>2</sup> of Impervious Surfaces to accommodate 5 Year ARI. Where an approved urban water management plan is in place stormwater management is to be in accordance with that plan.</b></p>	<p>Condition on Approval.</p>

**SPP 7.3 - RESIDENTIAL DESIGN CODES V1**

**5.1 CONTEXT**

5.1.1 SITE AREA			
Deemed to Comply Provision	DTC	Proposed	Complies (YES/NO) & Comments
<p><b>C1.1</b> Development which complies with the dwelling type and site area requirements set out in Table 1 and the following provisions.  <b>C1.2 - C1.4...</b> Please refer to R-Code document for full clause details.</p>	N/A	N/A	N/A – Lot is existing
5.1.2 STREET SETBACKS			
<p><b>C2.1</b> Buildings, <b>excluding carports, unenclosed porches, balconies, verandahs or equivalent</b>, set back from the primary street boundary</p> <ol style="list-style-type: none"> <li>i. In accordance with Table 1.</li> <li>ii. Corresponding to the average setback of existing dwellings on each adjacent property fronting the same street.</li> <li>iii. Reduced by up to 50% provided the area of any building, including a garage, encroaching into the setback area is compensated for in open area.</li> <li>iv. In the case of areas coded R15 or higher, the street setback may be reduced to 2.5, or 1.5m to the porch, balcony, verandah or equivalent, where... <ul style="list-style-type: none"> <li>• Refer to R-Code document for full clause details.</li> </ul> </li> <li>v. To provide for registered easements for essential services.</li> </ol>	<p><b>Front</b> R-codes do not apply - Application to be assessed against Schedule 4.</p>	N/A	N/A
<p><b>C2.2 &amp; C2.3</b> - Buildings setback from the secondary street and corner truncation in accordance with Table 1.</p>	<p><b>Eagle Bay Road is not deemed a secondary street as there is an access restriction in place to Eagle Bay Road in the form of a PAW. Only legal access from Sea View Crescent.</b></p>		
			
<p><b>C2.4</b> Unenclosed porch, balcony, verandah or equivalent may project into the primary street setback area* to a maximum of half</p>	<b>Front</b>	N/A	N/A

<p>the required primary street setback without applying the compensating area.</p> <p>* Subject to BCA requirements</p>	<p><b>R-codes do not apply - Application to be assessed against Schedule 4.</b></p>		
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5.1.3 LOT BOUNDARY SETBACK	
Deemed to Comply Provision	Proposed
<p><b>C3.1</b> Buildings which are set back in accordance with the following provisions, subject to any additional measures in other elements of the R-Codes:</p> <p>i. Buildings set back from lot boundaries in accordance with Table 1, and Tables 2a and 2b (refer to Figure Series 3 and 4);</p> <p>ii. <b>For patios, verandahs or equivalent structures</b>, the lot boundary setbacks in Table 1 and Tables 2a and 2b may be <b>reduced to nil</b> to the posts where the structure*:</p> <p style="margin-left: 20px;">a. is not more than 10m in length and 2.7m in height;</p> <p style="margin-left: 20px;">b. is located behind the primary street setback; and</p> <p style="margin-left: 20px;">c. has <b>eaves, gutters and roofs set back at least 450mm</b> from the lot boundary;</p> <p style="margin-left: 20px;">*<b>Note:</b> There are separate building code requirements which may also apply</p> <p>iii-vi Refer to R-Code document for full clause details.</p>	<p style="text-align: center;"><u>Rear</u></p> <p style="text-align: center;"><b>R-codes do not apply - Application to be assessed against Schedule 4.</b></p> <p style="text-align: center;"><u>Side (North)</u></p> <p style="text-align: center;">DTC - 7.5m</p> <p style="text-align: center;">Proposed – 10m</p> <p style="text-align: center;">Complies</p> <p style="text-align: center;"><u>Side (South)</u></p> <p style="text-align: center;">DTC – 7.5M</p> <p style="text-align: center;">Proposed – 16m</p> <p style="text-align: center;">Complies</p>

5.1.3 LOT BOUNDARY SETBACK	
Deemed to Comply Provision	Proposed
<p><b>C3.2</b> Boundary walls may be built behind the street setback (specified in Table 1 and in accordance with clauses 5.1.2 and 5.2.1), within the following limits and subject to the overshadowing provisions of clause 5.4.2:</p> <p>i. where the wall abuts an existing or simultaneously constructed boundary wall of equal or greater dimension; or</p> <p>ii. in areas coded <b>R20 and R25</b>, walls not higher than 3.5m, up to a maximum length of the greater of 9m or one-third the length of the balance of the site boundary behind the front setback, <b>up to two site boundaries</b>; or</p> <p>iii. in areas coded R30 and higher, walls not higher than 3.5m for two-thirds the length of the balance of the site boundary behind the front setback, to <b>up to two site boundaries</b>; or</p> <p>iv. where both the subject site and the affected adjoining site are created in a plan of subdivision submitted concurrently for the proposed development, and the boundary walls are interfacing and of equal dimension.</p> <p><b>C3.3 &amp; C3.4</b> Refer to R-Code document for full clause details.</p> <p><b>Note:</b></p> <ul style="list-style-type: none"> <li>• Pillars and posts with a horizontal dimension of 450mm by 450mm, or less, do not constitute a boundary wall.</li> <li>• Retaining walls do not constitute boundary walls for the purpose of this clause.</li> </ul>	<p style="text-align: center;"><b>No boundary walls proposed.</b></p>

5.1.4 C4 OPEN SPACE		
Deemed to Comply Provision	Proposed	Complies (YES/NO) & Comments

<p>Open Space (definition provided below) provided in accordance with Table 1...The site of the grouped dwelling, for the purpose of calculating the open space requirement, shall include the area allocated for the exclusive use of that dwelling and the proportionate share of any associated common property.</p>	<p>Table 1 - 80%</p>	<p>Residence – 473sqm Gym – 66.5sqm “Outdoor living (Games room, change room, storage and pump room)” – 113sqm Hobby/Workshop Garage – 120sqm</p> <p>NOTE : No outdoor living areas more than 1m above NGL and therefore no outdoor living areas have been included in built area,</p> <p>Total built area = 772.5sqm Open space area = 5134.5sqm % open space = 86%</p>	<p>Complies</p>
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**Open space**

Generally that area of a **lot** not occupied by any **building** and includes:

- open areas of accessible and useable flat roofs and outdoor living areas above **natural ground level**;
- areas beneath eaves;
- **verandahs, patios** or other such roofed structures not more than 0.5m above natural ground level, **unenclosed** on at least two sides, and covering no more than 10 per cent of the **site area** or 50m<sup>2</sup> whichever is the lesser;
- unroofed open structures such as **pergolas**;
- uncovered **driveways** (including access aisles in car parking areas) and uncovered car parking spaces;

but excludes:

- non-accessible roofs, verandahs, balconies and outdoor living areas over 1m above natural ground level; and/or
- covered car parking spaces and covered walkways, areas for rubbish disposal, stores, outbuildings or plant rooms.

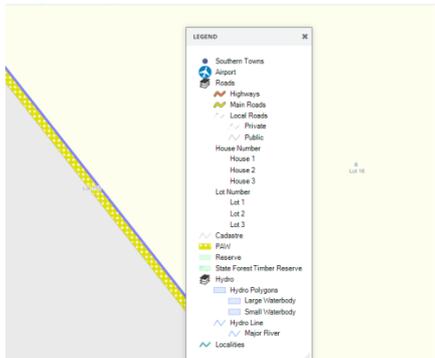
**5.2 STREETScape**

<b>5.2.1– SETBACK OF GARAGES AND CARPORTS</b>		
<b>Deemed to Comply Provision</b>	<b>Proposed</b>	<b>Complies (YES/NO) &amp; Comments</b>
<b>C1.1</b> Garages setback 4.5m from the primary street except that the setback may be reduced... refer to R-Code document for full clause details.	<b>Garage setback</b>	<b>YES</b>
<b>C1.2</b> Carports setback from the primary street as per clause 5.1.2 C2.1i, except that the setback may be reduced by up to 50% of the minimum setback stated in Table 1 where: <ul style="list-style-type: none"> <li>i. the width of the carport does not exceed 60 per cent of the frontage;</li> <li>ii. the construction allows an unobstructed view between the dwelling and the street, right-of-way or equivalent.; and</li> <li>iii. the carport roof pitch, colours and materials are compatible with the dwelling. (Refer to Figure 8a)</li> </ul>	<b>No carport proposed.</b>	<b>N/A</b>
<b>C1.3</b> Garages and carports built up to the boundary abutting a communal street or ROW which is not the primary or secondary street for the dwelling, with manoeuvring space of at least 6m, located	<b>N/A</b>	<b>N/A</b>

between the opening to the garage or carport and permanently available.		
<b>C1.4</b> Garages and carports setback 1.5m from a secondary street.	N/A	N/A
<b>C1.5</b> Carports within the street setback area in accordance with 5.1.2 C2.1iii provided the width of carport does not exceed 50% of the frontage at the building line and unobstructed view of the dwelling and street/RoW or equivalent.	N/A	N/A
<b>5.2.2 GARAGE WIDTH</b>		
<b>C2</b> A garage door and its supporting structures (or a garage wall where a garage is aligned parallel to the street) facing the primary street is not to occupy more than 50 per cent of the frontage at the setback line as viewed from the street....	<b>Garage is aligned parallel to the street</b> <b>6.7m/52m = 12% of frontage</b>	Yes
<b>5.2.3 STREET SURVEILLANCE</b>		
<b>C3.1</b> Street elevation of the dwelling to address the street with clearly definable entry points visible and accessed from the street.	<b>Complies – Porch and entry visible and access from Sea View Rise.</b>	YES
<b>C3.2</b> One major opening from a habitable room of the dwelling faces the street and pedestrian or vehicular approach to the dwelling	<b>Major openings in front elevation.</b>	YES
<b>C3.3</b> For battleaxe lots or sites with internal driveway access, at least one major opening from a habitable room to face the approach to the dwelling.	N/A	N/A
<b>5.2.4 STREET WALLS AND FENCES</b>		
<b>C4.1</b> Front fences within the primary street setback area that are visually permeable above 1.2m. (Refer Figure 12)	N/A	N/A
<b>C4.2</b> Solid pillars that form part of front fences not more than 1.8m above natural ground level provided. Pillars 400mm by 400mm and pillars separated by visually permeable fencing. (Refer Figure 12)	N/A	N/A
<b>5.2.5 SIGHT LINES</b>		
<b>C5</b> – Walls, fences and other structures truncated or reduced to no higher than 0.75m within 1.5m of where walls, fences, or other structures adjoin: i. a driveway that intersects a street, right-of-way or communal street; ii. a RoW or communal street that intersects a public street; and iii. two streets that intersect (refer Figure 9a).	<b>No structures within sightline area proposed.</b>	Yes

**5.3 SITE PLANNING AND DESIGN**

<b>5.3.1 OUTDOOR LIVING AREAS</b>			
Deemed to Comply Provision	Proposed		Complies (YES/NO) & Comments
Outdoor Living (m <sup>2</sup> ) as per Table 1	m <sup>2</sup>	No minimum	<b>No minimum area within Table 1.</b>
Min. Dimension (m) Note: Minimum dimension refers to min. length and width of all areas that contribute to the total req. outdoor living area space.	4.0m	N/A	
Location	Behind St. S/B	N/A	
Accessibility	Primary living space	N/A	
Roof Cover	Two-thirds no permanent roof cover	N/A	

5.3.2 LANDSCAPING																			
Deemed to Comply Provision	Proposed	Complies (YES/NO) & Comments																	
<p><b>C2.2</b> Landscaping of single houses, grouped dwellings and multiple dwellings to include the following:</p> <p>i. the minimum number of trees and associated planting areas in the table below; and</p>	<p><b>Applicant has provided a sufficient landscaping plan.</b></p>	<p><b>Yes</b></p>																	
<p>ii. Landscaping of the street setback area, with not more than 50% of the area to consist of impervious surfaces.</p>																			
<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="width: 25%;">Dwelling type</th> <th style="width: 25%;">Minimum tree requirement</th> <th style="width: 25%;">Minimum tree planting area</th> </tr> </thead> <tbody> <tr> <td>Single houses and grouped dwellings (tree per dwelling)</td> <td style="text-align: center;">1 tree</td> <td rowspan="3" style="text-align: center; vertical-align: middle;">2m x 2m</td> </tr> <tr> <td rowspan="2">Multiple dwellings (trees per site)</td> <td style="text-align: center;">Less than 700m<sup>2</sup></td> <td style="text-align: center;">2 trees</td> </tr> <tr> <td style="text-align: center;">700 - 1000m<sup>2</sup></td> <td style="text-align: center;">3 trees</td> </tr> <tr> <td></td> <td style="text-align: center;">Greater than 1000m<sup>2</sup></td> <td style="text-align: center;">4 trees</td> </tr> </tbody> </table>	Dwelling type	Minimum tree requirement	Minimum tree planting area	Single houses and grouped dwellings (tree per dwelling)	1 tree	2m x 2m	Multiple dwellings (trees per site)	Less than 700m <sup>2</sup>	2 trees	700 - 1000m <sup>2</sup>	3 trees		Greater than 1000m <sup>2</sup>	4 trees	<p>Note:</p> <p>i. The minimum tree planting area is to be provided for each tree and shown on the site plan that is submitted with the application.</p> <p>ii. The tree planting area is to be free of impervious surfaces and roof cover.</p>				
Dwelling type	Minimum tree requirement	Minimum tree planting area																	
Single houses and grouped dwellings (tree per dwelling)	1 tree	2m x 2m																	
Multiple dwellings (trees per site)	Less than 700m <sup>2</sup>		2 trees																
	700 - 1000m <sup>2</sup>		3 trees																
	Greater than 1000m <sup>2</sup>	4 trees																	
5.3.3 PARKING & 5.3.4 DESIGN OF PARKING SPACES																			
Deemed to Comply Provision	Provided	Complies (YES/NO) & Comments																	
<p><b>C3.1</b> No. required onsite car parking bays</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th rowspan="2" style="width: 25%;">Type of dwelling</th> <th colspan="2" style="width: 50%;">Car parking spaces</th> </tr> <tr> <th style="width: 25%;">Location A</th> <th style="width: 25%;">Location B</th> </tr> </thead> <tbody> <tr> <td>1 bedroom dwelling</td> <td style="text-align: center;">1</td> <td style="text-align: center;">1</td> </tr> <tr> <td>2 + bedroom dwelling</td> <td style="text-align: center;">1</td> <td style="text-align: center;">2</td> </tr> <tr> <td>Aged persons' dwelling</td> <td style="text-align: center;">1</td> <td style="text-align: center;">1</td> </tr> <tr> <td>Ancillary dwelling</td> <td style="text-align: center;">nil</td> <td style="text-align: center;">1</td> </tr> </tbody> </table> <p>Location A = within 250m of a high frequency bus route, or multiple bus routes that, if combined, have timed stops every 15 minutes during weekday peak periods (7 – 9am and 5 – 7pm), measured in a straight line from along any part of the bus route to any part of the lot.</p> <p>Location B = includes all land that is not within Location A</p>	Type of dwelling	Car parking spaces		Location A	Location B	1 bedroom dwelling	1	1	2 + bedroom dwelling	1	2	Aged persons' dwelling	1	1	Ancillary dwelling	nil	1	<p><b>+2 car bays proposed on site.</b></p>	<p><b>Yes</b></p>
Type of dwelling		Car parking spaces																	
	Location A	Location B																	
1 bedroom dwelling	1	1																	
2 + bedroom dwelling	1	2																	
Aged persons' dwelling	1	1																	
Ancillary dwelling	nil	1																	
<p><b>C4.1</b> Car parking spaces and manoeuvring areas designed and provided in accordance with AS2890.01 (as amended).</p> <ul style="list-style-type: none"> <li>Standard double garage 5.4m x 5.4m</li> </ul>	<p><b>Internal dimensions of garage comply with AS2890.01</b></p>	<p><b>Yes</b></p>																	
5.3.5 VEHICULAR ACCESS																			
<p><b>C5.1</b> Access to on-site car parking spaces to be provided:</p> <ul style="list-style-type: none"> <li>From a communal street or RoW if available.</li> <li>From a secondary street where no Communal Street or RoW exists.</li> <li>From the primary street frontage where no secondary street, communal street or RoW exists.</li> </ul>	<p style="text-align: center;"><b>Access restriction in place to Eagle Bay Road in the form of a PAW. Only legal access from Sea View Crescent.</b></p> 	<p><b>Yes</b></p>																	
<p><b>C5.2 &amp; C5.3</b> Driveways to primary or secondary street provided as follows:</p>	<p><b>Driveway is 6m in width. +0.5m from side lot boundary.</b></p>	<p><b>Yes</b></p>																	

<ul style="list-style-type: none"> <li>• Min: 3m</li> <li>• Max: 6m &amp; 9m (aggregate)</li> <li>• 0.5m from side lot boundary</li> <li>• 6m from street corner.</li> <li>• Aligned at right angles to the street alignment.</li> <li>• Located so as to avoid street trees, or where this is unavoidable, the street trees replaced at the applicants expense or re-planting arrangements approved.</li> <li>• Adequately paved and drained.</li> </ul>	<p><b>+6m from any street corner.</b></p> <p><b>Right angle.</b></p> <p><b>No obstructions.</b></p> <p><b>Paved and drained</b></p>															
<p><b>C5.4</b> Driveways designed for two way access to enter street in forward Gear where:</p> <ul style="list-style-type: none"> <li>• Driveway serves 5 or more dwellings.</li> <li>• Distance from a car space to street is 15m or more.</li> <li>• The street to which it connects is designated as a primary distributor or integrator arterial road.</li> </ul>	<p><b>Adequate manoeuvring and turning area of cars to exit in forward gear from garage.</b></p>	<p><b>Yes</b></p>														
<b>5.3.7 SITE WORKS</b>																
<p><b>C7.1 &amp; C7.2 Retaining walls, fill and excavation:</b></p> <ul style="list-style-type: none"> <li>• Maximum of 0.5m of fill or excavation within street setback, except to provide access for pedestrian, universal and/or vehicle access, drainage works or natural light to the dwelling.</li> <li>• Retaining walls, fill and excavation within the site and behind the required street setback to comply with Table 4:</li> </ul> <p>Table 4 – Setback of site works and retaining walls</p> <table border="1" data-bbox="215 996 566 1198"> <thead> <tr> <th>Height of site works and/or retaining walls</th> <th>Required minimum setback</th> </tr> </thead> <tbody> <tr> <td>0.5m or less</td> <td>0m</td> </tr> <tr> <td>1m</td> <td>1m</td> </tr> <tr> <td>1.5m</td> <td>1.5m</td> </tr> <tr> <td>2m</td> <td>2m</td> </tr> <tr> <td>2.5m</td> <td>2.5m</td> </tr> <tr> <td>3m</td> <td>3m</td> </tr> </tbody> </table> <p>Notes: i. Take the nearest higher value for all height and length calculations</p> <p><b>C7.3</b> Retaining walls that result in land which can be effectively used for the benefit of residents and do not detrimentally affect adjoining properties and are designed, engineered and landscaped having due regard to clauses 5.3.7 and 5.4.1.</p>	Height of site works and/or retaining walls	Required minimum setback	0.5m or less	0m	1m	1m	1.5m	1.5m	2m	2m	2.5m	2.5m	3m	3m	<p><b>No retaining walls within the front setback.</b></p> <p><b>All retaining walls are more 3m away from lot boundaries and therefore comply.</b></p> <p><b>Assessment against LPP requirements relating to retaining walls undertaken in other officer report.</b></p>	<p><b>Yes</b></p>
Height of site works and/or retaining walls	Required minimum setback															
0.5m or less	0m															
1m	1m															
1.5m	1.5m															
2m	2m															
2.5m	2.5m															
3m	3m															
<b>5.3.9 STORMWATER MANAGEMENT</b>																
<b>Deemed to Comply Provision</b>	<b>Proposed</b>	<b>Complies (YES/NO) &amp; Comments</b>														
<p><b>C9 - Retention of stormwater on-site</b></p> <p>Single House - 1m<sup>3</sup> per 65m<sup>2</sup> of impervious area</p> <p>Infill - 1m<sup>3</sup> per 40m<sup>2</sup> of impervious area</p>	<p><b>As per standard requirement to be enforced via a condition of development approval.</b></p>	<p><b>Yes</b></p>														

**5.4 BUILDING DESIGN**

<b>5.4.1 C1.1(i) - PRIVACY</b>		
<b>Deemed to Comply Provision</b>	<b>Proposed</b>	<b>Complies (YES/NO) &amp; Comments</b>
<p><b>Bedroom</b></p>	<p>4.5m</p>	<p><b>No major openings within 4.5m/6m or outdoor living areas within 7.5m of any lot boundary therefore cone of vision will fall within lot boundaries.</b></p>
<p><b>Other Habitable Room</b></p>	<p>6.0m</p>	
<p><b>Outdoor living area (including balcony)</b></p>	<p>7.5m</p>	

Note: Refer to R-Code document for full clause details for variations which are acceptable.		
<b>C1.1(ii)</b> Provided with permanent vertical screening	<b>N/A</b>	<b>N/A</b>
<b>C1.2</b> Screening devices to be at least 1.6m in height, at least 75% obscure, durable material and permanently fixed	<b>N/A</b>	<b>N/A</b>
<b>5.4.2 SOLAR ACCESS FOR ADJOINING SITES</b>		
<b>C2.1</b> Overshadowing - R25 and lower 25%, R30 – R40 35%, R-AC or above R40 50% of adjoining site area.	<b>Dwelling is setback 16m from the southern lot boundary. Overshadowing will fall within the lot boundaries.</b>	<b>Yes</b>
<b>C2.2</b> Development site shares southern boundary with a lot, refer figure 11b		
<ul style="list-style-type: none"> <li>Dividing fences of up to 2.0 metres in height do not contribute to overshadowing calculations</li> </ul>		
<b>5.4.4 EXTERNAL FIXTURES, UTILITIES &amp; FACILITIES</b>		
<b>Deemed to Comply Provision</b>	<b>Proposed</b>	<b>Complies (YES/NO) &amp; Comments</b>
<b>C4.1</b> Solar collectors installed on the roof or other parts of buildings.	<b>No solar collectors are proposed.</b>	<b>Yes</b>
<b>C4.2</b> Television aerials of the standard type, essential plumbing vent pipes above the roof line and external roof water down pipes.	<b>Noted.</b>	<b>Yes</b>
<b>C4.3</b> Other external fixtures provided they are: <ul style="list-style-type: none"> <li>i. not visible from the primary street;</li> <li>ii. are designed to integrate with the building; or</li> <li>iii. are located so as not to be visually obtrusive.</li> </ul>	<b>No external fixtures marked on the plans</b>	<b>Yes</b>
<b>C4.4</b> Antennas, satellite dishes and the like not visible from any primary and secondary street.	<b>Noted.</b>	<b>Yes</b>
<b>C4.6</b> Where rubbish bins are not collected from the street immediately adjoining a dwelling, there shall be provision of a communal pick-up area or areas which are: <ul style="list-style-type: none"> <li>i. conveniently located for rubbish and recycling pick-up;</li> <li>ii. accessible to residents;</li> <li>iii. adequate in area to store all rubbish bins; and</li> <li>iv. fully screened from view from the primary or secondary street.</li> </ul>	<b>Noted.</b>	<b>Yes</b>
<b>C4.7</b> Clothes-drying areas screened from view from the primary and secondary street.	<b>None marked on the plans.</b>	<b>Yes</b>

<b>REFERRALS</b>	
<b>INTERNAL</b> <ul style="list-style-type: none"> <li><b>Building</b></li> <li><b>Engineering</b></li> <li><b>Health</b></li> </ul>	<p><b><u>Building Comments</u></b></p> <ul style="list-style-type: none"> <li>Whilst it's a large house with a swimming pool it really doesn't require too much from a building point of view. Just the swimming pool mustn't have direct access from the dwelling, it must be isolated by a pool barrier fence. <ul style="list-style-type: none"> <li>All the stairs and balustrades must comply with the building code, the spiral type stair case must also be in accordance with the BCA.</li> <li>Bushfire requirements to be met and smoke detectors must be at all levels</li> </ul> </li> </ul> <p><b><u>Health Comments</u></b></p> <p>Development to be connected to an approved effluent disposal system in accordance with the Health (Treatment of Sewage and Disposal of Liquid Waste) Regulations 1974. An Application To Construct Or Install An Apparatus For The Treatment of Sewage is required at Building Application stage. In support of the application, ensure that the soil profile of the anticipated resulting leach field is included in the Geo Tech Assessment associated with the building application. <b>(To be attached as an advice note)</b></p>

	<p><b>Engineering Comments</b></p> <ul style="list-style-type: none"> <li>Pavement areas show many soakwells however can only see two for the buildings – plans to show supporting drainage calculations. <b>Condition</b></li> <li>Crossover to be constructed to in accordance with City requirements (noted on the plans)</li> </ul>
<p><b>EXTERNAL</b></p> <ul style="list-style-type: none"> <li>DPLH</li> <li>DFES</li> <li>DBCA</li> <li>DWER</li> </ul>	N/A
Other	N/A

**ADVERTISING AND NEIGHBOUR CONSULTATION**

Advertising Required?	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No		
Reasons for advertising?	<p>The proposal went as a Memo to Council 03 November 2023 and was subsequently presented to the Councillors on 15 November 2023 at the Planning and Development briefing. At this briefing session, the Council requested that consultation be undertaken.</p> <p>Consultation was undertaken with letters sent to 8 surrounding land owners and occupiers of 4 properties.</p>		
Start Date	24 November 2023	Finish Date	08 December 2023
Were any submissions received?	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No – 2 submissions were received.		

	Summary of Submission	Officer Response
<p><b>Name:</b> Residents of Eagle Bay Association <b>Address:</b> Eagle Bay</p>	<ol style="list-style-type: none"> <li><b>Building Height and Setback</b></li> <li><b>Site Fill</b></li> <li><b>Footprint</b></li> <li><b>Building bulk</b></li> <li><b>Visual Amenity</b></li> </ol>	<ol style="list-style-type: none"> <li>Variation to Building Height and Setback An assessment has been completed which justifies consideration of the variation, as per the summary above.</li> <li>It is considered the nominated FFL's are an appropriate design response to the natural contour of the lot. The proposal only required the minimal cut and fill to allow for a level build.</li> <li>Footprint The site is not subject to an endorsed building envelope plan to limit development to 1000m<sup>2</sup>.</li> <li>Building Bulk The proposal is of a scale relative to the size of site so that the impact of building bulk is limited. In addition, the large setbacks maintain adequate ventilation or sunlight to adjoining properties.</li> <li>Visual Amenity The impacts of proposed variations upon visual amenity have been assessed as part of a comprehensive report. As discussed above, the proposal is considered acceptable. This Site rises from the north to the south, as shown on the survey plan (from 6.3 AHD to 6.7 AHD). The proposed dwelling is not on or near any ridgeline, it is located on site so that it is not on the highest site contour. It is considered the design, siting and scale of the proposed development will not</li> </ol>

		<p>detrimentally impact the visual amenity of the area.</p> <p>The landscaping plan will also help to integrate the proposal in to the area, further protecting visual amenity.</p>
<p><b>Name:</b> Susie Bailey <b>Address:</b> Eagle Bay</p>	<p>The proposed plan for the property at 6 Seaview Rise, Eagle Bay is within precinct 3 of the Eagle Bay Special Character Area (SCA) Policy. The proposed development of this property is outside the building envelope of 7.5m height above natural ground level. The proposed development at 6 Seaview Rise proposes to build up the block height with fill material on the Seaview Rise side of the block. The proposed development is then a 3 story home 9m in height which, is currently existing natural ground level at the front of the block. According to the originally approved structure plan subdivision along Seaview Rise the maximum height is 7.5m this was to basically to limit buildings in this development to a maximum of 2 storeys and to date this plan has worked well for the area. This property will have a large footprint of greater than 1000m2 whereas the other properties in the area are around 500m2. There is no provision that I can see for a water tank which would be environmentally essential considering the number of trees being planted and fed from drip reticulation, the water pressure is extremely low already planting these will put further pressure on the system. The developers also plan to remove 2 extra-large gum trees which will impact the privacy of number 10 Seaview Rise, Eagle Bay. Eagle Bay is also defined as a Landscape Value Area (LVA) under the City of Busselton Town Planning Scheme, this is to ensure that any new developments fit in with the existing character of the area</p>	<p>Response as above, additional notes:</p> <p>Number 6 Seaview rises from the north (63.5AHD) to the south (67.10AHD).</p> <p>The FFL: Outbuilding garage - 66.214AHD Dwelling - 66.30AHD Gym – 66.30AHD Outdoor living – 66.643AHD</p> <p>It is considered the nominated FFL's are an appropriate design response to the natural contour of the lot. The proposal only required the minimal cut and fill to allow for a level build.</p> <p>The proposed dwellings (at it closest point to number 10) is set back 25m from number 10 (Lot 18). Number 10 is currently separated from the proposal by established vegetation on both Lots, and a driveway to number 6 (Lot 17).</p> <p>This along with the proposed landscaping plan will adequately protect the privacy of Number 10.</p>  <p>Given the design, scale, layout and landscaping plans provided it is considered that the proposed development is in keeping with the existing character of the area. No adverse impacts are anticipated.</p>
<p><b>Name:</b> M. Vincent <b>Address :</b> Eagle Bay</p>	<p>No issue with proposed development but concerns regarding landscaping and compliance with firebreaks. Concerns regarding proximity of trees to septic tanks and leach drain.</p>	<p>Firebreak and Fuel Hazard Reduction Notice (Bushfire Notice). The proposed landscaping plan comply with the requirements of the City's Bushfire Notice which requires a mineral earth firebreak 3m in width within 6m of all external lot boundaries (note: the firebreak does not need to be located on the lot boundary).</p> <p>The location of the septic tanks and leach drains will be subject to a separate application within the City's Environmental Health Services where the location if the septics and leach drains will be assessed.</p> <p>Bushfire consultant has confirmed, as set out above that the landscaping plan is bushfire compliment.</p>

<p><b>Name: M. Donaldson</b> <b>Address : Eagle Bay</b></p>	<p>The development does not comply with maximum height control of 7.5m above NGL. The development does not comply with the building envelope requirements</p>	<p>Building height has been assessed in the Council Report.</p> <p>Within the Scheme the applicable building envelope is 1,000sqm. The building envelope has been assessed 1,194sqm. This is considered appropriate as all other aspect, including open space and setbacks, comply with the deemed-to-comply criteria of the R-codes.</p>
<p><b>Officer Comment:</b></p> <p>Following submission amended plans were received, these plans reduce the highest point of the proposal from 9m to 8.7m, this is 0.1m lower than the neighbouring property to the north west.</p>		

Clause 67 – Matters to be Considered:	
Matters to be Considered	Comment
The aims and provisions of this Scheme and any other local planning scheme operating within the Scheme area;	The proposal is considered to satisfy the aims and provisions of the Scheme, specifically the objectives of the 'Residential' zone, the R-Codes and the development standards of the EBSCA established by Schedule 4.
The requirements of orderly and proper planning including any proposed local planning scheme or amendment to this Scheme that has been advertised under the Planning and Development (Local Planning Schemes) Regulations 2015 or any other proposed planning instrument that the local government is seriously considering adopting or approving;	Approval of the proposal is considered consistent with orderly and proper planning.
Any local planning policy for the Scheme area;	<p><i>The development meets the standards contained within the relevant local planning policies, being</i></p> <ul style="list-style-type: none"> <li>• <i>Local Planning Policy – 3.1 Reflective Building Materials,</i></li> <li>• <i>Local Planning Policy 3.3 – Eagle Bay Special Character Area (LPP 3.5),</i></li> <li>• <i>Local Planning Policy 4.3 – Bushfire, and</i></li> <li>• <i>Local Planning Policy 4.10 – Outbuildings and Other Non-habitable Buildings (LPP 4.10).</i></li> </ul>
<p>The compatibility of the development with its setting, including —</p> <p>(i) The compatibility of the development with the desired future character of its setting; and</p> <p>(ii) The relationship of the development to development on adjoining land or on other land in the locality, including but not limited to, the likely effect of the height, bulk, scale, orientation and appearance of the development;</p>	<p>The proposal is in keeping with the character of the area.</p> <p>The bulk and scale of the development is considered consistent with the 'Residential' zone, with the proposed setbacks are not considered to impact the amenity of the neighbouring properties.</p>
<p>The amenity of the locality including the following —</p> <p>(i) Environmental impacts of the development;</p> <p>(ii) The character of the locality;</p> <p>(iii) Social impacts of the development;</p>	<p><i>The development satisfies the elements of the EBSCA policy and is of a scale consistent with the 'Residential' zone.</i></p>

The likely effect of the development on the natural environment or water resources and any means that are proposed to protect or to mitigate impacts on the natural environment or water resource;	The development satisfies the elements of the EBSCA policy and is of a scale consistent with the 'Residential' zone. The style and form of the development is in keeping with the character of the locality. The proposal will not result in adverse impacts on the natural environment or the water resources.
Whether adequate provision has been made for the landscaping of the land to which the application relates and whether any trees or other vegetation on the land should be preserved;	The development will not result in negative impact to the natural environment or removal of significant vegetation.
The suitability of the land for the development taking into account the possible risk of flooding, tidal inundation, subsidence, landslip, bush fire, soil erosion, land degradation, or any other risk;	While the site is located in a 'Bushfire Prone Area,' the bushfire assessment above demonstrate that this proposal fully complies with the bushfire requirement and that the site is a Residential Built Out Area and BAL 19 can be achieved.  The land is suitable for development
Any submissions received on the application	Refer to submission section of report above.
<b>It is considered that the matters listed within Clause 67 of the Planning and Development (Local Planning Schemes) Regulations 2015 have been adequately address through the assessment of this application against the applicable Scheme and LPP requirements in the report above.</b>	

OTHER CONSIDERATIONS	
<p>Is a S.70A notification required?</p> <ul style="list-style-type: none"> <li>- Coastal inundation</li> <li>- Bushfire Prone</li> </ul>	<p><input checked="" type="checkbox"/> Yes <input type="checkbox"/> No</p> <p style="background-color: yellow;">Bushfire Prone Area. To be conditioned</p>



**SUMMARY OF SUBMISSIONS**  
**PROPOSAL: Single House (Swimming Pool, Gym and Outbuilding) - Lot 16 (No. 6) Seaview Rise, Eagle Bay**  
**SUBMISSIONS CLOSE: 08 December 2023**

No.	NAME	SUBMISSION	OFFICER COMMENT
1	<p>Name: D. Best on behalf of The Residents of Eagle Bay Association</p> <p>Locality: Eagle Bay</p>	<p><b>Objection</b></p> <p>Listed in following dot point format are areas of concern that we would request the City planners address during the planning approval process.</p> <ol style="list-style-type: none"> <li>1. According to the approved Structure Plan for the original subdivision along Seaview Rise, the maximum building height is 7.5 m above natural ground level at any point on the block. This obligation was basically to limit buildings in this subdivision area to a maximum of 2 storeys. This maximum height limit has worked to date and all the existing developments in Seaview Rise fit in well with the surrounding environment and also adjoining properties. This in turn has developed a special, low impact feel for this subdivision area. (which was the original intent of the Structure Plan).</li> </ol> <p>It seems that proposed development at 6 Seaview Rise proposes to build up the block height with fill material on the Seaview Rise side of the block. The development visual aspect from Seaview Rise will then be a 3 storey building. This will be a 9 m high building (above what is currently existing natural ground level at the front of the block) compared to the defined maximum allowable height of 7.5m above existing natural ground level.</p> <ol style="list-style-type: none"> <li>2. The actual overall footprint of the development area is a concern in terms of fitting in with the existing character of the other developments along Seaview Rise. Under the R2.5 zoning for the Seaview Rise area all buildings and development should be contained within a building envelope area of not more than 1,000 m2. The actual size of the building envelope area for 6 Seaview Rise is not shown on the plans provided by the City, but it would appear to be well over 1,000 m2. I.e. the block will therefore become over-developed from a visual impact point of view and not fit in with the existing visual amenity of this subdivision area.</li> <li>3. From the attachment to this email called "6 Seaview Rise Development Area" it can see that the proposed area of building development on 6 Seaview Rise is significantly larger than all the other properties along Seaview Rise. For instance the actual development footprint areas for the adjoining properties at No 8 &amp; 10 Seaview Rise are around 500 m2. The existing house across the road from No 6 (i.e. 9 Seaview Rise) has a development footprint area of around 480 m2. Similarly along the full extent of Seaview Rise building development footprint areas appear to be in the 500 to 800 m2 range. (i.e. relatively low visual impact).</li> <li>4. It would seem reasonable that a property owner should have flexibility in developing their dream home (within the constraints of the planning obligations that apply to the specific area), however in this case the owners seem to be pushing the boundaries on both building height and building development footprint area. This will create a development of significant bulk, and hence an impact on the existing visual amenity of this subdivision area within Eagle Bay.</li> <li>5. Seaview Rise is located within a defined Landscape Visual Amenity Area of the City of Busselton Town Planning Scheme, hence any proposed new development needs to be assessed by the town planners in regards to the impact the proposed development will have on the overall visual amenity of the area (i.e. the Eagle Bay residential area and adjoining tourist recreational areas). It should be noted that 6 Seaview Rise, and other lots on the west side of Seaview Rise, are located on a ridge line that is the highest elevation in Eagle Bay. Hence an over height building located on this ridge line is likely to give rise to some visual amenity issues that need to be meaningfully assessed and considered.</li> </ol> <p><b><u>Attachment to REBA submission prepared by Nigel Bancroft</u></b></p> <p>Information report on the City of Busselton Local Planning Scheme 21 development controls as applicable to Lot 16 Seaview Rise, Eagle Bay in respect to Development Proposal shown on Doc set 5250205 v1 (31/10/23).</p> <p>This overview is provided for information purposes based on a standard statutory planning assessment of the relevant development controls as applicable to a proposal within the Eagle Bay Special Character Area under City of Busselton Local Planning Scheme 21 (LPS). It is not legal advice and should not be interpreted as such. It is designed to inform REBA on the structure and hierarchy of the controls applicable to the appropriate statutory development assessment and determination of the application for a single dwelling at lot 16 Seaview Rise as shown on advertised plans (Doc set 5250205).</p> <p>The presentation of this overview is based on extracts of the City of Busselton TPS 21 (LPS) and associated components of the planning framework to identify precedence of controls and limitations.</p>	<p><b>Power to vary scheme standard - LPS 21, Clause 4.5</b></p> <p>Power to vary scheme standards has been addressed in Attachment 3 (Officer Assessment Report). LPS 21, Clause 4.5 has been addressed and it has been demonstrated that the development proposal complies with this clause.</p> <p><b>Special Provision Area</b></p> <p>Compliance with Special Provision Area No. 6 is demonstrated within Attachment 4 (Officer Assessment Report)</p> <p><b>Landscape Value Area</b></p> <p>It has been considered that adequate supporting information has been provided (Development Plans and Landscape Plan).</p> <p>As demonstrated within Attachment 4 (Officer Assessment Report), the proposal has been fully assessed against LPS 21, Clause 5.4 Landscape Value Area (LVA), has been found to satisfy the requirements of the LVA.</p> <p>The proposal will also be finished with prescribed materials (dark tones) which is in keeping with Local Planning Policy – 3.1 Reflective Building Materials.</p> <p><b>Special Character Area Provisions</b></p> <p>As demonstrated within Attachment 4 (Officer Assessment Report), the proposal complies with the provision of the Special Character Area.</p> <p><b>Eagle Bay Road setback</b></p> <p>As demonstrated within Attachment 4 (Officer Assessment Report), the proposed setback will not result in any adverse visual or amenity impact.</p> <p>In accordance with the <i>Planning and Development (Local Planning Schemes) Regulations 2015, Schedule 2, clause.67 (Consideration of application by local government) and clause 27(1) of the Deemed Provisions</i>: the structure plan is a "due regard" planning instrument only, therefore there is discretion to vary any provisions contained within the structure plan.</p> <p>Clause 4.5 of the Scheme provides discretion to vary site and development standard and requirements within the Scheme and it is considered that the provisions contained within Special Provision Special Control Areas are therefore able to varied.</p> <p>Therefore, as it is considered there is discretion to vary these provisions it is considered that a merit based assessment to determine the appropriateness of these discretions must be undertaken. The matters listed in clause 67 of the Regulations should be considered in the assessment of these discretions. Clause 67 has been assessed within</p>

**SUMMARY OF SUBMISSIONS**  
**PROPOSAL: Single House (Swimming Pool, Gym and Outbuilding) - Lot 16 (No. 6) Seaview Rise, Eagle Bay**  
**SUBMISSIONS CLOSE: 08 December 2023**

	<p>The subject lot is zoned R2.5 under the LPS, is within the Eagle Bay Special Character Area (EBSCA), Special Provision Area No. 6 (SP6) and is within the Landscape Value Area (LVA). It is also subject to an endorsed structure plan (Eagle Bay Structure Plan, Plan Reference No. SP 29) endorsed by the COB 2/4/2007 and the WAPC 27/06/2007 (and subject to the deemed provisions then valid until Oct 2025).</p> <p>The subject lot is within Precinct 3 of the Eagle Bay SCA Policy and Area C of App 4 of that policy Eagle Bay Special Character Area DGP and Building Envelope Plan Overlay. The SCA Policy has as its purpose to protect and enhance the unique and special character of Eagle Bay through land use and residential design controls. However, the Policy does not appear to have been updated following the gazettal of LPS21 and while it purports that the policy provisions prevail to the extent of any inconsistency with other elements of the planning framework this is not considered to accurately reflect the current status of the policy (other than those elements reflected in Schedule 4 of the LPS or other scheme provisions). The basis for this is related to cl 5.7 of the LPS, the requirements for setting out R code variations in an LPS and that the policy is a lower order planning document. In conjunction with the provisions of SP6 this does create a fairly complicated hierarchy of planning controls. To the extent necessary to identify the extent of compliance of the proposal with the scheme these are dealt with on a "development element" basis below. Further discussion on the hierarchy of the planning controls is contained in the previous information advice provided to REBA in respect of the proposal for 301 Carnarvon Castle Drive. For ease of reference the main elements of the scheme structure as are applicable are outlined below:</p> <p><b>Compliance with Development Standards</b></p> <p>Clause 4 of the LPS establishes the precedence of the application of the minimum standards of the scheme to any development and the applicability of the Residential Design Codes to the subject land as it is within the residential zone (R 2.5). It should be noted that where the R-codes are not varied by a provision of the scheme, including schedule 4, then the standards of the codes (to the extent they exist) and/or design principles are applicable. Cl 4.3.1 (g) as related to R code variations (reproduced below) is also relevant to this proposal as it provides an additional control over the spread of development on the subject site that works in addition to the Schedule 4 setback requirements.</p> <p style="margin-left: 40px;"><b>4.3 MODIFICATION OF R-CODES</b> <i>(Amendment No. 1 – GG, 4 Aug 2017)</i> <i>(Amendment No. 29 – GG, 7 Jun 2019)</i> <i>(Amendment No. 28 – GG, 16 Feb 2021)</i> <i>(Amendment No. 50 – GG 22 Jul 2022)</i></p> <p style="margin-left: 40px;">4.3.1 Notwithstanding any other provision of the Scheme, the following variations and exclusions to the R-Codes apply:</p> <p style="margin-left: 80px;">(e) All Special Character areas provided for in Schedule 4 and pursuant to clause 5.7 are excluded as necessary from the provisions of the R-Codes to the extent of any conflict between the standards set out in Schedule 4 and the R-Codes.</p> <p style="margin-left: 80px;">(f) Notwithstanding the deemed to comply provisions of the R-Codes, outbuildings that do not exceed a wall height of 2.7 metres and ridge height of 4.5 metres are deemed to meet the relevant performance criteria.</p> <p style="margin-left: 80px;">(g) On land coded R2, R2.5 or R5, all building development is to be contained within a building envelope area which does not exceed 1,000m<sup>2</sup>, or as otherwise required by a plan adopted pursuant to Part 4 of the Deemed Provisions.</p> <p>Clause 4.5 provides the power for the Council to issue approval for a development that does not comply with a standard of the scheme generally. Certain elements of the codes are excluded from this power (very limited given the design principles pathway available) as are development standards set out in Schedule 4 for land within a SCA. The preeminence of these Schedule 4 planning controls over the general power to vary a scheme standard was confirmed in the Pennock decision.</p> <p><b>Power to vary scheme standards</b></p> <p style="margin-left: 40px;"><b>4.5 VARIATIONS TO SITE &amp; DEVELOPMENT STANDARDS AND REQUIREMENTS</b> <i>(Amendment No. 1 – GG, 4 Aug 2017)</i> <i>(Amendment No. 29 – GG, 7 Jun 2019)</i></p> <p style="margin-left: 40px;">4.5.1 Except for development in respect of which the R-Codes apply, if a development is the subject of an application for development approval and does not comply with a standard or requirement prescribed under the Scheme (including a policy or plan adopted pursuant to the Scheme), the local government may, despite the non-compliance, approve the application unconditionally or subject to such conditions as the local government thinks fit.</p>	<p>Attachment 4 (Officer Assessment Report) and are considered to be satisfied.</p> <p><b>Building Height</b></p> <p>An assessment (Attachment 4) has been completed which justified consideration of the height variation.</p> <p>It is also worth noting that the neighbouring property, Lot 38 (No.4 Seaview Rise), adjoining the north/ northwest boundary of the Site has a varied building height (Approved 2018). At its highest points the neighbouring dwelling measured 8.1m, 8.5m and 8.8m, 0.1m higher than the proposed dwelling.</p> <p>It is considered that due to the design, layout, sloping site and extensive landscaping plan proposed the height of the building is acceptable. The highest point of the dwelling is set back from neighbours to the north by 15.4m which is considered an adequate distance and will not result in overlooking or over shadowing. The height points of the dwelling (elevations over 7.5m) are setback from Eagle Bay Road by approximately 54m and setback from Seaview Rise by approximately 22m- 37m.</p> <p>It is considered that a 7.5m high limit is not required on this site, the design, location and landscaping sustainably integrates the proposed dwelling on to this site without detrimental impact on the character of the area or residential amenity, which are considered to be the key outcome to be achieved via the clauses set out in the Eagle Bay Structure Plan.</p> <p><b>Landscape Value Area</b></p> <p>As state above:</p> <p>It has been considered that adequate supporting information has been provided (Development Plans and Landscape Plan).</p> <p>As demonstrated within Attachment 4 (Officer Assessment Report), the proposal has been fully assessed against LPS 21, Clause 5.4 Landscape Value Area (LVA), has been found to satisfy the requirements of the LVA.</p> <p>The proposal will also be finished with prescribed materials (dark tones) which is in keeping with Local Planning Policy – 3.1 Reflective Building Materials.</p> <p><b>Visual Impact</b></p> <p>It is considered that the design and layout of the proposed development appropriately response to the natural contours of the lot. This Site rises from the north to the south, as shown on the survey plan (from 6.3 AHD to 6.7 AHD).</p> <p>The proposed dwelling is not on or near any ridgeline. It has been located on site so that it is not on or near the highest site contour.</p> <p>It is considered the design, siting and scale of the proposed development will not detrimentally impact the visual amenity of the area. The Landscaping Plan will also help integrate the proposal in to the area, further</p>
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**SUMMARY OF SUBMISSIONS**  
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	<p>The exercise of the power in this clause does however have specific associated obligations and required assessments and formation of opinions which must be clearly undertaken in issuing a valid determination.</p> <p>4.5.3 In considering an application for development approval under this clause, where, in the opinion of the local government, the variation is likely to affect any owners or occupiers in the general locality or adjoining the site which is the subject of consideration for the variation, the local government is to -</p> <ul style="list-style-type: none"> <li>(a) consult the affected parties by following one or more of the provisions for advertising uses under clause 64 of the Deemed Provisions; and</li> <li>(b) have regard to any expressed views prior to making its determination to grant the variation.</li> </ul> <p>4.5.4 The power conferred by this clause may only be exercised if the local government is satisfied that -</p> <ul style="list-style-type: none"> <li>(a) approval of the proposed development would be appropriate having regard to the matters to be considered set out in clause 67 of the Deemed Provisions;</li> <li>(b) approval of the proposed development would be consistent with the orderly and proper planning of the locality and the preservation of the amenities of the locality; and</li> <li>(c) the non-compliance will not have an adverse effect upon the occupiers or users of the development, the inhabitants of the locality or the likely future development of the locality.</li> </ul> <p><b>Building Height</b> Clauses 4.3.2 and 4.8 provide the overall building height provisions of the scheme. For the subject land the applicable standards are set out in Schedule 4 (by reference) as these provide a more specific minimum standard and as per CL 5.7.3 (see below) take precedence.</p> <p>4.3.2 Building height provisions as specified under -</p> <ul style="list-style-type: none"> <li>(a) Table 3 and Deemed-to-Comply provision 5.1.6 C6 of Volume 1 of the R-Codes, and</li> <li>(b) Table 2.1, and acceptable Outcome A2.2.1 of Volume 2 of the R-Codes; do not apply, except for on land coded R-AC3. In all other areas, maximum building height requirements are required to comply with the provisions of clause 4.8 of the Scheme. <i>(Amendment No.50 – GG. 22 Jul 2022)</i></li> </ul> <p><b>4.8 HEIGHT OF BUILDINGS</b> <i>(Amendment No. 1 – GG. 4 Aug 2017)</i> <i>(Amendment No. 29 – GG. 7 Jun 2019)</i> <i>(Amendment No. 50 – GG. 22 Jul 2022)</i></p> <p>4.8.1 A person must not erect any building that -</p> <ul style="list-style-type: none"> <li>(a) contains more than two storeys or exceeds a height of 9 metres above natural ground level, where land is within 150 metres of the mean high water mark; or</li> <li>(b) contains more than three storeys or exceeds a height of 12 metres above natural ground level, where land is more than 150 metres from the mean high water mark, except where otherwise provided for in the Scheme. <i>(Amendment No. 50 – GG. 22 Jul 2022)</i></li> </ul> <p><b>Special Provision Area</b> The subject lot is also within Special Provision Area No. 6 as outlined on the scheme maps and as such the following scheme provision and schedule are applicable. These provision however are subject to the other provisions of the scheme and as such are subsidiary to any controls in Schedule 4, to the extent of any conflict, and are open to variation through approval of non-compliant applications under Cl 4.5. ie these SP6 provisions and standards do not have the pre-eminence of the Schedule 4 provisions.</p> <p>5.3.1. Notwithstanding any other provisions of the Scheme, use and development of land identified on the Scheme map within a Special Provision area and specified in Schedule 3, shall be</p>	<p>protecting visual amenity.</p> <p><b>Building Envelope</b> The Clause requires all building development is to be contained within a building envelope area which does not exceed 1,000m<sup>2</sup>, or as otherwise required by a plan adopted pursuant to Part 4 of the Deemed Provisions (Structure Plan).</p> <p>There is no designated building envelope for this lot, the proposed building development is calculated to be 1194m<sup>2</sup>, 194m<sup>2</sup> over the 1000m<sup>2</sup>. To consider the appropriateness of exercising discretion the R-Codes guide the extent of building development within the Residential zone and the proposal complies with the Open Space requirement, including the Design Principles of Open Space and all setbacks to neighbouring properties.</p> <p>It is considered appropriate to exercise discretion to allow for the 194m<sup>2</sup> increase in building development. Following the above it has been demonstrated that the proposal fully complies with the intent of Clause 4.3.1 (g).</p>
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**SUMMARY OF SUBMISSIONS**  
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		<table border="1"> <thead> <tr> <th>No.</th> <th>PARTICULARS OF LAND</th> <th>ZONE</th> <th>SPECIAL PROVISIONS</th> </tr> </thead> <tbody> <tr> <td>SP6</td> <td>Lot 5 &amp; Portion of Lot 30 Eagle Bay Rd, Eagle Bay</td> <td>Residential (R2 and R2.5)</td> <td> <p><b>GENERAL RULES</b></p> <ol style="list-style-type: none"> <li>Subdivision and development of the lots shall generally be in accordance with the Structure Plans adopted by the local government and approved by the WA Planning Commission.</li> <li>No further subdivision of lots will be supported by the local government.</li> <li>The standard minimum building setbacks shall be 15 metres from the front boundary and 10 metres from all other boundaries where setbacks are not specified on the Structure Plan.</li> <li>No building shall be constructed closer to Eagle Bay Road than the setbacks specified on the Structure Plan, with the exception of Lots 12, 13 and 14 which shall have a minimum setback of 30 metres from Eagle Bay Road.</li> <li>No effluent disposal area or buildings on lots abutting Meelup Reserve shall be set back less than 40 metres from the common boundary within Meelup Reserve.</li> <li>All outbuildings are to be contiguous with the main dwelling in terms of building materials, colour and form and shall be located within the building envelope indicated for each lot on the Subdivision Guide Plan and shall be limited to single storey development.</li> <li>Unless otherwise approved by the local government, each dwelling shall be connected to a water storage tank of not less than 135,000 litre capacity. The total catchment for the tank shall be not less than 150 m<sup>2</sup>. Water tanks shall be fitted with couplings compatible with Department of Fire and Emergency Services requirements so that the total tank capacity is available for fire fighting purposes at any given time.</li> <li>Rain water storage tanks are to be sited within the building envelope and form part of the dwelling and outbuilding group. Such tanks shall be painted mid green or similar and screened with vegetation to the satisfaction of the local government.</li> <li>The local government shall require each application for a building licence to be accompanied by a landscaping plan for the lot unless</li> </ol> <p><b>SPECIAL PROVISIONS</b></p> <p>such lot is substantially vegetated, indicating the areas to be subject to tree planting and revegetation in accordance with the Structure Plan.</p> <ol style="list-style-type: none"> <li>Pre-fabricated kit homes, relocated and transportable dwellings are not permitted.</li> <li>Dwelling houses must be limited to a maximum height of 7.5m above the natural ground level at any point, with any second storey (first floor) to comprise a gross floor area of not more than 50% of the gross ground floor area. On lots 12, 13 and 14 the maximum dwelling height shall be limited to the general tree canopy height on the lot and in any event must not exceed 7.5 m.</li> </ol> </td> </tr> </tbody> </table> <p><b>Landscape Value Area</b></p> <p>The LVA provisions (CL 5.4) act in addition to all other applicable provisions of the LPS in respect to the subject land and as they are not in conflict in any way with the Schedule 4 provisions, act in conjunction with those and the applicable Eagle Bay SCA policy provisions. The provisions provide important considerations in terms of the potential impact of a proposal on the scenic value and visual amenity of an area but do have a limited implementation scope. I have not made an assessment of consistency of the proposal with these provisions due to a lack of a visual impact assessment with the advertised plans but it is an issue that REBA may wish to pursue in any further consultation with the City on the proposal as it is my view that the detail provided to date is inadequate for the Council to determine the application under its obligations as set out in this clause.</p>	No.	PARTICULARS OF LAND	ZONE	SPECIAL PROVISIONS	SP6	Lot 5 & Portion of Lot 30 Eagle Bay Rd, Eagle Bay	Residential (R2 and R2.5)	<p><b>GENERAL RULES</b></p> <ol style="list-style-type: none"> <li>Subdivision and development of the lots shall generally be in accordance with the Structure Plans adopted by the local government and approved by the WA Planning Commission.</li> <li>No further subdivision of lots will be supported by the local government.</li> <li>The standard minimum building setbacks shall be 15 metres from the front boundary and 10 metres from all other boundaries where setbacks are not specified on the Structure Plan.</li> <li>No building shall be constructed closer to Eagle Bay Road than the setbacks specified on the Structure Plan, with the exception of Lots 12, 13 and 14 which shall have a minimum setback of 30 metres from Eagle Bay Road.</li> <li>No effluent disposal area or buildings on lots abutting Meelup Reserve shall be set back less than 40 metres from the common boundary within Meelup Reserve.</li> <li>All outbuildings are to be contiguous with the main dwelling in terms of building materials, colour and form and shall be located within the building envelope indicated for each lot on the Subdivision Guide Plan and shall be limited to single storey development.</li> <li>Unless otherwise approved by the local government, each dwelling shall be connected to a water storage tank of not less than 135,000 litre capacity. The total catchment for the tank shall be not less than 150 m<sup>2</sup>. Water tanks shall be fitted with couplings compatible with Department of Fire and Emergency Services requirements so that the total tank capacity is available for fire fighting purposes at any given time.</li> <li>Rain water storage tanks are to be sited within the building envelope and form part of the dwelling and outbuilding group. Such tanks shall be painted mid green or similar and screened with vegetation to the satisfaction of the local government.</li> <li>The local government shall require each application for a building licence to be accompanied by a landscaping plan for the lot unless</li> </ol> <p><b>SPECIAL PROVISIONS</b></p> <p>such lot is substantially vegetated, indicating the areas to be subject to tree planting and revegetation in accordance with the Structure Plan.</p> <ol style="list-style-type: none"> <li>Pre-fabricated kit homes, relocated and transportable dwellings are not permitted.</li> <li>Dwelling houses must be limited to a maximum height of 7.5m above the natural ground level at any point, with any second storey (first floor) to comprise a gross floor area of not more than 50% of the gross ground floor area. On lots 12, 13 and 14 the maximum dwelling height shall be limited to the general tree canopy height on the lot and in any event must not exceed 7.5 m.</li> </ol>	
No.	PARTICULARS OF LAND	ZONE	SPECIAL PROVISIONS								
SP6	Lot 5 & Portion of Lot 30 Eagle Bay Rd, Eagle Bay	Residential (R2 and R2.5)	<p><b>GENERAL RULES</b></p> <ol style="list-style-type: none"> <li>Subdivision and development of the lots shall generally be in accordance with the Structure Plans adopted by the local government and approved by the WA Planning Commission.</li> <li>No further subdivision of lots will be supported by the local government.</li> <li>The standard minimum building setbacks shall be 15 metres from the front boundary and 10 metres from all other boundaries where setbacks are not specified on the Structure Plan.</li> <li>No building shall be constructed closer to Eagle Bay Road than the setbacks specified on the Structure Plan, with the exception of Lots 12, 13 and 14 which shall have a minimum setback of 30 metres from Eagle Bay Road.</li> <li>No effluent disposal area or buildings on lots abutting Meelup Reserve shall be set back less than 40 metres from the common boundary within Meelup Reserve.</li> <li>All outbuildings are to be contiguous with the main dwelling in terms of building materials, colour and form and shall be located within the building envelope indicated for each lot on the Subdivision Guide Plan and shall be limited to single storey development.</li> <li>Unless otherwise approved by the local government, each dwelling shall be connected to a water storage tank of not less than 135,000 litre capacity. The total catchment for the tank shall be not less than 150 m<sup>2</sup>. Water tanks shall be fitted with couplings compatible with Department of Fire and Emergency Services requirements so that the total tank capacity is available for fire fighting purposes at any given time.</li> <li>Rain water storage tanks are to be sited within the building envelope and form part of the dwelling and outbuilding group. Such tanks shall be painted mid green or similar and screened with vegetation to the satisfaction of the local government.</li> <li>The local government shall require each application for a building licence to be accompanied by a landscaping plan for the lot unless</li> </ol> <p><b>SPECIAL PROVISIONS</b></p> <p>such lot is substantially vegetated, indicating the areas to be subject to tree planting and revegetation in accordance with the Structure Plan.</p> <ol style="list-style-type: none"> <li>Pre-fabricated kit homes, relocated and transportable dwellings are not permitted.</li> <li>Dwelling houses must be limited to a maximum height of 7.5m above the natural ground level at any point, with any second storey (first floor) to comprise a gross floor area of not more than 50% of the gross ground floor area. On lots 12, 13 and 14 the maximum dwelling height shall be limited to the general tree canopy height on the lot and in any event must not exceed 7.5 m.</li> </ol>								

**SUMMARY OF SUBMISSIONS**  
**PROPOSAL: Single House (Swimming Pool, Gym and Outbuilding) - Lot 16 (No. 6) Seaview Rise, Eagle Bay**  
**SUBMISSIONS CLOSE: 08 December 2023**

	<p><b>5.4 LANDSCAPE VALUE AREA</b> <i>(Amendment No. 40 - OG, 30 July 2022)</i></p> <p>5.4.1 Development within the Landscape Value Area requires the prior development approval of the local government.</p> <p>5.4.2 The local government shall not grant development approval for the clearing or development of any land identified within a Landscape Value area on the Scheme map, unless it has considered -</p> <ul style="list-style-type: none"> <li>(a) whether the development will be compatible with the maintenance and enhancement, as far as is practicable, of the existing rural and scenic character of the locality;</li> <li>(b) whether the development will materially affect any wildlife refuge, significant wetland, coastal environment or any identified site containing Aboriginal archaeological relics; and</li> <li>(c) disturbance to the natural environment, including -             <ul style="list-style-type: none"> <li>(i) visual effects of clearing for development;</li> <li>(ii) maintenance of rural character; and</li> <li>(iii) habitat disturbance.</li> </ul> </li> </ul> <p>5.4.3 The local government shall not grant development approval for the carrying out of development on land within the Landscape Value area or on land on or near any ridge-lines where, in the opinion of the local government, that development is likely to substantially detract from the visual amenity of the area, having regard to, among other things, the cumulative visual effect of the development related to other development that may be anticipated in the locality and in the area generally.</p> <p>5.4.4 Before granting development approval for the erection of a building on land within the Landscape Value area, the local government shall make an assessment as to whether it should impose conditions relating to -</p> <ul style="list-style-type: none"> <li>(a) the siting of the proposed building;</li> <li>(b) the use of prescribed materials on the external surfaces of the building; and</li> <li>(c) the number, type and location of existing trees and shrubs which are to be retained and the extent of landscaping to be carried out on the site.</li> </ul> <p>5.4.5 In clause 5.4.4 -</p> <p><b>"external surfaces"</b> means the external walls and cladding (if any), external doors, external floor and window frames, eaves, roofs, fences and any surface of a building or work visible from the exterior of a building or work; and</p> <p><b>"prescribed materials"</b> means materials with dark tones or dark colouring and of low reflective quality or materials which are painted or similarly treated with dark toned or dark coloured paint or pigment of low reflective quality.</p> <p><b>Special Character Area Provisions</b></p> <p>Clause 5.7 sets out the precedence of the Special Character Area provisions over other more general provisions of the scheme for development within the specified areas. As per the Pennock decision it has also been confirmed that the standards specified in Schedule 4 are not open to variation through approval of a non-compliant proposal, i.e. it is not open to the approving authority to exercise discretion to vary a minimum standard specified in Schedule 4.</p> <p><b>5.7 SPECIAL CHARACTER AREA</b> <i>(Amendment No. 46 - GG, 30 July 2023)</i></p> <p>5.7.1 Development within the Special Character Area requires the prior development approval of the local government.</p> <p>5.7.2 The local government may devise development objectives, provisions and/or controls to reinforce, retain or change the characteristics, form and nature of a designated Special Character area, with such objectives, provisions and/or controls being specified in Schedule 4.</p> <p>5.7.3 Where such objectives, provisions and/or controls are specified in Schedule 4 in relation to a designated Special Character area, those objectives, provisions and/or controls act in conjunction with the other provisions of this Scheme relevant to that area with the exception that the provisions of Schedule 4 will take precedence over any other provisions of this Scheme.</p> <p><b>Development Compliance</b></p> <p>The specific provisions applicable to development within the Eagle Bay SCA in Schedule 4 are copied below. With respect to Lot 16 Seaview Rise and the current development proposal as advertised the main issue of non-compliance are discussed herein.</p> <p><b>Building Setback:</b></p> <p>Lot 16 is within the Eagle Bay SCA and the area of an endorsed Structure plan. Clause (a) of the Eagle Bay SCA Schedule 4 provisions</p>	
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**SUMMARY OF SUBMISSIONS**  
**PROPOSAL: Single House (Swimming Pool, Gym and Outbuilding) - Lot 16 (No. 6) Seaview Rise, Eagle Bay**  
**SUBMISSIONS CLOSE: 08 December 2023**

	<p>provides that setbacks shall be as per the Table 1 to that clause other than where setbacks are provided in a Structure plan, which then take precedence. This wording in the Schedule 4 provision is clumsy and somewhat repetitive, however it is my view that there is no other reasonable conclusion from the structure of the introductory provision. This does not imply that the whole of the structure plan becomes applicable, only the standards specified for setbacks, which then should be read as a Schedule 4 standard. It is also not considered overly relevant as to the status of the structure plan, the reference to the standard could well be any other document containing specific setbacks for the lot, as long as that was adequately described to be clear on its identification.</p> <p>The various setback provisions applicable to the lot and the basis for this are set out below:</p> <p><b><u>Eagle Bay Road setback.</u></b></p> <p>The required setback is considered to be 40m from the Eagle Bay road reserve as set out on the Eagle Bay Structure Plan (SP.29) in the form of a drawn setback line on the plan. This is applicable by reference under Schedule 4, clause 5 (a) of the LPS. The development proposes a setback of 22m and as such is non-compliant and it is my view that there is no power to vary this requirement under Clause 4.5 due to the structure of Cl 5.7 and as per the Pennock decision.</p> <p>The reference in Table 1 of the Eagle Bay SCA, schedule 4 provisions of a 12m secondary street setback is not applicable due to the deferral to a structure plan standard as set out in Cl 5(a). The SP6 reference to setbacks to Eagle Bay Road to be as set out on an applicable structure plan, is subsidiary to but consistent with the Schedule 4 control, confirming the clear intent of a 40m setback to this road reserve.</p> <p>The proposal does not comply and in my view the non-compliance cannot be approved under LPS CL 4.5.          Seaview Rise setback.</p> <p>The applicable front (Seaview Rise) setback is 20m as set out in Table 1 to Schedule 4 as SP.29 is silent on this requirement. This is consistent with the R code requirement for the R2.5 zoning.          The reference in SP6 to a 15m front setback is subsidiary to the Schedule 4 requirement as per clause 5.7. The proposal achieves a setback of 20m.</p> <p>The proposal is compliant.</p> <p>Northern / Southern side Setback (to Lot 38/Lot 17)</p> <p>The applicable setback for the lot side boundaries is considered to be 10m as established in the SP6 provisions, which are subject to clause 4.5 and non-compliant development could be approved. With setbacks of 11m and 10m to the north and south respectively the proposal is compliant in any event. The relevant structure plan and the Eagle Bay SCA provision in schedule 4 are silent on required side setbacks hence SP6 applies. The R codes require a minimum 7.5m setback only.</p> <p>The proposal is compliant.</p> <p><b><u>Building Height:</u></b></p> <p>As lot 16 is within the Eagle Bay SCA the applicable height limits are as per clause 5 (d) of the Schedule 4 provisions. These provide height limits of 7.5m and 9.0m unless otherwise specified in zoning provisions or a Structure Plan. Provisions in SP6 and the SP.29 requirements are consistent in that they provide for development up to a maximum only of 7.5m above Natural Ground Level (NGL) at any point with any second level (first floor) of development limited to an area equivalent to 50% of the gross floor area of the ground floor. The endorsed structure plan SP.29 is the highest order height control measure which is given precedence through Schedule 4 and LPS clause 5.7.</p> <p>As both the maximum height limit and maximum area of two storey construction have their head of power in the Schedule 4 provisions and are part of the same Planning Policy Statement and both address a height control they are not considered separable, then as clarified by the Pennock decision, there is no capacity for the City to issue approval for a non-compliant development in either area. In my view the fact that the height controls are not specified in numeric terms in provision (d) of schedule 4 but established by reference to the Structure plan does not diminish the effect of Clause 5.7.3 of the LPS as limiting any discretionary determination, as interpreted in the Pennock decision.</p> <p>The proposal has the following maximum heights above NGL as set out on the elevations: (These may be further exceeded as without a</p>	
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**SUMMARY OF SUBMISSIONS**  
**PROPOSAL: Single House (Swimming Pool, Gym and Outbuilding) - Lot 16 (No. 6) Seaview Rise, Eagle Bay**  
**SUBMISSIONS CLOSE: 08 December 2023**

		<p>drawing of the height plane of the development above NGL it is difficult to establish with confidence the max height at any point).</p> <p>North – approx. 9m above NGL. East (Seaview Rise) – approx. 8.6m above NGL South – approx. 7.9m above NGL West – The western elevation (elevation 6) only shows the most western portion of the development, with a maximum height of approx. 4.2m. This however ignores the main dwelling beyond and as indicated in elevation 3 has a max height of approx. 8.3 above NGL.</p> <p>The proposal is non-compliant with the required maximum height of 7.5m above NGL at numerous points and when viewed from all elevations and in my view this non-compliance cannot be approved under LPS c4.5. Further PPS No. 8 of SP.29 provides that “Lots fronting Eagle Bay Road may require height limits of 4.5m to protect the tree canopy line.” This is clearly a height control measure and is considered to be given effect by clause (d) of the SCA controls in schedule 4. The application provides no analysis of the existing tree canopy and the impact of the proposed development and it is considered that as such the proposal does not contain sufficient information for it to be validly assessed and the need for application of the 4.5m height limit applied or otherwise. As proposed the development appears to be compliant with the requirement for the first level of development to not be greater in area than 50% of the gross area of the ground floor. At a proposed approx. 400m<sup>2</sup> the upper level is less than 50% of the ground floor GFA of approx. 904m<sup>2</sup>. However, given the proposal overall exceeds the permitted building envelope area as discussed below this issue would need to be reviewed in any revised proposal.</p> <p><b><u>Building Envelope.</u></b> There is no identification of a building envelope or overall development footprint schedule on the advertised plans for lot 16. The ground floor development has a building only footprint of over 900m<sup>2</sup> with extensive associated recreation and ancillary facilities outside this main footprint likely to result in a development area at least double this.</p> <p>Special Provision area No.6, special provision 6, provides that all outbuildings should be of a contiguous design to the main dwelling and located within the building envelope as set out on a subdivision guide plan. The existence of such a subdivision guide plan is still being determined with the COB???? At this point the proposal is considered compliant in terms of the design of outbuildings only.</p> <p>Clause 4.3(g) of the LPS as reproduced above identifies that on land zoned R 2.5 all building is to be contained within a building envelope area of maximum 1000m<sup>2</sup> or as otherwise set out on a plan adopted under part 4 of the deemed provisions (a structure plan or like). The LPS does not contain a definition of a “building Envelope” other than with reference to rural zones. Within the Planning Regulations and P&amp; D Act the only definition of a Building Envelope is within the Model Scheme Text (37 (1) of Schedule 1) which provides as follows:  “Building Envelope means the area of land within which all buildings and effluent disposal facilities on a lot must be contained.”</p> <p>For the purpose of assessment of the proposal and those elements of development to be considered as defining the building envelope this definition as the only one available is considered to be most applicable. Without being able to be definitive on the overall size of the envelope under this definition as no overall development area has been defined on the advertised plans it is considered that the proposal will far exceed the maximum allowance of 1000m<sup>2</sup> given the immediate ground floor building footprint of approx. 900 m<sup>2</sup>. As such the proposal is non-compliant with the requirements of the LPS. As these requirements are standards of the scheme and SP6 provisions they do not have the mandatory status of schedule 4 provisions and a non-compliant development could be approved after following the correct procedures and subject to Council determination that the variation would be consistent with proper planning and the protection of the amenities of the area etc under clause 4.5.</p> <p>Proposal is non-compliant with maximum building envelope requirements with plans inadequately detailed to determine the exact extent of that non-compliance.</p> <p><b><u>Landscape Value Area.</u></b> The proposal is located within an area identified as a Landscape Value Area under the scheme and as such is subject to the application, assessment and determination requirements of Clause 5.4 of the LPS. While it is acknowledged that further information may have been provided to the CoB as part of the application documents the information on the advertised plans is considered insufficient for the requirements of the LVA and for the City to make a proper assessment of the compliance or otherwise with the required outcomes for</p>	
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**SUMMARY OF SUBMISSIONS**  
**PROPOSAL: Single House (Swimming Pool, Gym and Outbuilding) - Lot 16 (No. 6) Seaview Rise, Eagle Bay**  
**SUBMISSIONS CLOSE: 08 December 2023**

development within the LVA. This information should include at least a basic seen area analysis of the development from public areas to address the requirement to protect the visual amenity of the area.

Further information on the proposal is considered to be required to enable the application to be fully considered consistent with the scheme requirements. There is not considered to be any discretion available in the determination of the application to ignore the clear requirements of LPS Cl 5.4.

**Conclusion**

This report has not assessed compliance of the development with other subsidiary components of the planning framework such as the R-codes except where detailed above. Given the setbacks of the proposed development from adjoining lots and the 'design principles' pathway under the codes such issues are considered unlikely to be significant in the determination of the application.

In summary the proposal is considered to be non-compliant in respect to the setback of buildings to Eagle Bay Road and in terms of the maximum height above NGL. Both of these development standards are established through the Eagle Bay SCA provisions in Schedule 4 and override any other provision of the scheme as per LPS Cl 5.7. As confirmed in the Pennock decision these development standards are not able to be subject to variation through approval of a non-compliant development under LPS cl4.5. Correspondingly it is my view that on a basic statutory planning interpretation of the scheme the Council would have no power to approve the application as represented in the advertised plans.

Further it is considered that the application is not valid as advertised given the inadequacy of information to address the requirements of LPS CL5.4, LVA requirements and would be unable to be determined by the Council until the full requirements of LPS CL 5.4 are addressed.

In respect to the requirements for all building in the R2.5 zone to be contained to an envelope of 1000m2, while a non-compliant development could be approved pursuant to LPS cl4.5 it is considered that the extent of non-compliance based on the advertised plans, once confirmed, would be of a scale and detrimental impact on the amenity and character of the area that it would prevent any reasonable decision maker from approving the extent of variation that would be required.

**5. EAGLE BAY SPECIAL CHARACTER AREA**  
*(Amendment No. 1 – GG, 4 Aug 2017)*

The following provisions shall apply to subdivision and development within the Eagle Bay Special Character Area as identified on the Scheme map--

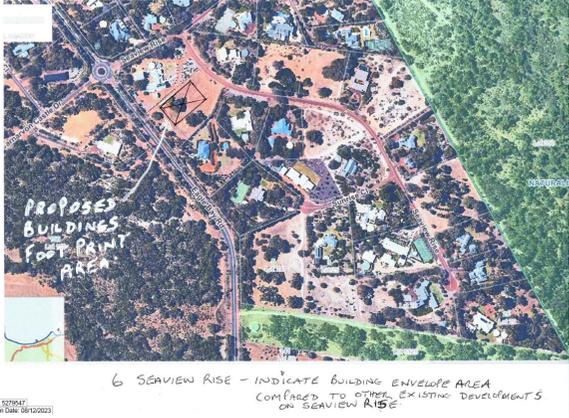
- (a) Other than provided for in a Structure Plan, Approved Building Envelope Plan or Structure Plan, development within the various planning sectors, as outlined in the Eagle Bay Special Character Area Local Planning Policy, shall comply with the following setback requirements:

Sector	Density	Minimum Setbacks for Development (in metres)		
		Front	Rear	Secondary Street
Sector 1	R5 Density	9.0*	5.0	4.5
Sector 2	R5 Density	15.0	5.0	6.0
	R2 Density	15.0	10.0	10.0
Sector 3	R2.5 Density	20.0	5.0	12.0
	R2 Density	20.0	10.0	12.0
Sector 4	Rural Residential	As per Structure Plan	As per Structure Plan	

\*In determining the appropriate front setback, consideration shall be given to the setback of dwellings on neighbouring/nearby properties and the impact a 3.0m front setback may have on the existing streetscape.

- (b) Incidental development shall be setback from front and neighbouring boundaries, including boundaries which interface with coastal foreshore and other public reserves, to protect the prevailing amenity of the locality in accordance with Table 1.
- (c) Where a lot abuts a foreshore or public open space reserve, the setback applicable to development shall be no less than the required front setback distance in order to retain the natural amenity of the Reserve and avoid the encroachment of development on the reserve area.
- (d) The applicable height limits will be 7.5 metres within 150 metres of the mean high water mark and 9.0 metres for all other areas unless otherwise determined by zoning provisions, a Structure Plan.

**SUMMARY OF SUBMISSIONS**  
**PROPOSAL: Single House (Swimming Pool, Gym and Outbuilding) - Lot 16 (No. 6) Seaview Rise, Eagle Bay**  
**SUBMISSIONS CLOSE: 08 December 2023**

			
2.	<p>Name: S. Bailey</p> <p>Locality: Eagle Bay</p>	<p><b>Objection</b></p> <p>The proposed plan for the property at 6 Seaview Rise, Eagle Bay is within precinct 3 of the Eagle Bay Special Character Area (SCA) Policy. The proposed development of this property is outside the building envelope of 7.5m height above natural ground level. The proposed development at 6 Seaview Rise proposes to build up the block height with fill material on the Seaview Rise side of the block. The proposed development is then a 3 story home 9m in height which, is currently existing natural ground level at the front of the block. According to the originally approved structure plan subdivision along Seaview Rise the maximum height is 7.5m this was to basically to limit buildings in this development to a maximum of 2 storeys and to date this plan has worked well for the area. This property will have a large footprint of greater than 1000m<sup>2</sup> whereas the other properties in the area are around 500m<sup>2</sup>. There is no provision that I can see for a water tank which would be environmentally essential considering the number of trees being planted and fed from drip reticulation, the water pressure is extremely low already planting these will put further pressure on the system. The developers also plan to remove 2 extra- large gum trees which will impact the privacy of number 10 Seaview Rise, Eagle Bay. Eagle Bay is also defined as a Landscape Value Area (LVA) under the City of Busseton Town Planning Scheme, this is to ensure that any new developments fit in with the existing character of the area.</p>	<p><b>Response as above, additional comments:</b></p> <p><b>Fill</b> Number 6 Seaview rises from the north (63.5AHD) to the south (67.10AHD). Proposed FFL: Outbuilding garage - 66.214AHD Dwelling - 66.30AHD Gym – 66.30AHD Outdoor living – 66.643AHD</p> <p>It is considered the nominated FFL's are an appropriate design response to the natural contour of the lot. The proposal only required the minimal cut and fill to allow for a level build.</p> <p>The proposed dwellings (at its closest point to neighbouring property) is set back 25m from number 10 (Lot 18). Number 10 is currently separated from the proposal by established vegetation on both Lots, and a driveway to number 6 (Lot 17).</p> <p>This along with the proposed landscaping plan will adequately protect the privacy of Number 10.</p> <p>Given the design, scale, layout and landscaping plans provided it is considered that the proposed development is in keeping with the existing character of the area. No adverse impacts are anticipated.</p>
3.	M. Vincent	<p><b>Objection.</b></p>	<p>The property is designated as being within Category 2 under the City's</p>

**SUMMARY OF SUBMISSIONS**  
**PROPOSAL: Single House (Swimming Pool, Gym and Outbuilding) - Lot 16 (No. 6) Seaview Rise, Eagle Bay**  
**SUBMISSIONS CLOSE: 08 December 2023**

	Locality : Eagle Bay	No issue with proposed development but concerns regarding landscaping and compliance with firebreaks. Concerns regarding proximity of trees to septic tanks and leach drain.	<p>Firebreak and Fuel Hazard Reduction Notice (Bushfire Notice). The proposed landscaping plan comply with the requirements of the City's Bushfire Notice which requires a mineral earth firebreak 3m in width within 6m of all external lot boundaries (note: the firebreak does not need to be located on the lot boundary).</p> <p>The Bushfire Consultant has confirmed that the landscaping plan is bushfire compliant.</p> <p>The location of the septic tanks and leach drains will be subject to a separate application within the City's Environmental Health Services where the location if the septic and leach drains will be assessed.</p>
4.	C. Donaldson Locality : Eagle Bay	<p><b>Objection.</b></p> <p>The development does not comply with maximum height control of 7.5m above NGL. The development does not comply with the building envelope requirements</p>	<p>Building height has been assessed in the Council Report.</p> <p>Within the Scheme the applicable building envelope is 1,000sqm. The building envelope has been assessed 1,194sqm. This is considered appropriate as all other aspect, including open space and setbacks, comply with the deemed-to-comply criteria of the R-codes.</p>





**CITY OF BUSSELTON  
MONTHLY FINANCIAL REPORT  
For the period ended 30th November 2023**

**LOCAL GOVERNMENT ACT 1995  
LOCAL GOVERNMENT (FINANCIAL MANAGEMENT) REGULATIONS 1996**

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**City of Busselton**

**Statement of Financial Activity**

**Year to Date As At 30th November 2023**

Note	2023/2024 Actual YTD	2023/2024 Amended Budget YTD	2023/2024 Original Budget YTD	2023/2024 Amended Budget	2023/2024 Original Budget	2023/24 YTD Bud (A) Variance	2023/24 YTD Bud (A) Variance
	\$	\$	\$	\$	\$	%	\$
<b>Operating Activities</b>							
<b>Revenue from Operating Activities</b>							
Rates	60,168,019	60,182,215	60,182,215	60,395,882	60,395,882	(0.02%)	(14,196)
Grants, Subsidies and Contributions	3.1 1,896,870	673,133	673,133	5,325,685	5,325,685	181.80%	1,223,737
Fees & Charges	14,693,761	13,571,456	13,571,456	22,404,393	22,404,393	8.27%	1,122,305
Other Revenue	3.2 214,319	84,947	84,947	334,932	334,932	152.30%	129,372
Interest Earnings	3.3 2,764,469	2,209,803	2,209,803	4,305,492	4,305,492	25.10%	554,666
Profit on Asset Disposals	97,808	5,754	5,754	24,120	24,120	1599.83%	92,054
	<b>79,835,246</b>	<b>76,727,307</b>	<b>76,727,307</b>	<b>92,790,504</b>	<b>92,790,504</b>	<b>4.05%</b>	<b>3,107,938</b>
<b>Expenses from Operating Activities</b>							
Employee Costs	(15,826,501)	(16,079,820)	(16,079,820)	(37,929,304)	(37,929,304)	1.58%	253,319
Materials & Contracts	3.4 (8,172,444)	(11,984,525)	(11,984,525)	(29,072,925)	(29,072,925)	31.81%	3,812,081
Utilities (Gas, Electricity, Water etc)	(1,072,410)	(1,054,300)	(1,054,300)	(2,980,399)	(2,980,399)	(1.72%)	(18,110)
Depreciation on non current assets	(11,288,638)	(10,725,430)	(10,725,430)	(25,541,373)	(25,541,373)	(5.25%)	(563,208)
Finance Costs	(499,473)	(472,394)	(472,394)	(1,749,875)	(1,749,875)	(5.73%)	(27,079)
Insurance Expenses	(883,198)	(818,743)	(818,743)	(894,395)	(894,395)	(7.87%)	(64,455)
Other Expenditure	3.5 (948,636)	(2,105,827)	(2,105,827)	(6,823,844)	(6,823,844)	54.95%	1,157,191
Allocations	431,302	831,918	831,918	1,996,605	1,996,605	48.16%	(400,616)
Loss on Asset Disposals	(62,777)	(32,026)	(32,026)	(205,278)	(205,278)	(96.02%)	(30,751)
	<b>(38,322,776)</b>	<b>(42,441,146)</b>	<b>(42,441,146)</b>	<b>(103,200,789)</b>	<b>(103,200,789)</b>	<b>9.70%</b>	<b>4,118,370</b>
Non-Cash Amounts Excluded from Operating Activities	2 10,104,790	10,751,702	10,751,702	16,372,855	16,372,855	(6.02%)	(646,911)
<b>Amount Attributable to Operating Activities</b>	<b>51,617,260</b>	<b>45,037,863</b>	<b>45,037,863</b>	<b>5,962,570</b>	<b>5,962,570</b>	<b>14.61%</b>	<b>6,579,398</b>
<b>Investing Activities</b>							
Capital Grants, Subsidies and Contributions	3.6 1,196,834	26,430	(73,570)	19,724,075	19,649,075	4428.32%	1,170,404
Land & Buildings	3.7 (6,135,227)	(14,679,375)	(14,679,375)	(29,976,325)	(29,976,325)	58.21%	8,544,148
Plant & Equipment	3.8 (318,751)	(736,305)	(736,305)	(6,078,421)	(6,078,421)	56.71%	417,554
Furniture & Equipment	3.9 (89,564)	(682,725)	(682,725)	(1,510,209)	(1,510,209)	86.88%	593,161
Infrastructure	3.10 (2,839,924)	(8,117,734)	(8,205,651)	(21,994,529)	(21,896,529)	65.02%	5,277,811
Proceeds from Sale of Assets	3.11 204,487	150,300	150,300	863,800	863,800	36.05%	54,187
Self Supporting Loans - Repayment of Principal	53,569	53,572	53,572	193,385	193,385	(0.00%)	(3)
	<b>(7,928,575)</b>	<b>(23,985,837)</b>	<b>(24,173,754)</b>	<b>(38,778,225)</b>	<b>(38,755,225)</b>	<b>66.94%</b>	<b>16,057,262</b>
<b>Financing Activities</b>							
Repayment of Borrowings	(1,422,481)	(1,422,483)	(1,422,483)	(4,738,701)	(4,738,701)	0.00%	2
Principal Elements of Finance Lease Payments	(65,594)	(85,769)	(85,769)	(114,651)	(114,651)	23.52%	20,175
Proceeds from New Loans	3.12 0	1,750,000	1,750,000	2,000,000	2,000,000	100.00%	(1,750,000)
Advances to Community Groups	0	0	0	(250,000)	(250,000)	0.00%	0
Transfer to Restricted Assets	3.13 (14,777,232)	(8,669,978)	0	(9,200,455)	(11,180)	(70.44%)	(6,107,254)
Transfer from Restricted Assets	3.14 6,681,590	8,567,950	8,567,950	22,096,174	20,563,074	(22.02%)	(1,886,360)
Transfer to Reserves	(23,693,742)	(23,789,585)	(23,789,585)	(39,002,737)	(39,002,738)	0.40%	95,843
Transfer from Reserves	3.15 27,113,867	22,640,277	13,970,299	60,071,435	52,392,260	19.76%	4,473,589
	<b>(6,163,592)</b>	<b>(1,009,587)</b>	<b>(1,009,587)</b>	<b>30,861,065</b>	<b>30,838,065</b>	<b>510.51%</b>	<b>(5,154,005)</b>
<b>Opening Funds Surplus/ (Deficit)</b>	1,955,161	1,954,590	1,954,590	1,954,590	1,954,590		
<b>Amount Attributable to Operating Activities</b>	51,617,260	45,037,863	45,037,863	5,962,570	5,962,570		
<b>Amount Attributable to Investing Activities</b>	(7,928,575)	(23,985,837)	(24,173,754)	(38,778,225)	(38,755,225)		
<b>Amount Attributable to Financing Activities</b>	(6,163,592)	(1,009,587)	(1,009,587)	30,861,065	30,838,065		
<b>Net Current Position - Surplus / (Deficit)</b>	2 <b>39,480,255</b>	<b>21,997,028</b>	<b>21,809,112</b>	<b>(0)</b>	<b>0</b>	<b>79.48%</b>	<b>17,483,227</b>

**City of Busselton**  
**Statement of Financial Position**  
**Year to Date As At 30th November 2023**

	2023/24 YTD Actual	2022/23 Actual
	\$	\$
<b>Current Assets</b>		
Cash & Cash Equivalents	43,874,792	21,032,276
Trade & Other Receivables - Current	21,518,556	3,750,792
Other Financial Assets	94,682,579	94,682,579
Inventories	1,272,715	1,272,715
Contract Assets	309,426	309,426
Other Assets	1,221,031	483,906
Non-Current Assets Held for Sale	0	0
<b>Total Current Assets</b>	<b>162,879,099</b>	<b>121,531,695</b>
<b>Non Current Assets</b>		
Other Receivables	647,983	678,222
Other Financial Assets	1,378,368	1,431,938
Property, Plant & Equipment	203,574,788	199,363,753
Infrastructure	646,254,676	651,595,672
Contract Assets	7,672,464	7,672,464
Right of Use Assets Furniture & Equipment	104,784	104,784
<b>Total Non Current Assets</b>	<b>859,633,063</b>	<b>860,846,832</b>
<b>Total Assets</b>	<b>1,022,512,162</b>	<b>982,378,526</b>
<b>Current Liabilities</b>		
Trade & Other Payables	8,284,098	8,873,525
Contract Liabilities	1,266,220	1,266,220
Grant Liabilities	984,500	984,500
Lease Liabilities	51,802	117,396
Borrowings	4,620,953	4,620,953
Employee Provisions	5,885,190	5,885,190
Other Provisions	2,259,750	6,504,000
<b>Total Current Liabilities</b>	<b>23,352,513</b>	<b>28,251,784</b>
<b>Non Current Liabilities</b>		
Sundry Payable	0	0
Contract Liabilities	9,847,373	9,205,220
Grant Liabilities	9,964,989	6,861,059
Lease Liabilities	72,873	72,873
Borrowings	42,198,720	43,621,201
Employee Provisions	577,890	577,890
<b>Total Non Current Liabilities</b>	<b>62,661,845</b>	<b>60,338,242</b>
<b>Total Liabilities</b>	<b>86,014,358</b>	<b>88,590,026</b>
<b>Net Assets</b>	<b>936,497,804</b>	<b>893,788,500</b>
<b>Equity</b>		
Retained Surplus	529,952,642	483,823,213
Reserves	74,718,056	78,138,181
Revaluation Surplus	331,827,106	331,827,106
<b>Total Equity</b>	<b>936,497,804</b>	<b>893,788,500</b>

**CITY OF BUSSELTON**  
**NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY**  
**For the period ended 30th November 2023**

**Note 1 BASIS OF PREPARATION AND SIGNIFICANT ACCOUNTING POLICIES**

**BASIS OF PREPARATION**

This prescribed financial report has been prepared in accordance with the *Local Government Act 1995* and accompanying regulations.

**Local Government Act 1995 requirements**

Section 6.4(2) of the *Local Government Act 1995* read with the *Local Government (Financial Management) Regulations 1996*, prescribe that the financial report be prepared in accordance with the *Local Government Act 1995* and, to the extent that they are not inconsistent with the Act, the Australian Accounting Standards. The Australian Accounting Standards (as they apply to local governments and not-for-profit entities) and Interpretations of the Australian Accounting Standards Board were applied where no inconsistencies exist.

The *Local Government (Financial Management) Regulations 1996* specify that vested land is a right-of-use asset to be measured at cost, and is considered a zero cost concessionary lease. All right-of-use assets under zero cost concessionary leases are measured at zero cost rather than at fair value, except for vested improvements on concessionary land leases such as roads, buildings or other infrastructure which continue to be reported at fair value, as opposed to the vested land which is measured at zero cost. The measurement of vested improvements at fair value is a departure from AASB 16 which would have required the City to measure any vested improvements at zero cost.

*Local Government (Financial Management) Regulations 1996*, regulation 34 prescribes contents of the financial report. Supporting information does not form part of the financial report.

Accounting policies which have been adopted in the preparation of this financial report have been consistently applied unless stated otherwise. Except for cash flow and rate setting information, the financial report has been prepared on the accrual basis and is based on historical costs, modified, where applicable, by the measurement at fair value of selected non-current assets, financial assets and liabilities.

**THE LOCAL GOVERNMENT REPORTING ENTITY**

All funds through which the City controls resources to carry on its functions have been included in the financial statements forming part of this financial report.

All monies held in the Trust Fund are excluded from the financial statements.

**Judgements and estimates**

The preparation of a financial report in conformity with Australian Accounting Standards requires management to make judgements, estimates and assumptions that effect the application of policies and reported amounts of assets and liabilities, income and expenses.

The estimates and associated assumptions are based on historical experience and various other factors believed to be reasonable under the circumstances; the results of which form the basis of making the judgements about carrying values of assets and liabilities that are not readily apparent from other sources. Actual results may differ from these estimates.

The balances, transactions and disclosures impacted by accounting estimates are as follows:

- estimated fair value of certain financial assets
- impairment of financial assets
- estimation of fair values of land and buildings, infrastructure and investment property
- estimation uncertainties made in relation to lease accounting
- estimated useful life of intangible assets

**SIGNIFICANT ACCOUNTING POLICIES**

Significant accounting policies utilised in the preparation of these statements are as described within the 2023-24 Annual Budget. Please refer to the adopted budget document for details of these policies.

**PREPARATION TIMING AND REVIEW**

Date prepared: All known transactions YTD

CITY OF BUSSELTON  
NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY  
For the period ended 30th November 2023

Note 2 STATEMENT OF FINANCIAL ACTIVITY INFORMATION

	Adopted Budget Closing 30-Jun-24	Last Year Closing 30-Jun-23	Year to Date 31-Oct-23
<b>Net current assets used in the Statement of Financial Activity</b>			
<b>Current assets</b>	\$	\$	\$
Cash and cash equivalents	79,919,613	115,532,276	138,374,792
Trade and other receivables	2,523,748	2,734,948	21,518,556
Inventories	1,200,000	1,272,715	1,272,715
	<u>83,643,361</u>	<u>119,539,940</u>	<u>161,166,063</u>
<b>Less: current liabilities</b>			
Trade and other payables	3,765,648	3,765,648	3,191,160
Other liabilities	6,627,934	6,342,753	8,839,021
	<u>10,393,582</u>	<u>10,108,401</u>	<u>12,030,181</u>
<b>Net current assets</b>	<b>73,249,779</b>	<b>109,431,539</b>	<b>149,135,882</b>
<b>Less: Total adjustments to net current assets</b>	(c) (73,249,779)	(107,476,377)	(109,655,627)
<b>Closing funding surplus / (deficit)</b>	<b>0</b>	<b>1,955,161</b>	<b>39,480,255</b>

(a) Non-cash amounts excluded from Net Current Position

The following non-cash revenue and expenditure has been excluded from operating activities within the Statement of Financial Activity in accordance with *Financial Management Regulation 32*.

	Adopted Budget Closing 30-Jun-24	Last Year Closing 30-Jun-23	Year to Date 31-Oct-23
<b>Non-cash amounts excluded from operating activities</b>	\$	\$	\$
<b>Adjustments to operating activities</b>			
Less: Profit on asset disposals	(24,120)	(200,266)	(97,808)
Less: Movement in finance leases	0	(47,238)	0
Less: Movement Other	0	(368,664)	(725,458)
Less: Fair value adjustments to financial assets at amortised cost	0	(6,450)	0
Less: Movement in pensioner deferred rates (non-current)	0	(49,037)	18,573
Less: Movement in employee benefit provisions (non-current)	0	(69,787)	(4,244,250)
Add: Loss on disposal of assets	205,278	346,370	62,777
Less: Movement in other provisions (non-current)	0	6,504,000	0
Add: Depreciation on non-current assets	25,541,373	26,481,887	11,288,638
<b>Total non-cash amounts excluded from operating activities</b>	<b>25,722,531</b>	<b>32,590,815</b>	<b>6,302,471</b>

(b) Non-cash amounts excluded from investing activities

The following non-cash revenue and expenditure has been excluded from investing activities within the Statement of Financial Activity in accordance with *Financial Management Regulation 32*.

<b>Adjustments to investing activities</b>			
Infrastructure received for substantially less than fair value	(9,300,000)	(5,204,907)	(944,669)
Movement in current liabilities for transfers to acquire or construct non-financial assets to be controlled by the entity associated with restricted cash	(49,677)	2,037,309	4,746,988
<b>Total non-cash amounts excluded from investing activities</b>	<b>(9,349,677)</b>	<b>(3,167,599)</b>	<b>3,802,319</b>
<b>Total non-cash amounts excluded from Net Current Position</b>	<b>16,372,854</b>	<b>29,423,216</b>	<b>10,104,790</b>

(c) Current assets and liabilities excluded from budgeted deficiency

The following current assets and liabilities have been excluded from the net current assets used in the Statement of Financial Activity in accordance with *Financial Management Regulation 32* to agree to the surplus/(deficit) after imposition of general rates.

	Adopted Budget Closing 30-Jun-24	Last Year Closing 30-Jun-23	Year to Date 31-Oct-23
<b>Adjustments to net current assets</b>	\$	\$	\$
Less: Reserves/ Restricted - cash backed	(79,877,713)	(113,819,130)	(118,269,470)
- Current portion of contract liability held in restricted assets	1,551,401	1,266,220	1,266,220
- Current portion of unspent capital grants held in restricted assets	984,500	984,500	759,323
- Current portion of performance bonds held in restricted assets	4,092,033	4,092,033	5,092,938
<b>Total adjustments to net current assets</b>	<b>(73,249,779)</b>	<b>(107,476,377)</b>	<b>(111,150,990)</b>

**CURRENT AND NON-CURRENT CLASSIFICATION**

In the determination of whether an asset or liability is current or non-current, consideration is given to the time when each asset or liability is expected to be settled. Unless otherwise stated assets or liabilities are classified as current if expected to be settled within the next 12 months, being the Council's operational cycle.

CITY OF BUSSELTON  
NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY  
FOR THE PERIOD ENDED 30th NOVEMBER 2023

Note 3 Material Variance Explanations

Cost Centre or Project	Description	Actual YTD \$	Budget YTD \$	Full Year Budget \$	Variance YTD %	Variance YTD \$	Change in Variance Current Month \$
<b>Amounts Attributable to Operating Activities</b>		<b>51,617,260</b>	<b>45,037,863</b>	<b>5,962,570</b>	<b>14.61%</b>	<b>6,579,398</b>	<b>1,161,920</b>
<b>Revenue from Operating Activities</b>		<b>79,835,246</b>	<b>76,727,307</b>	<b>92,790,504</b>	<b>4.05%</b>	<b>3,107,938</b>	<b>1,255,797</b>
The material variance items contained within this category, on the face of the FAS, are explained below:							
<b>1</b>	<b>Grants, Subsidies and Contributions</b>	<b>1,896,870</b>	<b>673,133</b>	<b>5,325,685</b>	<b>181.80%</b>	<b>1,223,737</b>	<b>709,196</b>
<b>Corporate Strategy and Performance</b>		<b>343,704</b>	<b>67,933</b>	<b>175,615</b>	<b>405.95%</b>	<b>275,771</b>	<b>244,331</b>
1032	Financial Services Management	133,469	-	-	100.00%	133,469	133,469
This unbudgeted income amount is a result of the transfer of restricted developer contribution funds held aside for activities that were completed/aquitted many years ago. This was discovered as part of the reserve consolidation process.							
1033	Finance	100,951	31,702	100,867	218.43%	69,249	78,566
This positive variance is a result of the transfer of restricted developer contribution funds held aside for activities that were completed/aquitted many years ago. This was discovered as part of the reserve consolidation process.							
1034	Rates	92,183	17,120	39,709	438.44%	75,062	32,631
The positive variance is mainly attributable to the 22/23 adjustment to the Financial Assistance Grants, plus an adjustment paid in relation to the advance payment made in June for the 23/24 year, none of which was not known or could have been predicted at time of budgeting (note that this item should have been allocated against the Finance cost centre).							
<b>Economic and Business Development</b>		<b>249,917</b>	<b>310,358</b>	<b>878,331</b>	<b>(19.47%)</b>	<b>(60,441)</b>	<b>2,324</b>
1003	Cultural Precinct	5,629	50,376	210,248	(88.83%)	(44,747)	-
The variance is due to a timing delay with programming for Saltwater. The grant income has been received but not yet allocated to the project as the expenses have not yet been incurred.							
1005	Library Services	73,867	84,394	91,619	(12.47%)	(10,527)	150
This budget relates to the contributions for the SW Library Consortia and eContent selector roles, which are contributed to by other LG's and the State Library. The variance relates to the timing of invoicing. All consortia invoices have now been issued, and the eSelector contributions will be invoiced every 2 months in arrears. It is expected that we will be over budget by approximately \$10K by the end of the FY.							
<b>Community Planning</b>		<b>440,680</b>	<b>192,622</b>	<b>1,455,631</b>	<b>128.78%</b>	<b>248,057</b>	<b>(46,735)</b>
1010	Community and Recreation Development	2,500	31,042	106,500	(91.95%)	(28,542)	(12,208)
The Community Capacity Building Program grant funding from Lotterywest and the Every Club grant funding from DLGSC are yet to be received.							
1013	Youth Development	85,718	-	85,620	100.00%	85,718	109
Funding was received for activities that have already been completed, so was recognised in revenue immediately. The budget however was allocated to the end of financial year, when the spend involved in most grant funded activities is fully reconciled and any associated grant funding recognised.							
1049	Strategic Planning	75,000	-	142,000	100.00%	75,000	-
Funding was received for activities that have already been completed, so was recognised in revenue immediately. The budget however was allocated to the end of financial year, when the spend involved in most grant funded activities is fully reconciled and any associated grant funding recognised.							
1051	Environmental Health Services	72,563	54,475	83,682	33.21%	18,088	(41,238)
The majority of the mosquito management program funding from Department of Health was received earlier than originally budgeted. A negative variance of approximately \$10K is expected by year end.							
1056	Emergency Services	200,084	99,040	1,022,972	102.02%	101,044	-
Variance is simply timing of the bushfire mitigation fund grant received versus budget spread.							
<b>Infrastructure and Environment</b>		<b>859,200</b>	<b>102,028</b>	<b>2,815,416</b>	<b>742.13%</b>	<b>757,173</b>	<b>720,264</b>
1014	Infrastructure and Environment Directorate	666,698	-	858,776	100.00%	666,698	666,661
Actuals represent the half yearly Jetty contribution from BJINC, which was received earlier than expected.							
1015	Civil Infrastructure Management	114,385	20,916	71,605	446.89%	93,469	46,465
Positive variance relates to the receipt of workers compensation insurance reimbursements, which can never be predicted with any accuracy.							
1017	Parks & Gardens	36,107	22,195	48,169	62.68%	13,912	3,871
\$8.5K of the variance relates to unbudgeted RAC EV charging station reimbursements received, with the remainder relating to timing variance between when grants received from DPIRD for completed environmental activities were received as opposed to when they were budgeted to be recognised as revenue.							
1025	Fleet Services	37,711	49,492	122,592	(23.80%)	(11,782)	(2,087)
The negative variance is due to the month lag for the BAS refund of the diesel fuel rebate.							
<b>2</b>	<b>Other Revenue</b>	<b>214,319</b>	<b>84,947</b>	<b>334,932</b>	<b>152.30%</b>	<b>129,372</b>	<b>2,952</b>
<b>Corporate Strategy and Performance</b>		<b>62,441</b>	<b>17,618</b>	<b>29,869</b>	<b>254.42%</b>	<b>44,823</b>	<b>9,146</b>
1033	Finance	21,758	208	500	10343.02%	21,550	(12,974)
The positive over budget variance is due to the receipt of salary packaging amounts from the former third party staff salary packaging provider, but the new provider has not as yet reconciled all of the various accounts and requested the funds to be transferred to them.							
1034	Rates	22,137	7,622	7,622	190.44%	14,515	19,536
The ESL Levy administration fee (done by the Rates team), came in a lot higher than forecast.							
<b>Community Planning</b>		<b>16,450</b>	<b>35,820</b>	<b>87,796</b>	<b>(54.08%)</b>	<b>(19,370)</b>	<b>(3,614)</b>
1054	Ranger Services	12,290	32,099	75,676	(61.71%)	(19,809)	(3,884)
Animal registration renewals have gone out for those that expire on 31 October 2023. As these are renewed our income in this area will increase. Lower than expected parking infringement revenues YTD.							
<b>Infrastructure and Environment</b>		<b>128,816</b>	<b>28,663</b>	<b>210,898</b>	<b>349.41%</b>	<b>100,153</b>	<b>(4,200)</b>

CITY OF BUSSELTON  
NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY  
FOR THE PERIOD ENDED 30th NOVEMBER 2023

**Note 3 Material Variance Explanations**

Cost Centre or Project	Description	Actual YTD \$	Budget YTD \$	Full Year Budget \$	Variance YTD %	Variance YTD \$	Change in Variance Current Month \$
1026	Waste Services	116,620	27,183	184,847	329.01%	89,436	(4,200)
Over the YTD budget amount due higher volumes of sales of scrap metal at higher than expected prevailing prices.							
3	Interest Earnings	2,764,469	2,209,803	4,305,492	25.10%	554,666	177,533
Better than YTD budget due to higher than expected interest rates and cash on hand position earlier in the year, with the roll over of invested funds coming to fruition overlapping into the next financial year.							
<b>Expenses from Operating Activities</b>		<b>(38,322,776)</b>	<b>(42,441,146)</b>	<b>(103,200,789)</b>	<b>9.70%</b>	<b>4,118,370</b>	<b>991,328</b>
The expense line items on the face of the financial statement that have a YTD variance that meet the material reporting threshold are outlined below:							
4	Materials & Contracts	(8,172,444)	(11,984,525)	(29,072,925)	31.81%	3,812,081	142,126
Office of the CEO							
1000	Office of the CEO	6,318	1,760	5,224	(259.02%)	(4,558)	(1,263)
Organisational grocery (eg milk) purchases have been coded against the CEO other administration / office expenses line item without corresponding budget.							
1031	Stakeholder Relations	9,973	1,359	3,261	(633.93%)	(8,614)	(9,702)
The variance relates to subscription costs that have not been budgeted for against 1031 associated with the website and mailchimp. These have been budgeted organisationally and so the funds are covered.							
<b>Corporate Strategy and Performance</b>		<b>801,539</b>	<b>995,167</b>	<b>2,303,008</b>	<b>19.46%</b>	<b>193,628</b>	<b>193,480</b>
1028	People and Performance Management	45,636	547	1,312	(8248.28%)	(45,089)	871
Governance related WALGA subscriptions have been allocated here in error. Will be re-allocated to prepaid expenses account in December and expensed to Governance on a monthly basis over the term of the subscriptions.							
1029	Governance Services	45,765	159,017	286,090	71.22%	113,252	94,411
Underspend is primarily due to consultancy costs budgeted for and not expended in the period, and subscriptions costs not incurred against the budget YTD (timing variance). Utilisation of this forecast is expected in subsequent periods. Additionally some computer software license and publication costs have been misallocated to an incorrect cost centre budget.							
1030	Human Resources and WHS	25,478	47,753	114,608	46.65%	22,275	9,551
Underspend is due to expected consultancy spend being behind schedule on the Mercer Remuneration Project.							
1033	Finance	66,917	39,825	95,729	(68.03%)	(27,091)	(1,019)
The overspend is primarily due to a timing issue with how bank charges have been shaped through the year. The budget has been evenly spread, however the bulk of the bank charges arise when people pay their rates in September. There has also been higher than expected consultancy costs due to the resetting of the Long Term Financial Plan model.							
1034	Rates	104,059	92,075	220,979	(13.02%)	(11,984)	4,486
Printing and postage costs for the City's annual rates plus the first instalment reminder notices were higher than expected. This will be somewhat offset by year end due to savings against the photocopying budget.							
1036	Information Technology	324,648	555,046	1,332,305	41.51%	230,399	87,511
The budget has been equally proportioned across the financial year, however, the significant IT projects that are expected to require consultancy have had a delayed start. Also, there has been delays in receiving and/or processing various software licensing renewals.							
1040	Legal Services	147,652	63,550	152,520	(132.34%)	(84,102)	(1,067)
Legal expenditure is exceeding the YTD budget because the City has had to engage external legal advisors for a number of contractual disputes, State Administrative Tribunal matters and prosecutions.							
<b>Economic and Business Development</b>		<b>713,756</b>	<b>1,046,596</b>	<b>2,592,692</b>	<b>31.80%</b>	<b>332,840</b>	<b>5,562</b>
1003	Cultural Precinct	21,186	79,948	287,154	73.50%	58,762	2,680
The variance is due to a timing delay with programming expenses for Saltwater. We've pushed back the expenses related to the grant for the opening production.							
1004	Events	19,318	8,911	21,387	(116.78%)	(10,407)	(17,536)
The expenditure was for the annual subscription to the SmartyGrants grant management system. No further budget will be required this financial year.							
1005	Library Services	47,874	65,208	144,683	26.58%	17,334	898
The underspend in the libraries budget YTD is due to delays in the finalisation of procurement waivers. Orders are now in place with all suppliers and we are waiting on delivery of goods and the associated processing of the invoices.							
1007	Airport Operations	326,954	508,715	1,186,936	35.73%	181,760	10,830
Some activities falling under Airport Screening services, Security costs, Airport Maintenance, Inspections, Consultancy, Contractors and purchase of materials have started and commitments have been raised, other works have either not commenced and hence PO's not raised. Account codes such as Airport Maintenance, Consultancy, Contractors and purchase of materials will be dependent on staff and contractor availability and hence variations occurring as a result of timing issues.							
1008	Economic Development	252,030	295,018	735,262	14.57%	42,987	3,201
Variance is due to a month lag in the invoicing for the contracted management services of the Jetty Tourist Park being processed and paid.							
1042	Aged and Community Housing	24,934	59,056	141,735	57.78%	34,123	3,815
The variance is due to the timing of invoices being issued as there are a number of commitments recorded against the operating expenses for both the Harris Road and Winderlup Villa units. There has also been a delay in obtaining quotes from contractors for certain works.							
<b>Community Planning</b>		<b>755,878</b>	<b>1,022,636</b>	<b>2,515,077</b>	<b>26.09%</b>	<b>266,757</b>	<b>(241,817)</b>
1010	Community and Recreation Development	69,530	189,507	390,089	63.31%	119,978	(9,959)
The YTD underspend variance is due to lower consultancy fees because not all club development workshops have been completed. Also, the Surf Lifesaving fees and GLC consultant costs are yet to be paid out.							

**CITY OF BUSSELTON  
NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY  
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**Note 3 Material Variance Explanations**

Cost Centre or Project	Description	Actual YTD \$	Budget YTD \$	Full Year Budget \$	Variance YTD %	Variance YTD \$	Change in Variance Current Month \$
1011	Leisure Centres	218,680	200,727	477,767	(8.94%)	(17,953)	5,874
Actuals have exceeded YTD forecast due to unplanned maintenance work required to fix the indoor pool liner and invoicing of the Geothermal license fee occurring earlier than expected.							
1013	Youth Development	16,519	34,292	81,500	51.83%	17,773	(120)
Planned large mental health week and Halloween activities/events did not occur due to staffing issues. Investigations are underway regarding other events to be run during Feb/ March 2024 to meet contractual funding agreement with Rio.							
1048	Statutory Planning	5,724	23,486	57,023	75.63%	17,762	3,856
This relates mainly to consultancy spend, which in this area mainly relates to design review or peer review of technical documents (e.g. traffic impact assessments) in support of applications. The timing of that spend largely relates to the lodgement and assessment of applications, and is not determined by the City.							
1049	Strategic Planning	10,289	103,808	430,540	90.09%	93,519	29,507
A large proportion of total spend in this area (approx.. 280K) is for coastal planning project in the Siesta Park/Marybrook area, and the timing of that spend will be determined by a RFT and subsequent contractual process, but none of that spend has yet been incurred. The variance in this area is entirely attributable to that.							
1051	Environmental Health Services	60,950	49,413	118,591	(23.35%)	(11,537)	8,733
Mosquito management product was purchased earlier than originally budgeted as a bulk purchase in preparation for commencement of the mosquito season. Analytical services were higher than budgeted this quarter as the annual one-off payment for the WA state analytical sampling program was paid in July.							
1054	Ranger Services	43,723	95,384	177,121	54.16%	51,661	29,482
Variance due to spread of budget versus timing of projects.							
<b>Infrastructure and Environment</b>		<b>5,884,709</b>	<b>8,917,007</b>	<b>21,653,663</b>	<b>34.01%</b>	<b>3,032,298</b>	<b>195,866</b>
1014	Infrastructure and Environment Directorate	330	186,105	446,651	99.82%	185,775	37,221
This is a timing variance only as works have not commenced and or been invoiced.							
1016	Construction and Maintenance	794,853	935,655	2,365,801	15.05%	140,802	(78,929)
The main variances relate to budget timing and include: <ul style="list-style-type: none"> <li>• \$108,398 Bridge maintenance – works scheduled for second half of the financial year</li> <li>• \$34,535 Rural Verge Maintenance</li> <li>• \$31,487 Street &amp; Drain Cleaning</li> <li>• \$19,171 Roadside spraying rural areas</li> <li>• (\$56,676) Rural-Tree Pruning</li> </ul> Expenditure is on track in relation to the full year budget.							
1017	Parks & Gardens	1,153,009	1,716,414	4,117,442	32.82%	563,405	(44,422)
Activities to date have been largely focused on general Open Space maintenance activities. This is consistent with lower YTD costs at this time of year compared to YTD budget which is evenly spread over the year. Spring and Summer maintenance/ renovation activities have seen a large increase in commitments and completed works which will see an increase in expenditure by the end of the second quarter. Annual Winter materials and plant ordered will be committed with a large amount of expenditure expected in the 3rd and 4th quarter when they are required.							
1019	Facilities Services	1,364,640	1,697,008	4,107,318	19.59%	332,368	(45,493)
This is a timing issue only, expenditure will increase as the tourist season approaches at that point the variance is expected to reduce. It is expected that all funds will be expended this FY.							
1020	Engineering and Facilities Services Management	181,091	1,743,000	4,165,954	89.61%	1,561,909	257,062
The main variances can be attributed to the following: (1) Coastal Works, that's currently in its planning phase, with Coastal Modelling being undertaken, that will guide the extent of the Coastal Works to be undertaken. The majority of this funding is associated with the Federal Coastal Grant that the City secured, that's currently in the planning phase. (2) Street lighting replacement and new installations programs, that's also currently in the planning phase. (3) Safer Speed Trial JV Initiative – grant that is being rolled out by the RAC, this project is currently in progress. (4) General Design and Traffic Modelling, this project is currently underway in terms of finalising the project scope to procure for an overall traffic model for Busselton and Dunsborough. Due to the nature of the four project groups above, all require upfront planning and design before the bulk expenditure will be recorded. Therefore the variance can be attributed to timing.							
1021	Asset Planning	2,905	33,340	80,015	91.29%	30,434	6,668
The underspend for this area can be attributed to the timing of the procurement of contractors to assist with data collection. Subscriptions and memberships are also underspend significantly, mainly also due to timing.							
1022	Design and Survey	1,185	15,982	35,357	92.59%	14,797	1,599
The underspend for this area can be attributed to the timing of the procurement of contractors to undertake the feasibility study for the Dunsborough to Yallingup Footpath, as well as the procurement of contractors for general design services.							
1023	Development Control	1,258	31,152	98,764	95.96%	29,894	1,995
The variance in this area can be attributed to the timing for the procurement of contractor services to undertake drainage data collection. This is currently in the planning phase, as the timing of the drainage data collection is normally undertaken during the dryer summer months. The variance can be attributed to timing.							
1025	Fleet Services	1,096,892	961,306	2,307,134	(14.10%)	(135,586)	(53,594)

CITY OF BUSSELTON  
NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY  
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**Note 3 Material Variance Explanations**

Cost Centre or Project	Description	Actual YTD \$	Budget YTD \$	Full Year Budget \$	Variance YTD %	Variance YTD \$	Change in Variance Current Month \$
Actuals over YTD budget associated to an increase in Fuels, Oils and Lubricants overall. Also the amount spent and number of replacement parts have increased as well.							
<b>5</b>	<b>Other Expenditure</b>	<b>(948,636)</b>	<b>(2,105,827)</b>	<b>(6,823,844)</b>	<b>54.95%</b>	<b>1,157,191</b>	<b>(294,340)</b>
<b>Office of the CEO</b>							
		64,045	92,116	251,126	30.47%	28,071	4,561
1031	Stakeholder Relations	18,897	40,625	103,902	53.48%	21,727	(163)
Half of the underspend (\$11,000) relates to a timing variance with respect to the City's community scorecard survey; this will take place early in the new year. Additionally there has been lower than anticipated spend year to date against the marketing, promotions and public relations accounts. The budget is expected to be fully expended however.							
<b>Corporate Strategy and Performance</b>							
		221,309	334,046	1,483,915	33.75%	112,737	27,881
1028	People and Performance Management	19,511	41,667	100,000	53.17%	22,155	4,847
The budget relates to the You Choose program which in future years will be reallocated to stakeholder relations. These funds will not be expended until the end of the financial year. The year to date actuals relate to misallocations against this cost centre from the members of council budget that should be allocated against 1029.							
1029	Governance Services	168,418	268,067	898,091	37.17%	99,649	28,139
The underspend on year to date budget is primarily due to \$60,000 budgeted for election expenses and not expended at the end of the period (utilisation of forecast is expected in subsequent period/s). There are likewise some scheduling variances between monthly forecast and utilisation of budget for miscellaneous other expenses including allowances and sitting fees, elected member conference and training, and refreshment and functions.							
<b>Economic and Business Development</b>							
		402,471	1,408,474	4,410,944	71.43%	1,006,002	(333,944)
1003	Cultural Precinct	1,188	35,564	108,649	96.66%	34,376	7,001
This is due to delayed timing of marketing and promotions for Saltwater, based on new opening dates for the venue.							
1004	Events	548,023	575,721	1,394,788	4.81%	27,698	(104,112)
The underspend variance is due to the timing of invoicing. Due to the establishment of milestone payments relating to post event reporting, this has created a lag in release of final payments to event sponsorships.							
1007	Airport Operations	(272,742)	686,396	2,663,611	139.74%	959,138	(193,878)
The April – June statements to finalise the 2022/23 FY incentive program have been received however are incorrect and Jetstar have been asked to review and provide corrected statements. Once this has been done then the final reconciliation for the 2022/23 accruals can be completed. Jetstar have been asked for the YTD 2023/24 statements, however they have not been sent.							
<b>Community Planning</b>							
		202,667	227,033	551,650	10.73%	24,366	6,818
1010	Community and Recreation Development	77,511	141,775	370,056	45.33%	64,263	23,358
The underspend variance is attributed to the budget being evenly distributed across the FY, however the actuals for the Community Assistance Program acquittals are sporadic and difficult to predict.							
1046	Building Services	19,272	-	-	100.00%	(19,272)	-
The unbudgeted spend YTD represents a contribution of Long Service Leave entitlements to another Local Government Authority, for a previous employee. This is funded from the LSL Reserve.							
1051	Environmental Health Services	40,766	19,324	28,757	(110.96%)	(21,442)	1,535
Mosquito management product was purchased earlier than originally budgeted as a bulk purchase in preparation for commencement of the mosquito season. Analytical services were higher than budgeted this quarter as the annual one-off payment for the WA state analytical sampling program was paid in July.							
<b>Infrastructure and Environment</b>							
		57,248	44,159	126,210	(29.64%)	(13,090)	1,239
1017	Parks & Gardens	4,107	-	22,515	100.00%	(4,107)	1,471
This relates to a late Biodiversity Incentive rate rebate that the City pays from last financial year.							
1019	Facilities Services	21,247	12,347	21,653	(72.08%)	(8,900)	329
Outgoings relate to the annual Emergency Services Levy charges from DFES on all of the City properties. Costs are as expected per the full year budget.							
<b>Amounts Attributable to Investing Activities</b>							
		(7,928,575)	(23,985,837)	(38,778,225)	66.94%	16,057,262	3,146,934
Items included in Investing Activities are Capital Grants, Subsidies & Contributions, Capital Expenditure in the various classes of assets, and Proceeds from Sale of Assets. The attachments to this report includes the detailed listings of all capital expenditure (project) items. The main areas of YTD variance to budget for Investing Activities that appear on the face of the Financial Activity Statement are summarised as follows:							
<b>6</b>	<b>Capital Grants, Subsidies and Contributions</b>	<b>1,196,834</b>	<b>26,430</b>	<b>19,724,075</b>	<b>4428.32%</b>	<b>1,170,404</b>	<b>1,001,979</b>
The positive variance of \$1.2M is due to a reimbursement of the municipal fund required for cashflow purposes, a lot earlier than was initially budgeted. The majority of externally funded capital projects are reconciled toward year end, with the associated funding recognised as revenue at that time. As such, the majority of the full year budget for this items has been allocated to the June period.							
<b>7</b>	<b>Land &amp; Buildings</b>	<b>(6,135,227)</b>	<b>(14,679,375)</b>	<b>(29,976,325)</b>	<b>58.21%</b>	<b>8,544,148</b>	<b>266,206</b>
12847 & 15073	Strategic Land Purchases	454,545	3,900,000	3,900,000	88.34%	3,445,455	(454,545)
Strategic land purchases are currently under negotiation and the expenditure is subject to agreements being reached.							
Various	BPACC (Saltwater)	5,323,902	9,508,038	20,707,633	44.01%	4,184,135	504,816
This variance is due to smaller than anticipated progress claims submitted by the contractor. This is due to delays experienced on site due to materials and contractor availability.							
10025	Weld Theatre (C)	50,146	-	250,000	-	(50,146)	10,000

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**Note 3 Material Variance Explanations**

Cost Centre or Project	Description	Actual YTD \$	Budget YTD \$	Full Year Budget \$	Variance YTD %	Variance YTD \$	Change in Variance Current Month \$
This is timing variances only. Purchases associated with the project have commenced showing a negative balance year to date but under the total budget.							
10585	BMRA Hangars(C)	-	202,527	303,790	100.00%	202,527	101,263
This project is to construct 2 hangars in the GA precinct. This project has not been started yet due to competing priorities and unlikely to go out to RFQ until the end of March 2024.							
10589	Airport Construction, Existing Terminal Upgrade(C)	33,443	103,847	103,847	67.80%	70,404	-
We have completed the remaining terminal improvements which included the installation of the training/office transportable and ablution block (which came in under budget). With the exception of approximately \$15k (one invoice raised against the Airport Ops budget and one invoice still to be paid) the remaining funds are unallocated.							
10642	Dunsborough Lakes Sporting Precinct- Pavilion/Changeroom F(C)	160,892	-	1,620,434	-	(160,892)	(20,343)
The new pavilion project is currently on hold and subject to further design and consultation with no specific timeline developed for implementation until this is completed. Expenditure to date is for the provision of temporary facilities for the area to be used in the meantime.							
10787	Geographe Leisure Centre Upgrade(C)	2,120	236,443	567,464	99.10%	234,324	47,289
The YTD underspend is due to the procurement process taking longer than anticipated due to contractor availability.							
12331	Smiths Beach New Public Toilet(C)	-	104,167	250,000	100.00%	104,167	20,833
This variance is due to timing of works – planning and procurement has taken longer than anticipated.							
12428	Re-use shop Busselton Transfer Station(C)	-	158,167	275,000	100.00%	158,167	2,316
Reuse Shop is currently still in the development phase, which has taken longer than expected.							
12440	Energy Efficiency Initiatives (Various Buildings) (C)	-	32,569	78,166	100.00%	32,569	6,514
Project is delayed due to Western Power delays.							
12796	Depot Training Room (C)	6,979	31,250	75,000	77.67%	24,271	(729)
This variance is due to timing of works – planning and procurement has taken longer than anticipated.							
12800	Old Courthouse and Jail Cells (C)	1,500	42,855	102,853	96.50%	41,355	8,571
This variance is due to timing of works – planning and procurement has taken longer than anticipated.							
12804	Airport Terminal Building(C)	-	11,867	28,481	100.00%	11,867	2,373
This is a timing issue – works will commence in the new year and be complete by March.							
12939	Administration Building- 2-16 Southern Drive(C)	-	18,750	45,000	100.00%	18,750	3,750
This is a timing variance only, works will be completed appropriately by the end of the financial year.							
12940	Depot Building-Busselton(C)	3,069	112,500	270,000	97.27%	109,431	22,031
This is a timing issue due to the budget being evenly spread across the year. The actual timing of the works is scheduled for December/January, depending on contractor availability.							
14942	Lions Park Shopping Centre Toilets (C)	-	104,167	250,000	100.00%	104,167	20,833
City officers have been given direction from council to re-negotiate terms with the Strata company. Therefore works have not been able to progress as planned.							
14943	Changing Places Accessible Ablutions (C)	-	41,667	100,000	100.00%	41,667	8,333
This is a timing issue. Public consultation has been completed, with the block to be situated next to the existing toilet block at Signal Park. The RFT documents are ready to be uploaded, Legal are just carrying out their final review. Will be uploaded by end of December.							
15077	Lunch / Meeting Room for Transfer Station(C)	47,500	-	50,000	-	(47,500)	-
This facility was purchased earlier than originally forecast. The YTD variance is timing only.							
<b>8</b>	<b>Plant &amp; Equipment</b>	<b>318,751</b>	<b>736,305</b>	<b>6,078,421</b>	<b>56.71%</b>	<b>417,554</b>	<b>79,109</b>
Delay in procuring some of the new Plant and Fleet equipment as manufacturers pause taking any new orders due to the global semiconductor shortage from the COVID pandemic. Orders are expected to pick up in the new year.							
<b>9</b>	<b>Furniture &amp; Office Equipment</b>	<b>89,564</b>	<b>682,725</b>	<b>1,510,209</b>	<b>86.88%</b>	<b>593,161</b>	<b>98,660</b>
12781	Saltwater - Loose Furniture & Fittings(C)	-	142,844	228,551	100.00%	142,844	28,569
Delays to the Saltwater project timeline have meant that procurement of Furniture and Fittings has not yet commenced.							
12783	Saltwater - IT Hardware & Software (C)	15,641	29,688	71,250	47.32%	14,047	5,172
About 12k of the \$15k shown in the table above is for operational expenses to do with Saltwater and will be moved mid-December 2023. Once this has been moved, there will be a total of about \$71k in commitments. This will be made up of \$25k for project consulting costs for the Saltwater financial system and \$45k for the core networking hardware. We are also yet to start procurement for WiFi access points and UPSs, however this is unlikely to happen in this financial year.							
12876	ICT Services - Equipment & Software Purchases(C)	31,834	402,290	965,497	92.09%	370,457	80,458
The significant items that will comprise much of the allocated capital budget will be procured between November 2023 and June 2024 to coincide with the related implementation projects.							
12878	Naturaliste Community Centre (NCC) - Purchase Office Furn(C)	-	20,000	20,000	100.00%	20,000	-
Carryover of capital budget, most likely to be expensed by mid-year dependant on supply availability.							
14730	Geographe Leisure Centre Fitness Equipment(C)	-	18,160	18,160	100.00%	18,160	-
Carryover of capital budget, most likely be expensed by mid-year.							
15065	Differential GPS (C)	17,310	-	20,000	-	(17,310)	(17,310)

CITY OF BUSSELTON  
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**Note 3 Material Variance Explanations**

Cost Centre or Project	Description	Actual YTD \$	Budget YTD \$	Full Year Budget \$	Variance YTD %	Variance YTD \$	Change in Variance Current Month \$
The GPS has been ordered earlier than originally anticipated, to expedite the collection of infrastructure asset data.							
15088	Busseton Depot Building Furniture & Equipment (C)	(2,600)	10,813	10,813	124.05%	13,413	-
This is a timing issue as the purchase of furniture is linked to the refurbishment of the training room. Once completed the furniture will be purchased using these funds.							
<b>10</b>	<b>Infrastructure</b>	<b>2,839,924</b>	<b>8,117,734</b>	<b>21,896,529</b>	<b>65.02%</b>	<b>5,277,811</b>	<b>1,727,049</b>
Various	Roads	2,035,201	4,532,556	10,093,354	55.10%	2,497,355	774,850
The budget spread for the majority of projects in this item is even across the financial year and based on the construction schedule there are some budget timing variances within this item. Projects on Cook St, Kershaw St, Artisan St, Yallingup Beach Rd, Downs Road, Wilyabrup Road, Payne Road, Bussell Highway resurfacing, Silverwood Road have been completed. Road projects commenced in November include: • Jalbarragup Road New Shoulders • Boallia Road Reconstruction							
Various	Car Parks	110,685	325,604	541,885	66.01%	214,919	(43,567)
The budget timing reflects an even spread of expenditure across the financial year, with the exception of Carpark Hotel Site 2, which has the budget spread over July and August. As a result there are some budget timing variances within this item, with Carpark Hotel Site 2 comprising \$90k of the variance and Forrest Beach Horse Float Carparking Area \$81k. Carpark Hotel Site 2 civil works are complete and landscaping largely completed.							
Various	Footpaths & Cycleways	174,430	849,175	1,824,587	79.46%	674,745	151,294
Based on the construction schedule there are some budget timing variances within this item, these include the following projects: • \$358k – Forth St to Holgate and Vincent Street to Geographe Bay Road shared path projects which are planned for construction March – June • \$67k – Townscape Works Busseton • \$61k – Peron Avenue on which landowners are currently being consulted– planned for April - May • \$52k – Bayview Street footpath construction - delayed to follow Busseton Water asset replacement • \$47k – Carey Street footpath construction planned following drainage upgrades in February/ March							
Various	Parks, Gardens & Reserves	454,764	1,661,665	7,043,950	72.63%	1,206,901	443,475
Budget timing for most projects within this item reflect an even spread of expenditure across the financial year. A large number of projects within this item are currently in design and procurement stage. Projects totalling \$2,576,000.00 have seen minimal commitment. • \$880k - Dunsborough Lakes Sporting Precinct (Stage 1) TBA • \$550k - Churchill Park - Main Oval Renewal – scheduled February to April • \$265k - New Shade Sails – scheduled February to April • \$356k - Meelup Coastal Parking & Landscaping – carpark, drainage and footpath completed in November, with footpath artwork March. • Rotary Park \$525k January to April.							
Various	Drainage	64,844	409,173	1,104,476	84.15%	344,330	62,785
Budget timing does not reflect construction schedule for projects within this item, which sees projects delivered largely in the 3rd and 4th quarters.							
Various	Regional Airport and Industrial Park	-	339,561	1,288,278	100.00%	339,561	337,895
This category is made up of various projects including the Airfield construction, RFDS, Apron lighting, Apron CCTV, GA reseal of which a number have now been awarded and works will commence in January and February. Most of these projects are weather dependent (cannot be commenced until after October) and contractor availability.							
<b>11</b>	<b>Proceeds from Sale of Assets</b>	<b>204,487</b>	<b>150,300</b>	<b>863,800</b>	<b>36.05%</b>	<b>54,187</b>	<b>(15,000)</b>
Amount over budget YTD due to the delayed sale of assets from last financial year, resulting in the proceeds from the auction were received in July, instead of June.							
<b>Amounts Attributable to Financing Activities</b>		<b>(7,506,465)</b>	<b>(1,009,587)</b>	<b>30,861,065</b>	<b>643.52%</b>	<b>(6,496,878)</b>	<b>(1,639,451)</b>
<b>12</b>	<b>Proceeds from New Loans</b>	<b>-</b>	<b>1,750,000</b>	<b>2,000,000</b>	<b>(100.00%)</b>	<b>(1,750,000)</b>	<b>-</b>
Planning and negotiations for the strategic purchases of land have been delayed, therefore the requirements to apply for and draw down on loan facilities has not been required to date.							
<b>13</b>	<b>Transfer to Restricted Assets</b>	<b>(14,777,232)</b>	<b>(8,669,978)</b>	<b>(9,200,455)</b>	<b>(70.44%)</b>	<b>(6,107,254)</b>	<b>(944,781)</b>
There is a YTD variance in transfers to Restricted Assets because there is no budget at all for this item. The transfers are not possible to predict, and are fully reconciled only at year end. At the time of budgeting it is not possible to predict what grants, contributions or developer bonds will be received, and in what timeframe, nor when they will be spent and hence potentially transferred to Restricted Assets (or unspent portions thereof). Transfers to restricted assets are offset by the incoming receipt, be it a grant, contribution, subsidy or even loan proceeds, so the net impact on the Net Current Position is always nil (once full reconciliation has occurred at year end). It should be noted that per Council budget amendment resolution, \$9.3M has been moved from reserves to restricted funds in relation to historical developer contributions for community facilities that have not yet been utilised (not recognised in revenue), so could not be included in reserves. The remaining \$5.5M YTD transfers relates to \$3.3M in various government grants that were restricted, as well as \$1M in Roadwork Bonds, \$450K in BJTP deposits, \$33K in other developer contributions and \$717K in various other deposits and bonds.							
<b>14</b>	<b>Transfer from Restricted Assets</b>	<b>6,681,590</b>	<b>8,567,950</b>	<b>22,096,174</b>	<b>(22.02%)</b>	<b>(1,886,360)</b>	<b>58,664</b>

**CITY OF BUSSELTON**  
**NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY**  
**FOR THE PERIOD ENDED 30th NOVEMBER 2023**

**Note 3      Material Variance Explanations**

Cost Centre or Project	Description	Actual YTD \$	Budget YTD \$	Full Year Budget \$	Variance YTD %	Variance YTD \$	Change in Variance Current Month \$
<p>The transfers are usually not possible to predict, and are fully reconciled only at year end. The transfers YTD are attributable to \$5.2M of unspent loan funds utilised, \$199K of prepaid grants utilised, \$164K of Developer Contributions utilised, \$98K of BJTP deposits utilised or returned, \$398K of Roadwork Bonds returned and \$641K of various other bonds and deposits returned or utilised.</p>							
<b>15</b>	<b>Transfer from Reserves</b>	<b>27,113,867</b>	<b>22,640,277</b>	<b>60,071,435</b>	<b>19.76%</b>	<b>4,473,589</b>	<b>515,537</b>
<p>Works on a number of large reserve funded projects have been substantively completed, so for unrestricted cashflow purposes some of the reserve transfers were partially brought forward from the end of year, when all reserve funded project reconciliations are usually done.</p>							

<b>City of Busselton Capital Acquisitions &amp; Construction Report For the period ended 30th November 2023</b>						
Project	Project Description	2023/24 Actual YTD	2023/24 Amended Budget YTD	2023/24 Original Budget YTD	2023/24 Amended Budget	2023/24 Budget Original
		\$	\$	\$	\$	\$
<b>Land</b>						
12847	Strategic Land Purchase - Sues Road(C)	454,545	500,000	500,000	500,000	500,000
15073	Strategic Land Purchase - Dunsborough (C)	-	3,400,000	3,400,000	3,400,000	3,400,000
	<b>Total Land</b>	<b>454,545</b>	<b>3,900,000</b>	<b>3,900,000</b>	<b>3,900,000</b>	<b>3,900,000</b>
<b>Buildings</b>						
<u>Aged Housing</u>						
12231	Aged Housing Capital Improvements - Harris Road(C)	28,515	7,128	7,128	22,000	22,000
12233	Aged Housing Capital Improvements - Winderlup(C)	-	24,290	24,290	40,000	40,000
12234	Aged Housing Capital Improvements - Winderlup Court (City)(C)	15,580	18,511	18,511	49,000	49,000
		44,095	49,928	49,928	111,000	111,000
<u>Saltwater</u>						
12471	BPACC - Construction(C)	4,011,696	7,600,257	7,600,257	18,240,616	18,240,616
12652	BPACC - Design of Structure / Professional Fees(C)	399,016	88,621	88,621	212,690	212,690
12779	BPACC - Contingency (C)	871,511	1,759,714	1,759,714	2,111,657	2,111,657
12782	BPACC - Utility Costs (C)	41,680	59,446	59,446	142,670	142,670
		5,323,902	9,508,038	9,508,038	20,707,633	20,707,633
10016	Civic and Administration Centre Minor Upgrades(C)	-	-	-	35,000	35,000
10025	Weld Theatre(C)	50,146	-	-	250,000	250,000
10585	BMRA Hangars(C)	-	202,527	202,527	303,790	303,790
10589	Airport Construction, Existing Terminal Upgrade(C)	33,443	103,847	103,847	103,847	103,847
10592	Airport Terminal Stage 2(C)	81	-	-	-	-
10642	Dunsborough Lakes Sporting Precinct-Pavilion/Changeroom F(C)	160,892	-	-	1,620,434	1,620,434
10787	Geographe Leisure Centre Upgrade(C)	2,120	236,443	236,443	567,464	567,464
12331	Smiths Beach New Public Toilet(C)	-	104,167	104,167	250,000	250,000
12428	Re-use shop Busselton Transfer Station(C)	-	158,167	158,167	275,000	275,000
12440	Energy Efficiency Initiatives (Various Buildings) (C)	-	32,569	32,569	78,166	78,166
12795	Busselton Depot CCTV & Electronic Gate(C)	6,952	16,259	16,259	16,259	16,259
12796	Depot Training Room (C)	6,979	31,250	31,250	75,000	75,000
12798	High Street Hall (C)	-	4,375	4,375	10,500	10,500
12800	Old Courthouse and Jail Cells (C)	1,500	42,855	42,855	102,853	102,853
12804	Airport Terminal Building(C)	-	11,867	11,867	28,481	28,481
12868	Busselton Jetty Tourist Park - Cabin Replacement/Upgrade (C)	-	-	-	540,000	540,000
12939	Administration Building- 2-16 Southern Drive(C)	-	18,750	18,750	45,000	45,000
12940	Depot Building-Busselton((C)	3,069	112,500	112,500	270,000	270,000
14942	Lions Park Shopping Centre Toilets (C)	-	104,167	104,167	250,000	250,000
14943	Changing Places Accessable Ablutions (C)	-	41,667	41,667	100,000	100,000
15067	Commonage Fire Facility (C)	-	-	-	40,000	40,000
15074	Vasse Toilet (C)	-	-	-	125,000	125,000
15077	Lunch / Meeting Room for Transfer Station(C )	47,500	-	-	50,000	50,000
15087	Mosquito Management Facility (C)	-	-	-	120,899	120,899
	<b>Total Buildings</b>	<b>5,680,681</b>	<b>10,779,375</b>	<b>10,779,375</b>	<b>26,076,325</b>	<b>26,076,325</b>
	<b>Total Land &amp; Buildings</b>	<b>6,135,227</b>	<b>14,679,375</b>	<b>14,679,375</b>	<b>29,976,325</b>	<b>29,976,325</b>
<b>Plant and Equipment</b>						
10700	Dunsborough Cemetery(C)	-	8,865	8,865	21,276	21,276
12870	Busselton Jetty Tourist Park- Replace Holden Colorado 4x4(C)	36,160	35,000	35,000	35,000	35,000
12872	Busselton Jetty Tourist Park - Battery Powered Mule (C)	-	35,000	35,000	35,000	35,000
12881	HINO FD1024 Beavertail Truck (Turf) (C)	-	-	-	100,000	100,000
12882	Isuzu NPR65-190 Single Cab (Turf) (C)	-	-	-	100,000	100,000
12884	KUBOTA F3690 OUTFRONT MOWER (C)	-	29,200	29,200	29,200	29,200
12885	KUBOTA F3690 OUTFRONT MOWER (C)	-	29,200	29,200	29,200	29,200
12886	Brush / Tree Chipper (C)	-	100,000	100,000	100,000	100,000
12887	Plant Purchases (P11) - Redexim Easyspread 1600 Sand Spre(C)	-	20,000	20,000	20,000	20,000
12888	HINO FS2848 Tipper Truck (C&M) (C)	-	-	-	230,000	230,000
12890	Caterpillar CS56 Roller (C&M) (C)	-	-	-	180,000	180,000
12892	ISUZU FVZ1400 Tipper Truck (C&M) (C)	-	-	-	230,000	230,000
12894	Nissan Xtrail ST-L 2WD (Strategic Planning Coord) (C)	-	-	-	35,000	35,000
12895	NISSAN X-TRAIL ST-L 2WD SUV (ASSET MGMT COORD) (C)	-	-	-	35,000	35,000
12896	Facilities Operational Ute(C)	35,280	-	-	42,000	42,000
12897	ISUZU D-MAX SX 4X2 SINGLE CAB UTILITY (IRRIGATION) (C)	-	-	-	42,000	42,000
12898	MITSUBISHI TRITON 4X2 SINGLE CAB UTE (P&G) (C)	-	42,000	42,000	42,000	42,000
12899	Construction & Maintenance Ute (C)	41,124	-	-	42,000	42,000
12900	TOYOTA HILUX DUAL CAB 4X4 (CONST SUPERVISOR) (C)	-	-	-	45,000	45,000
12901	TOYOTA HILUX DUAL CAB 4X4 (MTCE SUPERVISOR) (C)	-	45,000	45,000	45,000	45,000
12902	Manager Governance & Corp Services Vehicle (C)	39,134	-	-	40,000	40,000
12904	Plant Purchases (P11) - Minor Plant (P&G) (C)	-	-	-	15,000	15,000
12906	Plant Purchases (P12) - Minor Plant (C&M) (C)	-	-	-	20,000	20,000
12908	Program 12 - Minor Plant (Workshop) (C)	-	-	-	10,000	10,000

<b>City of Busseton Capital Acquisitions &amp; Construction Report For the period ended 30th November 2023</b>						
Project	Project Description	2023/24 Actual YTD	2023/24 Amended Budget YTD	2023/24 Original Budget YTD	2023/24 Amended Budget	2023/24 Budget Original
		\$	\$	\$	\$	\$
12911	VOLVO FE320 WASTE TRUCK (C)	-	-	-	459,660	459,660
12914	Minor Plant - Program 10 (C)	-	-	-	10,000	10,000
12932	Airport Operations - Replacement Toro Groundmaster 7200 M(C)	-	30,000	30,000	30,000	30,000
12948	TOYOTA HILUX SR 4X4 DUAL CAB UTILITY (BMO) (C)	-	-	-	45,000	45,000
14744	MITSUBISHI PAJERO GLX 4X4 (MGR REG SERV) (C)	-	41,500	41,500	41,500	41,500
14818	Wheeled 20t excavator Inc. Accessories(C)	-	-	-	485,000	485,000
14819	Positrak (Cat 299) with Accessories (C)	-	-	-	250,000	250,000
14820	Trailer for Towing Positrak & Traffic Lights(C)	-	-	-	40,000	40,000
14821	Trailer Traffic Lights x 2 (and Sign Cones)(C)	43,090	45,000	45,000	45,000	45,000
14823	VOLVO FE320 WASTE TRUCK (C)	-	-	-	473,545	473,545
14842	HINO CREW CAB TIPPER TRUCK (C)	102,664	100,540	100,540	100,540	100,540
14946	PERUZZO BULL SLASHER (C)	-	60,000	60,000	60,000	60,000
14949	KUBOTA MULE (C)	-	-	-	15,000	15,000
14951	1500L SELF BUNDED DIESEL FUEL TANK (C)	-	15,000	15,000	15,000	15,000
14954	MITSUBISHI TRITON 4x2 SINGLE CAB (C)	-	35,000	35,000	35,000	35,000
14960	VOLVO FE320 WASTE TRUCK (C)	-	-	-	480,000	480,000
14962	WATER TANK FOR DUST SUPPRESSION (C)	-	25,000	25,000	25,000	25,000
14966	DIESEL TANK DWF (C)	-	-	-	50,000	50,000
14968	HOOKLIFT BIN BWTS (C)	-	-	-	18,000	18,000
14970	HOOKLIFT BIN BWTS (C)	-	-	-	18,000	18,000
14972	HOOKLIFT BIN BWTS (C)	-	-	-	18,000	18,000
14974	HOOKLIFT BIN (C)	-	-	-	18,000	18,000
14976	HOOKLIFT BIN (C)	-	-	-	18,000	18,000
14978	ABLE GL3013 GENERATOR (C)	21,300	-	-	50,000	50,000
14980	WASTE COORDINATOR VEHICLE (C)	-	-	-	35,000	35,000
14982	TOYOTA HILUX 4x2 SINGLE CAB (C)	-	-	-	36,000	36,000
14984	ISUZU D-MAX 4x4 DUAL CAB (C)	-	-	-	40,000	40,000
14986	HOLDEN COLORADO 4x4 DUAL CAB (C)	-	-	-	36,000	36,000
15004	HUSQVARNA RC318T RIDE ON MOWER (C)	-	-	-	18,000	18,000
15006	HINO 917 CREW CAB (C&M) (C)	-	-	-	100,000	100,000
15008	ISUZU NPR400 CREW CAB (C&M) (C)	-	-	-	100,000	100,000
15013	HINO GH1728 AUTO TRUCK (C&M) (C)	-	-	-	400,000	400,000
15016	HINO GF1628 REAR TIPPING TRUCK (P&G) (C)	-	-	-	150,000	150,000
15018	TORO 4010D GROUNDMASTER MOWER (P&G) (C)	-	-	-	150,000	150,000
15023	ISUZU DMAX 4X2 SINGLE CAB (P&G) FORESHORE IRRIGATION (C)	-	-	-	42,000	42,000
15025	TOYOTA HILUX 4X4 EXTRA CAB UTILITY (IRRIGATION) (C)	-	-	-	45,000	45,000
15028	FORD RANGER 4X4 DUAL CAB UTE (CESM) (C)	-	-	-	55,000	55,000
15032	SUBARU OUTBACK 2.5i PREMIUM WAGON (C)	-	-	-	35,000	35,000
15034	SUBARU OUTBACK AWD WAGON (RANGERS CO-ORDINATOR) (C)	-	-	-	35,000	35,000
15036	SUBARU OUTBACK 2.5i AWD PREMIUM (MGR CULTURE & EVENTS) (C)	-	-	-	40,000	40,000
15038	MAZDA CX9 FWD SUV (Director CCS) (C)	-	-	-	50,000	50,000
15040	SUBARU OUTBACK AWD (Director PDS) (C)	-	-	-	50,000	50,000
15042	Nissan X-Trail Ti 4x4 SUV (Mgr Fin Serv) (C)	-	-	-	40,000	40,000
15045	Toyota Fortuner GX 4x4 SUV (Mgr Eng Tech Srv) (C)	-	-	-	40,000	40,000
15048	SUBARU FORESTER 2.5i-S AWD (MGR DEV SERV) (C)	-	-	-	40,000	40,000
15050	NISSAN X TRAIL STL 4X4 SUV (LIESURE CENTRES COORD) (C)	-	-	-	35,000	35,000
15052	MITSUBISHI OUTLANDER LS AWD (HR COORD) (C)	-	-	-	35,000	35,000
15078	HAZMET and Dangerous Goods Storage(C)	-	-	-	108,500	108,500
15083	New Airport Manager Vehicle (C)	-	40,000	40,000	40,000	40,000
<b>Total Plant &amp; Equipment</b>		<b>318,751</b>	<b>736,305</b>	<b>736,305</b>	<b>6,078,421</b>	<b>6,078,421</b>
<b>Furniture and Equipment</b>						
10965	P&E - P&G Smart Technologies(C)	19,817	31,250	31,250	75,000	75,000
12781	BPACC - Loose Furniture & Fittings (C)	-	142,844	142,844	228,551	228,551
12783	BPACC - IT Hardware & Software (C)	15,641	29,688	29,688	71,250	71,250
12876	ICT Services - Equipment & Software Purchases(C)	31,834	402,290	402,290	965,497	965,497
12878	Naturaliste Community Centre (NCC) - Purchase Office Furn(C)	-	20,000	20,000	20,000	20,000
14729	Events Furniture & Equipment(C)	-	5,138	5,138	5,138	5,138
14730	Geographe Leisure Centre Fitness Equipment(C)	-	18,160	18,160	18,160	18,160
14731	Cultural Planning Furniture & Equipment(C)	-	9,500	9,500	9,500	9,500
14927	Dunsborough Depot(C)	-	8,875	8,875	21,300	21,300
14939	Ice Machine (C)	-	4,167	4,167	10,000	10,000
15059	Sculpture By The Bay (C)	-	-	-	12,000	12,000
15063	NCC - Les Mills Pump Rack (C)	7,086	-	-	8,000	8,000
15064	GLC Cafe Double Fridge (C)	-	-	-	20,000	20,000
15065	Differential GPS (C)	17,310	-	-	20,000	20,000
15082	Administration Building Furniture& Equipment (C)	476	-	-	15,000	15,000
15088	Busseton Depot Building Furniture & Equipment (C)	2,600	10,813	10,813	10,813	10,813
<b>Total Furniture &amp; Equipment</b>		<b>89,564</b>	<b>682,725</b>	<b>682,725</b>	<b>1,510,209</b>	<b>1,510,209</b>
<b>Roads Infrastructure</b>						

<b>City of Busselton Capital Acquisitions &amp; Construction Report For the period ended 30th November 2023</b>						
Project	Project Description	2023/24 Actual YTD	2023/24 Amended Budget YTD	2023/24 Original Budget YTD	2023/24 Amended Budget	2023/24 Budget Original
		\$	\$	\$	\$	\$
11193	Wilyabrup Road Resheet(C)	65,980	20,161	20,161	80,643	80,643
11199	Bussell Highway(C)	803,763	847,797	847,797	2,034,712	2,034,712
11987	Caves Road - Median Crossing(C)	-	4,261	4,261	17,043	17,043
11996	Geographe Bay Road Quindalup(C)	6	-	-	-	-
11998	Kaloorup Road(C)	10,130	-	-	60,000	-
12005	Payne Road(C)	431,687	365,861	365,861	878,066	878,066
12609	Geographe Bay Road - Gifford Road to Elmore Road(C)	631	-	-	-	-
12611	North Jindong Road(C)	5,153	330,250	330,250	792,600	792,600
12645	Marine Terrace(C)	1,598	-	-	-	-
12739	Tuart Drive(C)	124	-	-	-	-
12824	Acton Park Road(C)	578	-	-	-	-
12827	Commonage Road(C)	54,753	320,625	320,625	769,500	769,500
12835	Ludlow-Hithergreen Road(C)	2,851	-	-	-	-
12837	Pre-emptive Design Works (C)	2,422	-	-	-	-
12841	Strelly Street (C)	6,144	-	-	-	-
12842	Nuttman Road (C)	-	13,681	13,681	54,724	54,724
12851	Bussell Highway(C)	104	-	-	-	-
14817	Road Access Seal - Busselton Animal care facility(C)	104	12,442	12,442	49,766	49,766
14850	Cook Street - Asphalt Overlay SLK 0.39 to 1.25 (C)	182,459	146,489	146,489	291,574	351,574
14851	Strelly Street - Rehabilitation SLK 0.05 to 1.2 (C)	21,936	300,000	300,000	720,000	720,000
14852	North Jindong Road - Prelims and Land Acq SLK 0 to 0 (C)	-	5,000	5,000	12,000	12,000
14853	Jalbarragup Road - Reseal SLK 0 to 10.75 (C)	727	250,000	250,000	600,000	600,000
14854	Boallia Road - Recon and Widening SLK 1.5 to 2.3 (C)	119,738	230,763	230,763	553,832	553,832
14855	Vasse Yallingup Siding Road - Design only SLK 0 to 0 (C)	-	6,875	6,875	16,500	16,500
14856	Peron Avenue - Asphalt Overlay SLK 0.17 to 0.76 (C)	28,592	187,513	187,513	450,030	450,030
14857	West Street - Rehabilitation SLK 1.02 to 1.15 (C)	1,163	-	75,417	-	181,000
14858	Artisan Street - Asphalt Overlay SLK 0.02 to 0.16 (C)	25,019	19,230	19,230	36,974	46,152
14859	Kershaw Street - Asphalt Overlay SLK 0 to 0.25 (C)	50,446	28,863	28,863	50,446	69,271
14860	Thornbill Court - Asphalt Overlay SLK 0 to 0.08 (C)	-	8,609	8,609	20,662	20,662
14861	Grevillea Cove - Asphalt Overlay SLK 0 to 0.08 (C)	-	9,268	9,268	22,243	22,243
14862	Baudin Street - Asphalt Overlay SLK 0 to 0.1 (C)	403	39,638	39,638	95,132	95,132
14863	Cormorant Court - Asphalt Overlay SLK 0 to 0.08 (C)	-	9,840	9,840	23,616	23,616
14864	Sugarloaf Road - Second Coat Seal SLK 0 to 0 (C)	-	27,717	27,717	66,521	66,521
14865	Yallingup Beach Road - Reseal SLK 0 to 0.19 (C)	24,805	12,791	12,791	30,698	30,698
14866	Old Vasse Highway - Reseal SLK 0 to 1.83 (C)	-	5,640	5,640	13,535	13,535
14867	McDonald Road - Unsealed Resheet SLK 0.02 to 0.88 (C)	-	25,222	25,222	60,534	60,534
14868	Chambers Road - Unsealed Resheet SLK 2.1 to 4.26 (C)	-	63,270	63,270	151,848	151,848
14869	Silverwood Road - Unsealed Resheet SLK 0.01 to 1.3 (C)	67,973	38,047	38,047	91,314	91,314
14870	Downs Road - Unsealed Resheet SLK 2.0 to 3.35 (C)	38,764	39,758	39,758	95,418	95,418
15085	Jalbarragup Road New Shoulders (C)	-	1,162,947	1,162,947	1,744,420	1,744,420
15103	Prince Street Laneway(C)	28,003	-	-	28,003	-
15108	Bussell Highway Resurfacing SLK 0 to 3.8 (C)	66,256	-	-	181,000	-
	<b>Total Roads Infrastructure</b>	<b>2,035,201</b>	<b>4,532,556</b>	<b>4,607,973</b>	<b>10,093,354</b>	<b>10,093,354</b>
<b>Bridges Infrastructure</b>						
12379	Roy Road - Bridge Construction - Bridge 3373A(C)	-	-	-	-	-
	<b>Total Bridges Infrastructure</b>	<b>-</b>	<b>-</b>	<b>-</b>	<b>-</b>	<b>-</b>
<b>Car Parks Infrastructure</b>						
10608	Dunsborough Yacht Club Carpark(C)	379	-	-	-	-
12653	Hotel Site 2 Carpark(C)	600	-	-	-	-
14815	Carpark Hotel Site 2(C)	101,776	192,546	192,546	192,546	192,546
14873	Forrest Beach Horse Float Carparking Area(C)	-	81,019	81,019	194,446	194,446
14874	Disabled Parking Bays BussFforeshore (Old ennisCourt)(C)	-	4,167	4,167	10,000	10,000
14875	Eagle Bay Boat Ramp Parking off Fern Road(C)	2,589	-	12,500	-	30,000
14876	Eagle Crescent on road parking - Eagle Bay(C)	4,319	15,417	15,417	60,000	37,000
14877	Rotary Park Stage 1 - Design, permits and clearing(C)	-	8,333	8,333	20,000	20,000
14878	Senior Citizens Universal Access Parking(C)	1,022	24,122	24,122	57,893	57,893
	<b>Total Car Parks Infrastructure</b>	<b>110,685</b>	<b>325,604</b>	<b>338,104</b>	<b>534,885</b>	<b>541,885</b>
<b>Footpaths Infrastructure</b>						
10741	Busselton CBD Footpath Renewal(C)	398	36,390	36,390	54,585	54,585
10903	DAIP - Disability Access(C)	6,231	12,500	12,500	30,000	30,000
11986	Causeway Road Shared Path(C)	104,696	-	-	105,000	-
12727	Valley Road Footpath(C)	103	-	-	-	-
12816	Coastal Principled Shares Path (C)	241	70,488	70,488	70,488	70,488
12818	Kaloorup Road (C)	124	-	-	-	-
14814	Townscape Works Busselton(C)	-	66,666	66,666	100,000	100,000
14887	Coastal Shared Path - Forth St to Holgate(C)	-	191,667	191,667	460,000	460,000
14888	Bay View Street - Bussell Hwy to Dorset Street(C)	1,039	52,583	52,583	126,198	126,198

<b>City of Busselton Capital Acquisitions &amp; Construction Report For the period ended 30th November 2023</b>						
Project	Project Description	2023/24 Actual YTD	2023/24 Amended Budget YTD	2023/24 Original Budget YTD	2023/24 Amended Budget	2023/24 Budget Original
		\$	\$	\$	\$	\$
14889	Queen St - Queen St & Prince St Roundabout Zebra Cross(C)	-	6,667	6,667	16,000	16,000
14890	Carey St - Kent Street and Harris to Peel(C)	-	47,453	47,453	113,886	113,886
14891	Adelaide Street - Lot 11 to Stanley St(C)	4,134	23,513	23,513	56,430	56,430
14892	Peron Avenue - Chester Way to Pimlea Parade(C)	-	61,250	61,250	147,000	147,000
14893	Bay View Crescent - Curtis Bay Lot 62 to Lot 4(C)	3,065	22,917	22,917	55,000	55,000
14894	Telstra Park - Lot 4 Naturaliste Terrace(C)	17,673	8,333	8,333	20,000	20,000
14895	Vincent Street to Geographe Bay Road (Reserve 44343)(C)	548	166,667	166,667	400,000	400,000
14896	Dunsborough Lakes Drive - Caves Road(C)	1,576	6,250	6,250	15,000	15,000
14897	Naturaliste Terrace Footbridge Renewal #6017 Lot 254(C)	3,380	45,833	45,833	110,000	110,000
14905	GLC Footpath NIB (C)	8,284	12,000	12,000	20,000	20,000
15071	Upgrade Wagin Gardens Footpath Access (C)	5,335	12,000	12,000	20,000	20,000
15072	Upgrade Harris Rd POS Access (C)	17,602	6,000	6,000	10,000	10,000
	<b>Total Footpaths Infrastructure</b>	<b>174,430</b>	<b>849,175</b>	<b>849,175</b>	<b>1,929,587</b>	<b>1,824,587</b>
<b>Parks, Gardens and Reserves</b>						
10613	Meelup Coastal Parking & Landscaping(C)	74,558	89,119	89,119	178,238	178,238
10688	Depot Washdown Facility Upgrades(C)	-	19,175	19,175	76,700	76,700
10765	Foreshore Busselton - High Street to Carey Street(C)	-	20,833	20,833	50,000	50,000
10766	Foreshore Yallingup Capital(C)	-	8,333	8,333	20,000	20,000
10767	Port Geographe General Improvements/ Foreshore(C)	182	-	-	-	-
10831	Rotary Park / War Memorial Relocation(C)	37,183	75,000	75,000	525,000	525,000
10951	Mitchell Park Upgrade(C)	401	-	-	37,000	37,000
10952	Possum Park Barnard East Upgrade(C)	-	10,000	10,000	30,000	30,000
10953	Youth Skate Park(C)	-	8,333	8,333	20,000	20,000
10963	Minor Capital Improvements, Fencing, Seating, Lighting et(C)	-	16,667	16,667	40,000	40,000
10966	Provence SAR Area General Improvements to the Area(C)	-	66,667	66,667	160,000	160,000
10967	New Shade Sails – multiple locations (C)	-	110,485	110,485	265,165	265,165
10969	Vasse SAR Area General Improvements to the Area(C)	10,979	29,167	29,167	70,000	70,000
11024	Pioneer Cemetery - Implement Conservation Plan(C)	319	8,865	8,865	21,276	21,276
11035	Park Furniture Renewals - Replace aged & unsafe Equip(C)	8,405	22,917	22,917	55,000	55,000
11117	Limestone Quarry - POS Upgrade(C)	1,017	-	-	-	-
12251	Cricket Wicket Renewal(C)	8,636	-	-	30,000	30,000
12254	Busselton Tennis Club - Infrastructure(C)	37,843	24,298	24,298	46,000	46,000
12267	Busselton Townscape Street Furniture Renewals (C)	-	16,667	16,667	40,000	40,000
12382	Rails to Trails - Continuation of Implementation Plan(C)	1,650	43,750	43,750	105,000	105,000
12407	Dunsborough Lakes Sporting Precinct (Stage 1)(C)	160	312,857	312,857	730,000	730,000
12421	City Lined Landfill Stage 2 - Preliminary Works(C)	49,657	289	289	80,000	80,000
12425	Busselton Landfill Post-closure Capping, Rehab & Remediat(C)	-	-	-	2,000,000	2,000,000
12511	Dunsborough - BMX / Skatebowl(C)	-	4,167	4,167	10,000	10,000
12528	Dunsborough Town Centre/ Foreshore(C)	-	12,500	12,500	30,000	30,000
12661	Busselton Foreshore Renewals(C)	536	-	-	-	-
12780	BPACC - Landscaping (C)	3,213	-	-	500,000	500,000
12821	Churchill Park - Main Oval Renewal (C)	5,969	229,167	229,167	550,000	550,000
12846	Mitchell Park Landscape Upgrade Stage 2(C)	441	-	-	150,000	150,000
12849	Sport Oval Lighting - Vasse Ovals(C)	152,159	-	-	116,731	116,731
12850	Dunsborough Lakes Sporting Precinct (Stage 1) - Planning (C)	-	125,000	125,000	150,000	150,000
14811	RBFS Old Dunsborough Boat Ramp Scour Protection (C)	-	22,160	22,160	33,240	33,240
14900	Metricup Cemetery - Paths & Ashes iternment(C)	209	4,167	4,167	10,000	10,000
14901	Port Geographe POS Retaining Wall Renewals(C)	1,273	22,083	22,083	53,000	53,000
14903	Centennial Park Irrigation renewal(C)	-	52,083	52,083	125,000	125,000
14904	Beach Shower/Water Fountain Ren Busselton Foreshore(C)	503	8,333	8,333	20,000	20,000
14906	Dunsborough Oval Skatepark Lighting Renewal(C)	-	29,167	29,167	70,000	70,000
14907	Busselton Foreshore - Annual Renewals (C)	20,891	20,833	20,833	50,000	50,000
14908	Dunsborough Town Centre & Foreshore - Annual Renewals (C)	-	12,500	12,500	30,000	30,000
14909	Lou Weston Basketball System upgrades (C)	16,550	13,333	13,333	32,000	32,000
14910	Seymour Park - Replacement of Decking(C)	1,150	21,083	21,083	50,600	50,600
14911	Jarraewood Playground Equipment Renewal(C)	-	10,833	10,833	26,000	26,000
14912	Newtown Oval New Playground Equipment(C)	-	6,250	6,250	15,000	15,000
14913	Heron Place - New New Playground Equipment(C)	-	4,167	4,167	10,000	10,000
14914	Lou Weston Playground Equipment Renewal(C)	-	25,000	25,000	60,000	60,000
14915	Busselton Foreshore disability carousel renewal(C)	-	18,750	18,750	45,000	45,000
14916	Dunsborough Foreshore disability carousel renewal(C)	-	18,750	18,750	45,000	45,000
14917	Meelup Regional Park - Capital Projects(C)	20,878	74,167	74,167	178,000	178,000
14918	BBQ Placement and Replacement(C)	-	12,500	12,500	30,000	30,000
14919	Beach Access stairs (C)	-	22,917	22,917	55,000	55,000
14944	Busselton Works Depot Parks & Gardens Product Bays(C)	-	8,333	8,333	20,000	20,000
	<b>Total Parks, Gardens and Reserves</b>	<b>454,764</b>	<b>1,661,665</b>	<b>1,661,665</b>	<b>7,043,950</b>	<b>7,043,950</b>
<b>Drainage Infrastructure</b>						
10692	Carey Street Drainage Upgrade(C)	84	-	-	-	-

City of Busselton Capital Acquisitions & Construction Report For the period ended 30th November 2023						
Project	Project Description	2023/24 Actual YTD	2023/24 Amended Budget YTD	2023/24 Original Budget YTD	2023/24 Amended Budget	2023/24 Budget Original
		\$	\$	\$	\$	\$
12815	North Street Drainage Works(C)	9,299	57,787	57,787	231,149	231,149
14879	Busselton LIA Drainage Investigation & Design(C)	522	20,833	20,833	50,000	50,000
14880	Chapman Street Mitigate private property inundation(C)	-	104,167	104,167	250,000	250,000
14881	Dunsborough Lakes Drive No 69/71(C)	239	46,756	46,756	112,215	112,215
14882	Dunsborough Lakes Condition inspection & Design(C)	-	20,833	20,833	50,000	50,000
14883	Carey Street Drainage Renewals(C)	8,015	105,372	105,372	252,894	252,894
14884	Depuch Close - Eagle Bay(C)	40,486	31,864	31,864	76,473	76,473
14885	Heppingstone & Hale Street intersection(C)	617	14,049	14,049	33,718	33,718
14886	Hester Steet Drainage Renewal SLK0.33 (C)	5,749	7,511	7,511	18,027	18,027
14945	WSUD Improv Drainage Outlets and Catchments(C)	-	-	-	30,000	30,000
	<b>Total Drainage Infrastructure</b>	<b>64,844</b>	<b>409,173</b>	<b>409,173</b>	<b>1,104,476</b>	<b>1,104,476</b>
<b>Regional Airport and Industrial Park</b>						
10583	Airport Construction Stage 2, Airfield(C)	-	337,478	337,478	337,478	337,478
12941	Airport - Other Buildings(C)	-	2,083	2,083	5,000	5,000
14836	RFDS relocation(C)	-	-	-	366,180	366,180
14838	BMRAP - Apron Lighting(C)	-	-	-	65,000	65,000
14839	BMRAP - Apron CCTV(C)	-	-	-	50,000	50,000
15060	BMRA MAGS Improvements (C)	-	-	-	17,000	17,000
15061	BMRA Gate 1 Replacement (C)	-	-	-	10,000	10,000
15062	BMRA GA Precinct Reseal (C)	-	-	-	437,620	437,620
	<b>Total Regional Airport and Industrial Park</b>	<b>-</b>	<b>339,561</b>	<b>339,561</b>	<b>1,288,278</b>	<b>1,288,278</b>
	<b>Infrastructure Total</b>	<b>2,839,924</b>	<b>8,117,734</b>	<b>8,205,651</b>	<b>21,994,529</b>	<b>21,896,529</b>
	<b>Grand Total</b>	<b>9,383,465</b>	<b>24,216,139</b>	<b>24,304,056</b>	<b>59,559,485</b>	<b>59,461,485</b>

**City of Busseton Reserve Movements**  
**For Period Ended 30 November 2023**

Project Number	Transaction Type	Project Description	YTD Actual	YTD Budget	Budget Original	YTD BudgetA	Budget Amended
<b>1001 - Airport Existing Terminal Building Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(456,382.11)	(456,382.11)	(456,382.11)	(456,382.11)	(456,382.11)
10904	Interest Earned	Other General Purpose Funding(O)	3,569.34	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	452,812.77	456,382.11	456,382.11	456,382.11	456,382.11
		<i>Airport Existing Terminal Building Reserve Closing Balance</i>	0.00	0.00	0.00	0.00	0.00
<b>1003 - Airport Marketing &amp; Incentive Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(4,895,380.26)	(4,895,380.26)	(4,895,380.26)	(4,895,380.26)	(4,895,380.26)
10904	Interest Earned	Other General Purpose Funding(O)	43,808.44	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	4,851,571.82	4,895,380.26	4,895,380.26	4,895,380.26	4,895,380.26
		<i>Airport Marketing &amp; Incentive Reserve Closing Balance</i>	0.00	0.00	0.00	0.00	0.00
<b>1004 - Airport New Terminal Building Reserve</b>							
			0.00	0.00	0.00	0.00	0.00
10904	Interest Earned	Other General Purpose Funding(O)	0.00	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
		<i>Airport New Terminal Building Reserve Closing Balance</i>	0.00	0.00	0.00	0.00	0.00
<b>1005 - Barnard Park Sports Pavilion Building Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(115,031.42)	(115,031.42)	(115,031.42)	(115,031.42)	(115,031.42)
10904	Interest Earned	Other General Purpose Funding(O)	909.80	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	114,121.62	115,031.42	115,031.42	115,031.42	115,031.42
		<i>Barnard Park Sports Pavilion Building Reserve Closing Balance</i>	0.00	0.00	0.00	0.00	0.00
<b>1006 - Building Asset Renewal Reserve Fund - General Buildings</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(3,389,334.17)	(3,389,334.17)	(3,389,334.17)	(3,389,334.17)	(3,389,334.17)
10904	Interest Earned	Other General Purpose Funding(O)	(147,210.18)	(97,911.25)	(234,986.97)	(97,911.25)	(234,986.97)
10027	Transfer To Reserve	Asset Management Administration(O)	(1,083,507.78)	(3,459,152.23)	(4,441,390.00)	(3,459,152.23)	(4,441,390.00)
10904	Transfer To Reserve	Other General Purpose Funding(O)	(2,375,644.45)	0.00	0.00	0.00	0.00
10016	Transfer From Reserve	Civic and Administration Centre Minor Upgrades(C)	0.00	0.00	35,000.00	0.00	35,000.00
10025	Transfer From Reserve	Weld Theatre(C)	0.00	0.00	250,000.00	0.00	250,000.00
10027	Transfer From Reserve	Asset Management Administration(O)	0.00	0.00	21,600.00	0.00	21,600.00
10163	Transfer From Reserve	Old Fire Station Building(O)	0.00	0.00	15,000.00	0.00	15,000.00
10167	Transfer From Reserve	Dunsborough Hall(O)	0.00	0.00	5,000.00	0.00	5,000.00
10179	Transfer From Reserve	Youth Services(O)	0.00	0.00	14,000.00	0.00	14,000.00
10243	Transfer From Reserve	Depot Building-Busseton(O)	0.00	0.00	10,037.00	0.00	10,037.00
10787	Transfer From Reserve	Geographe Leisure Centre Upgrade(C)	0.00	0.00	567,464.00	0.00	567,464.00
10853	Transfer From Reserve	Asbestos Removal & Replacement(O)	0.00	0.00	50,000.00	0.00	50,000.00
11761	Transfer From Reserve	Abbey Beach Boat Ramp Toilets(O)	0.00	0.00	8,533.00	0.00	8,533.00
12331	Transfer From Reserve	Smiths Beach New Public Toilet(C)	0.00	0.00	250,000.00	0.00	250,000.00
12795	Transfer From Reserve	Busseton Depot CCTV & Electronic Gate(C)	0.00	0.00	16,259.00	0.00	16,259.00
12796	Transfer From Reserve	Depot Training Room (C)	0.00	0.00	75,000.00	0.00	75,000.00
12798	Transfer From Reserve	High Street Hall (C)	0.00	0.00	10,500.00	0.00	10,500.00
12800	Transfer From Reserve	Old Courthouse and Jail Cells (C)	0.00	0.00	102,853.00	0.00	102,853.00
12939	Transfer From Reserve	Administration Building- 2-16 Southern Drive(C)	0.00	0.00	45,000.00	0.00	45,000.00
12940	Transfer From Reserve	Depot Building-Busseton((C)	0.00	0.00	270,000.00	0.00	270,000.00
12947	Transfer From Reserve	Old Court House(O)	0.00	0.00	79,200.00	0.00	79,200.00
14942	Transfer From Reserve	Lions Park Shopping Centre Toilets (C)	0.00	0.00	250,000.00	0.00	250,000.00
14943	Transfer From Reserve	Changing Places Accessable Ablutions (C)	0.00	0.00	100,000.00	0.00	100,000.00
15063	Transfer From Reserve	NCC - Les Mills Pump Rack (C)	0.00	0.00	8,000.00	0.00	8,000.00
15064	Transfer From Reserve	GLC Cafe Double Fridge (C)	0.00	0.00	20,000.00	0.00	20,000.00
15074	Transfer From Reserve	Vasse Toilet (C)	0.00	0.00	125,000.00	0.00	125,000.00
15075	Transfer From Reserve	Vasse Toilet (O)	0.00	0.00	35,000.00	0.00	35,000.00
		<i>Building Asset Renewal Reserve Fund - General Buildings Closing Balance</i>	(6,995,696.58)	(6,946,397.65)	(5,702,265.14)	(6,946,397.65)	(5,702,265.14)
<b>1007 - Busseton Area Drainage and Waterways Improvement Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(134,559.27)	(134,559.27)	(134,559.27)	(134,559.27)	(134,559.27)
10904	Interest Earned	Other General Purpose Funding(O)	1,090.34	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	133,468.93	134,559.27	134,559.27	134,559.27	134,559.27
		<i>Busseton Area Drainage and Waterways Improvement Reserve Closing Balance</i>	0.00	0.00	0.00	0.00	0.00
<b>1008 - Busseton Community Resource Centre Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(531,460.32)	(531,460.32)	(531,460.32)	(531,460.32)	(531,460.32)
10904	Interest Earned	Other General Purpose Funding(O)	4,337.70	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	527,122.62	531,460.32	531,460.32	531,460.32	531,460.32
		<i>Busseton Community Resource Centre Reserve Closing Balance</i>	0.00	0.00	0.00	0.00	0.00
<b>1009 - Busseton Foreshore Maintenance Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(13,115.45)	(13,115.45)	(13,115.45)	(13,115.45)	(13,115.45)
10904	Interest Earned	Other General Purpose Funding(O)	106.28	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	13,009.17	13,115.45	13,115.45	13,115.45	13,115.45
		<i>Busseton Foreshore Maintenance Reserve Closing Balance</i>	0.00	0.00	0.00	0.00	0.00
<b>1010 - Busseton Jetty Tourist Park Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(1,971,964.57)	(1,971,964.57)	(1,971,964.57)	(1,971,964.57)	(1,971,964.57)
10904	Interest Earned	Other General Purpose Funding(O)	(42,654.49)	(21,956.80)	(52,696.38)	(21,956.80)	(52,696.38)
10297	Transfer To Reserve	Busseton Jetty Tourist Park(O)	(189,138.50)	(189,138.50)	(453,932.50)	(189,138.50)	(453,932.50)
11040	Transfer From Reserve	Economic and Business Development(O)	0.00	0.00	149,344.00	0.00	149,344.00
12868	Transfer From Reserve	Busseton Jetty Tourist Park - Cabin Replacement/Upgrade (C)	0.00	0.00	540,000.00	0.00	540,000.00
12870	Transfer From Reserve	Busseton Jetty Tourist Park- Replace Holden Colorado 4x4(C)	0.00	0.00	25,000.00	0.00	25,000.00
12872	Transfer From Reserve	Busseton Jetty Tourist Park - Battery Powered Mule (C)	0.00	0.00	30,000.00	0.00	30,000.00
		<i>Busseton Jetty Tourist Park Reserve Closing Balance</i>	(2,203,757.56)	(2,183,059.87)	(1,734,249.45)	(2,183,059.87)	(1,734,249.45)
<b>1011 - Busseton Library Building Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(155,509.60)	(155,509.60)	(155,509.60)	(155,509.60)	(155,509.60)
10904	Interest Earned	Other General Purpose Funding(O)	1,217.70	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	154,291.90	155,509.60	155,509.60	155,509.60	155,509.60
		<i>Busseton Library Building Reserve Closing Balance</i>	0.00	0.00	0.00	0.00	0.00

**City of Busselton Reserve Movements  
For Period Ended 30 November 2023**

Project Number	Transaction Type	Project Description	YTD Actual	YTD Budget	Budget Original	YTD BudgetA	Budget Amended
<b>1012 - CBD Enhancement Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(9,195.11)	(9,195.11)	(9,195.11)	(9,195.11)	(9,195.11)
10904	Interest Earned	Other General Purpose Funding(O)	36.36	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	9,158.75	9,195.11	9,195.11	9,195.11	9,195.11
		<i>CBD Enhancement Reserve Closing Balance</i>	0.00	0.00	0.00	0.00	0.00
<b>1013 - Cemetery Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(365,556.55)	(365,556.55)	(365,556.55)	(365,556.55)	(365,556.55)
10904	Interest Earned	Other General Purpose Funding(O)	2,933.49	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	362,623.06	365,556.55	365,556.55	365,556.55	365,556.55
		<i>Cemetery Reserve Closing Balance</i>	0.00	0.00	0.00	0.00	0.00
<b>1014 - City Car Parking and Access Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(1,436,683.08)	(1,436,683.08)	(1,436,683.08)	(1,436,683.08)	(1,436,683.08)
10904	Interest Earned	Other General Purpose Funding(O)	(23,912.08)	(24,350.85)	(58,442.00)	(24,350.85)	(58,442.00)
10027	Transfer To Reserve	Asset Management Administration(O)	(72,330.85)	(72,330.85)	(173,594.00)	(72,330.85)	(173,594.00)
10904	Transfer From Reserve	Other General Purpose Funding(O)	250,000.00	250,000.00	250,000.00	250,000.00	250,000.00
14815	Transfer From Reserve	Carpark Hotel Site 2(C)	0.00	0.00	192,546.00	0.00	192,546.00
14873	Transfer From Reserve	Forrest Beach Horse Float Carparking Area(C)	0.00	0.00	194,446.00	0.00	194,446.00
14874	Transfer From Reserve	Disabled Parking Bays BussForeshore (Old ennisCourt)(C)	0.00	0.00	10,000.00	0.00	10,000.00
14875	Transfer From Reserve	Eagle Bay Boat Ramp Parking off Fern Road(C)	0.00	0.00	30,000.00	0.00	0.00
14876	Transfer From Reserve	Eagle Crescent on road parking - Eagle Bay(C)	0.00	0.00	37,000.00	0.00	37,000.00
14877	Transfer From Reserve	Rotary Park Stage 1 - Design, permits and clearing(C)	0.00	0.00	20,000.00	0.00	20,000.00
14878	Transfer From Reserve	Senior Citizens Universal Access Parking(C)	0.00	0.00	57,892.74	0.00	57,892.74
		<i>City Car Parking and Access Reserve Closing Balance</i>	(1,282,926.01)	(1,283,364.78)	(876,834.34)	(1,283,364.78)	(906,834.34)
<b>1015 - Civic and Administration Building Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(900,462.52)	(900,462.52)	(900,462.52)	(900,462.52)	(900,462.52)
10904	Interest Earned	Other General Purpose Funding(O)	7,140.23	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	893,322.29	900,462.52	900,462.52	900,462.52	900,462.52
		<i>Civic and Administration Building Reserve Closing Balance</i>	0.00	0.00	0.00	0.00	0.00
<b>1016 - Coastal and Climate Adaptation Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(1,194,818.65)	(1,194,818.65)	(1,194,818.65)	(1,194,818.65)	(1,194,818.65)
10904	Interest Earned	Other General Purpose Funding(O)	(28,675.12)	(8,630.75)	(20,713.84)	(8,630.75)	(20,713.84)
10027	Transfer To Reserve	Asset Management Administration(O)	(477,061.25)	(477,061.25)	(1,144,947.00)	(477,061.25)	(1,144,947.00)
12811	Transfer From Reserve	Sand Re-Nourishment(O)	0.00	0.00	110,000.00	0.00	110,000.00
12873	Transfer From Reserve	Coastal Protection - Beach Monitoring Year 10 (O)	0.00	0.00	53,300.00	0.00	53,300.00
12875	Transfer From Reserve	Coastal Protection - Coastal Structures Inspections and R(O)	0.00	0.00	6,664.00	0.00	6,664.00
14736	Transfer From Reserve	PACP Coastal Grant Projects(O)	0.00	0.00	1,324,521.00	0.00	1,324,521.00
14929	Transfer From Reserve	Coastal Adaptation and Protection Plan(O)	0.00	0.00	142,000.00	0.00	142,000.00
		<i>Coastal and Climate Adaptation Reserve Closing Balance</i>	(1,700,555.02)	(1,680,510.65)	(723,994.49)	(1,680,510.65)	(723,994.49)
<b>1017 - Commonage Community Facilities Dunsborough Lakes SR</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(76,996.33)	(76,996.33)	(76,996.33)	(76,996.33)	(76,996.33)
10904	Interest Earned	Other General Purpose Funding(O)	623.91	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	76,372.42	76,996.33	76,996.33	76,996.33	76,996.33
		<i>Commonage Community Facilities Dunsborough Lakes SR Closing Balance</i>	0.00	0.00	0.00	0.00	0.00
<b>1018 - Commonage Community Facilities South Biddle Precinct Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(938,927.52)	(938,927.52)	(938,927.52)	(938,927.52)	(938,927.52)
10904	Interest Earned	Other General Purpose Funding(O)	746.24	(17,821.20)	(42,770.88)	(17,821.20)	(42,770.88)
			0.00	0.00	0.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	938,181.28	0.00	0.00	956,748.72	981,698.40
		<i>Commonage Community Facilities South Biddle Precinct Reserve Closing Balance</i>	0.00	(956,748.72)	(981,698.40)	0.00	0.00
<b>1019 - Commonage Precinct Bushfire Facilities Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(60,709.28)	(60,709.28)	(60,709.28)	(60,709.28)	(60,709.28)
10904	Interest Earned	Other General Purpose Funding(O)	48.24	(1,152.30)	(2,765.50)	(1,152.30)	(2,765.50)
			0.00	0.00	0.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	60,661.04	0.00	0.00	61,861.58	63,474.78
		<i>Commonage Precinct Bushfire Facilities Reserve Closing Balance</i>	0.00	(61,861.58)	(23,474.78)	0.00	0.00
<b>1020 - Commonage Precinct Infrastructure Road Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(844.18)	(844.18)	(844.18)	(844.18)	(844.18)
10904	Interest Earned	Other General Purpose Funding(O)	6.84	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
		<i>Commonage Precinct Infrastructure Road Reserve Closing Balance</i>	0.00	0.00	0.00	0.00	0.00
<b>1021 - Community Facilities - Airport North</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(3,282,875.59)	(3,282,875.59)	(3,282,875.59)	(3,282,875.59)	(3,282,875.59)
10904	Interest Earned	Other General Purpose Funding(O)	2,609.12	(64,579.65)	(154,991.14)	(64,579.65)	(154,991.14)
10904	Transfer To Reserve	Other General Purpose Funding(O)	(78,666.41)	0.00	0.00	0.00	0.00
11026	Transfer To Reserve	Planning and Development Management(O)	0.00	(49,460.00)	(118,704.00)	(49,460.00)	(118,704.00)
11026	Transfer From Reserve	Planning and Development Management(O)	0.00	0.00	0.00	49,460.00	118,704.00
		<i>Community Facilities - Airport North Closing Balance</i>	0.00	(3,396,915.24)	(3,556,570.73)	0.00	0.00
<b>1022 - Community Facilities - Broadwater</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(275,330.68)	(275,330.68)	(275,330.68)	(275,330.68)	(275,330.68)
10904	Interest Earned	Other General Purpose Funding(O)	(939.46)	(4,184.15)	(10,041.91)	(4,184.15)	(10,041.91)
10904	Transfer From Reserve	Other General Purpose Funding(O)	276,270.14	0.00	0.00	279,514.83	285,372.59
11026	Transfer From Reserve	Planning and Development Management(O)	0.00	0.00	0.00	6,776.25	16,263.00
		<i>Community Facilities - Broadwater Closing Balance</i>	0.00	(286,291.08)	(301,635.59)	0.00	0.00

**City of Busseton Reserve Movements**  
**For Period Ended 30 November 2023**

Project Number	Transaction Type	Project Description	YTD Actual	YTD Budget	Budget Original	YTD BudgetA	Budget Amended
<b>1023 - Community Facilities - Busseton</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(101,930.40)	(101,930.40)	(101,930.40)	(101,930.40)	(101,930.40)
10904	Interest Earned	Other General Purpose Funding(O)	(7,421.96)	(1,879.25)	(4,510.19)	(1,879.25)	(4,510.19)
10904	Transfer From Reserve	Other General Purpose Funding(O)	109,352.36	0.00	0.00	103,809.65	106,440.59
11026	Transfer From Reserve	Planning and Development Management(O)	0.00	0.00	0.00	9,207.10	22,097.00
14909	Transfer From Reserve	Lou Weston Basketball System upgrades (C)	0.00	0.00	32,000.00	0.00	0.00
14914	Transfer From Reserve	Lou Weston Playground Equipment Renewal(C)	0.00	0.00	60,000.00	0.00	0.00
<i>Community Facilities - Busseton Closing Balance</i>			<b>0.00</b>	(113,016.75)	(36,537.59)	(0.00)	0.00
<b>1024 - Community Facilities - City District</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(1,320,534.07)	(1,320,534.07)	(1,320,534.07)	(1,320,534.07)	(1,320,534.07)
10904	Interest Earned	Other General Purpose Funding(O)	1,357.01	(15,619.75)	(37,487.44)	(15,619.75)	(37,487.44)
10904	Transfer To Reserve	Other General Purpose Funding(O)	(55,139.38)	0.00	0.00	0.00	0.00
11026	Transfer To Reserve	Planning and Development Management(O)	0.00	(148,895.00)	(357,348.00)	(148,895.00)	(357,348.00)
10642	Transfer From Reserve	Dunsborough Lakes Sporting Precinct-Pavilion/Changeroom F(C)	0.00	0.00	95,350.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	1,374,316.44	0.00	0.00	1,469,429.07	1,677,882.07
10952	Transfer From Reserve	Possum Park Barnard East Upgrade(C)	0.00	0.00	30,000.00	0.00	0.00
11026	Transfer From Reserve	Planning and Development Management(O)	0.00	0.00	0.00	15,619.75	37,487.44
12382	Transfer From Reserve	Rails to Trails - Continuation of Implementation Plan(C)	0.00	0.00	105,000.00	0.00	0.00
12850	Transfer From Reserve	Dunsborough Lakes Sporting Precinct (Stage 1) - Planning (C)	0.00	0.00	150,000.00	0.00	0.00
14915	Transfer From Reserve	Busseton Foreshore disability carousel renewal(C)	0.00	0.00	45,000.00	0.00	0.00
<i>Community Facilities - City District Closing Balance</i>			<b>0.00</b>	(1,485,048.82)	(1,240,019.51)	0.00	0.00
<b>1025 - Community Facilities - Dunsborough</b>							
10904	Interest Earned	Other General Purpose Funding(O)	415.87	(9,691.80)	(23,260.34)	(9,691.80)	(23,260.34)
11026	Transfer To Reserve	Planning and Development Management(O)	0.00	(14,587.10)	(35,009.00)	(14,587.10)	(35,009.00)
10766	Transfer From Reserve	Foreshore Yallingup Capital(C)	0.00	0.00	20,000.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	522,836.54	0.00	0.00	532,944.21	546,512.75
11026	Transfer From Reserve	Planning and Development Management(O)	0.00	0.00	0.00	14,587.10	35,009.00
12528	Transfer From Reserve	Dunsborough Town Centre/ Foreshore(C)	0.00	0.00	30,000.00	0.00	0.00
14903	Transfer From Reserve	Centennial Park Irrigation renewal(C)	0.00	0.00	125,000.00	0.00	0.00
14916	Transfer From Reserve	Dunsborough Foreshore disability carousel renewal(C)	0.00	0.00	45,000.00	0.00	0.00
<i>Community Facilities - Dunsborough Closing Balance</i>			<b>0.00</b>	(547,531.31)	(361,521.75)	0.00	0.00
<b>1026 - Community Facilities - Dunsborough Lakes</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(900,385.65)	(900,385.65)	(900,385.65)	(900,385.65)	(900,385.65)
10904	Interest Earned	Other General Purpose Funding(O)	715.59	(5,548.80)	(13,317.08)	(5,548.80)	(13,317.08)
			0.00	0.00	0.00	0.00	0.00
10642	Transfer From Reserve	Dunsborough Lakes Sporting Precinct-Pavilion/Changeroom F(C)	0.00	0.00	375,596.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	899,670.06	0.00	0.00	905,934.45	913,702.73
<i>Community Facilities - Dunsborough Lakes Closing Balance</i>			<b>0.00</b>	(905,934.45)	(336,680.73)	(0.00)	0.00
<b>1027 - Community Facilities - Geographe</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(156,185.31)	(156,185.31)	(156,185.31)	(156,185.31)	(156,185.31)
10904	Interest Earned	Other General Purpose Funding(O)	124.11	(2,929.25)	(7,030.20)	(2,929.25)	(7,030.20)
10904	Transfer To Reserve	Other General Purpose Funding(O)	(1,132.57)	0.00	0.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	157,193.77	0.00	0.00	159,114.56	163,215.51
11026	Transfer From Reserve	Planning and Development Management(O)	0.00	0.00	0.00	4,542.50	10,902.00
<i>Community Facilities - Geographe Closing Balance</i>			<b>(0.00)</b>	(163,657.06)	(174,117.51)	0.00	0.00
<b>1028 - Community Facilities - Port Geographe</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(364,198.33)	(364,198.33)	(364,198.33)	(364,198.33)	(364,198.33)
			0.00	0.00	0.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	363,908.89	0.00	0.00	371,110.98	380,788.65
<i>Community Facilities - Port Geographe Closing Balance</i>			<b>0.00</b>	(371,110.98)	(380,788.65)	0.00	0.00
<b>1029 - Community Facilities - Vasse</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(108,013.75)	(108,013.75)	(108,013.75)	(108,013.75)	(108,013.75)
10904	Interest Earned	Other General Purpose Funding(O)	92.30	0.00	0.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	107,921.45	0.00	0.00	108,013.75	116,730.75
12849	Transfer From Reserve	Sport Oval Lighting - Vasse Ovals(C)	0.00	0.00	116,731.00	0.00	0.00
<i>Community Facilities - Vasse Closing Balance</i>			<b>0.00</b>	(108,013.75)	0.25	0.00	0.00
<b>1030 - Corporate IT Systems Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(329,864.57)	(329,864.57)	(329,864.57)	(329,864.57)	(329,864.57)
10027	Transfer To Reserve	Asset Management Administration(O)	(332,807.30)	(332,807.30)	(798,737.64)	(332,807.30)	(798,737.64)
10904	Transfer To Reserve	Other General Purpose Funding(O)	(97,084.09)	(97,863.46)	(97,863.46)	(97,863.46)	(97,863.46)
12876	Transfer From Reserve	ICT Services - Equipment & Software Purchases(C)	0.00	0.00	792,506.00	0.00	792,506.00
<i>Corporate IT Systems Reserve Closing Balance</i>			<b>(773,994.95)</b>	(761,956.23)	(437,369.89)	(761,956.23)	(437,369.89)
<b>1031 - Debt Default Reserve</b>							
10904	Interest Earned	Other General Purpose Funding(O)	857.24	0.00	0.00	0.00	0.00
10904	Transfer To Reserve	Other General Purpose Funding(O)	(857.24)	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
<i>Debt Default Reserve Closing Balance</i>			<b>0.00</b>	0.00	0.00	0.00	0.00
<b>1032 - Election, Value and Corporate Expense Reserve</b>							
10904	Interest Earned	Other General Purpose Funding(O)	(8,551.89)	(6,192.50)	(14,862.02)	(6,192.50)	(14,862.02)
10830	Transfer To Reserve	Members of Council(O)	(61,166.65)	(61,166.65)	(146,800.00)	(61,166.65)	(146,800.00)
10830	Transfer From Reserve	Members of Council(O)	0.00	0.00	180,000.00	0.00	180,000.00
<i>Election, Value and Corporate Expense Reserve Closing Balance</i>			<b>(471,158.88)</b>	(468,799.49)	(383,102.36)	(468,799.49)	(383,102.36)
<b>1033 - Emergency Disaster Recovery Reserve</b>							
10904	Interest Earned	Other General Purpose Funding(O)	779.37	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	97,084.09	97,863.46	97,863.46	97,863.46	97,863.46
<i>Emergency Disaster Recovery Reserve Closing Balance</i>			<b>0.00</b>	0.00	0.00	0.00	0.00

**City of Busseton Reserve Movements**  
**For Period Ended 30 November 2023**

Project Number	Transaction Type	Project Description	YTD Actual	YTD Budget	Budget Original	YTD BudgetA	Budget Amended
<b>1034 - Energy Sustainability Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(247,224.49)	(247,224.49)	(247,224.49)	(247,224.49)	(247,224.49)
10904	Interest Earned	Other General Purpose Funding(O)	2,230.55	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	244,993.94	247,224.49	247,224.49	247,224.49	247,224.49
<i>Energy Sustainability Reserve Closing Balance</i>			<b>0.00</b>	0.00	0.00	0.00	0.00
<b>1035 - Footpath and Cycleways Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(1,726,462.66)	(1,726,462.66)	(1,726,462.66)	(1,726,462.66)	(1,726,462.66)
10904	Interest Earned	Other General Purpose Funding(O)	(39,679.95)	(24,463.25)	(58,711.80)	(24,463.25)	(58,711.80)
10027	Transfer To Reserve	Asset Management Administration(O)	(559,357.90)	(559,357.90)	(1,342,459.00)	(559,357.90)	(1,342,459.00)
10027	Transfer From Reserve	Asset Management Administration(O)	0.00	8,999.60	21,599.00	8,999.60	21,599.00
10741	Transfer From Reserve	Busseton CBD Footpath Renewal(C)	0.00	0.00	54,585.00	0.00	54,585.00
10903	Transfer From Reserve	DAIP - Disability Access(C)	0.00	0.00	30,000.00	0.00	30,000.00
11986	Transfer From Reserve	Causeway Road Shared Path(C)	0.00	0.00	0.00	0.00	30,000.00
11998	Transfer From Reserve	Kaloorup Road(C)	0.00	0.00	0.00	0.00	60,000.00
12816	Transfer From Reserve	Coastal Principled Shares Path (C)	0.00	0.00	70,488.00	0.00	70,488.00
14814	Transfer From Reserve	Townscape Works Busseton(C )	0.00	0.00	100,000.00	0.00	100,000.00
14876	Transfer From Reserve	Eagle Crescent on road parking - Eagle Bay(C)	0.00	0.00	0.00	0.00	23,000.00
14888	Transfer From Reserve	Bay View Street - Bussell Hwy to Dorset Street(C)	0.00	0.00	126,198.00	0.00	126,198.00
14889	Transfer From Reserve	Queen St - Queen St & Prince St Roundabout Zebra Cross(C)	0.00	0.00	16,000.00	0.00	16,000.00
14890	Transfer From Reserve	Carey St - Kent Street and Harris to Peel(C)	0.00	0.00	113,886.00	0.00	113,886.00
14891	Transfer From Reserve	Adelaide Street - Lot 11 to Stanley St(C)	0.00	0.00	56,430.06	0.00	56,430.06
14892	Transfer From Reserve	Peron Avenue - Chester Way to Pimlea Parade(C)	0.00	0.00	147,000.00	0.00	147,000.00
14893	Transfer From Reserve	Bay View Crescent - Curtis Bay Lot 62 to Lot 4(C)	0.00	0.00	55,000.00	0.00	55,000.00
14894	Transfer From Reserve	Telstra Park - Lot 4 Naturaliste Terrace(C)	0.00	0.00	20,000.00	0.00	20,000.00
14895	Transfer From Reserve	Vincent Street to Geographe Bay Road (Reserve 44343)(C)	0.00	0.00	400,000.00	0.00	400,000.00
14896	Transfer From Reserve	Dunsborough Lakes Drive - Caves Road(C)	0.00	0.00	15,000.00	0.00	15,000.00
14897	Transfer From Reserve	Naturaliste Terrace Footbridge Renewal #6017 Lot 254(C)	0.00	0.00	110,000.00	0.00	110,000.00
14899	Transfer From Reserve	Feasibility Study -Dunsborough to Yallingup(O)	0.00	0.00	10,000.00	0.00	10,000.00
14905	Transfer From Reserve	GLC Footpath NIB (C)	0.00	0.00	20,000.00	0.00	20,000.00
15071	Transfer From Reserve	Upgrade Wagin Gardens Footpath Access (C)	0.00	0.00	20,000.00	0.00	20,000.00
<i>Footpath and Cycleways Reserve Closing Balance</i>			<b>(2,325,500.51)</b>	(2,301,284.21)	(1,501,447.40)	(2,301,284.21)	(1,388,447.40)
<b>1036 - Furniture and Equipment Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(483,155.93)	(483,155.93)	(483,155.93)	(483,155.93)	(483,155.93)
10904	Interest Earned	Other General Purpose Funding(O)	(9,804.65)	(1,052.80)	(2,526.78)	(1,052.80)	(2,526.78)
10027	Transfer To Reserve	Asset Management Administration(O)	(61,181.65)	(61,181.65)	(146,836.00)	(61,181.65)	(146,836.00)
12878	Transfer From Reserve	Naturaliste Community Centre (NCC) - Purchase Office Furn(C)	0.00	0.00	20,000.00	0.00	20,000.00
14730	Transfer From Reserve	Geographe Leisure Centre Fitness Equipment(C)	0.00	0.00	18,160.00	0.00	18,160.00
14927	Transfer From Reserve	Dunsborough Depot(C)	0.00	0.00	21,300.00	0.00	21,300.00
14939	Transfer From Reserve	Ice Machine (C)	0.00	0.00	10,000.00	0.00	10,000.00
15082	Transfer From Reserve	Administration Building Furniture& Equipment C	0.00	0.00	15,000.00	0.00	15,000.00
15088	Transfer From Reserve	Busseton Depot Building Furniture & Equipment (C)	0.00	0.00	10,813.00	0.00	10,813.00
<i>Furniture and Equipment Reserve Closing Balance</i>			<b>(554,142.23)</b>	(545,390.38)	(364,254.71)	(545,390.38)	(364,254.71)
<b>1037 - Geographe Leisure Centre Building (GLC) Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(232,470.11)	(232,470.11)	(232,470.11)	(232,470.11)	(232,470.11)
10904	Interest Earned	Other General Purpose Funding(O)	3,399.96	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
<i>Geographe Leisure Centre Building (GLC) Reserve Closing Balance</i>			<b>0.00</b>	0.00	0.00	0.00	0.00
<b>1038 - Jetty Maintenance Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(7,345,734.77)	(7,345,734.77)	(7,345,734.77)	(7,345,734.77)	(7,345,734.77)
10904	Interest Earned	Other General Purpose Funding(O)	(146,470.30)	(132,815.75)	(318,757.80)	(132,815.75)	(318,757.80)
10027	Transfer To Reserve	Asset Management Administration(O)	(143,014.15)	(143,014.15)	(343,234.00)	(143,014.15)	(343,234.00)
10160	Transfer To Reserve	Equinox Cafe(O)	(39,969.90)	(39,969.90)	(95,927.79)	(39,969.90)	(95,927.79)
10161	Transfer To Reserve	Goose Café(O)	(22,155.80)	(22,155.80)	(53,173.90)	(22,155.80)	(53,173.90)
10162	Transfer To Reserve	Micro Brewery Building(O)	(40,298.05)	(40,298.05)	(96,715.27)	(40,298.05)	(96,715.27)
11704	Transfer To Reserve	Busseton Tennis Club Building (Old)(O)	(10,822.90)	(10,822.90)	(25,974.96)	(10,822.90)	(25,974.96)
11798	Transfer To Reserve	Hotel Site 2(O)	(15,790.20)	(15,790.20)	(37,896.49)	(15,790.20)	(37,896.49)
10048	Transfer From Reserve	Busseton Jetty(O)	0.00	0.00	1,828,444.00	0.00	1,828,444.00
<i>Jetty Maintenance Reserve Closing Balance</i>			<b>(8,122,079.37)</b>	(8,108,424.82)	(7,347,746.98)	(8,108,424.82)	(7,347,746.98)
10904	Opening Balance	Other General Purpose Funding(O)	(672,757.83)	(672,757.83)	(672,757.83)	(672,757.83)	(672,757.83)
10904	Interest Earned	Other General Purpose Funding(O)	(13,250.63)	(12,747.95)	(30,595.12)	(12,747.95)	(30,595.12)
10048	Transfer To Reserve	Busseton Jetty(O)	(33,928.70)	(33,928.70)	(81,428.88)	(33,928.70)	(81,428.88)
			0.00	0.00	0.00	0.00	0.00
<i>Jetty Self Insurance Reserve Closing Balance</i>			<b>(719,937.16)</b>	(719,434.48)	(784,781.83)	(719,434.48)	(784,781.83)
10904	Opening Balance	Other General Purpose Funding(O)	(1,656,730.67)	(1,656,730.67)	(1,656,730.67)	(1,656,730.67)	(1,656,730.67)
10904	Interest Earned	Other General Purpose Funding(O)	(31,355.46)	(32,025.25)	(76,860.56)	(32,025.25)	(76,860.56)
11683	Transfer To Reserve	Harris Road Aged Housing(O)	0.00	0.00	(78,865.00)	0.00	(78,865.00)
11685	Transfer To Reserve	Winderup Villas Aged Housing(O)	0.00	0.00	(188,862.00)	0.00	(188,862.00)
12231	Transfer From Reserve	Aged Housing Capital Improvements - Harris Road(C)	0.00	0.00	22,000.00	0.00	22,000.00
12233	Transfer From Reserve	Aged Housing Capital Improvements - Winderup(C)	0.00	0.00	40,000.00	0.00	40,000.00
<b>1041 - Legal Expenses Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(579,578.14)	(579,578.14)	(579,578.14)	(579,578.14)	(579,578.14)
10904	Interest Earned	Other General Purpose Funding(O)	(10,243.20)	(12,476.05)	(29,942.55)	(12,476.05)	(29,942.55)
			0.00	0.00	0.00	0.00	0.00
10824	Transfer From Reserve	Legal Services(O)	0.00	0.00	150,000.00	0.00	150,000.00
<i>Legal Expenses Reserve Closing Balance</i>			<b>(589,821.34)</b>	(592,054.19)	(459,520.69)	(592,054.19)	(459,520.69)

**City of Busselton Reserve Movements**  
**For Period Ended 30 November 2023**

Project Number	Transaction Type	Project Description	YTD Actual	YTD Budget	Budget Original	YTD BudgetA	Budget Amended
<b>1042 - Locke Estate Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(171,194.44)	(171,194.44)	(171,194.44)	(171,194.44)	(171,194.44)
10904	Interest Earned	Other General Purpose Funding(O)	(5,009.94)	(3,281.55)	(7,875.71)	(3,281.55)	(7,875.71)
10180	Transfer To Reserve	Locke Estate(O)	(26,666.65)	(26,666.65)	(64,000.00)	(26,666.65)	(64,000.00)
			0.00	0.00	0.00	0.00	0.00
<i>Locke Estate Reserve Closing Balance</i>			<b>(202,871.03)</b>	(201,142.64)	(243,070.15)	(201,142.64)	(243,070.15)
<b>1043 - Long Service Leave Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(3,940,671.00)	(3,940,671.00)	(3,940,671.00)	(3,940,671.00)	(3,940,671.00)
10904	Interest Earned	Other General Purpose Funding(O)	(71,522.25)	(75,128.95)	(180,309.50)	(75,128.95)	(180,309.50)
10810	Transfer To Reserve	Human Resources & Payroll(O)	(187,500.00)	(187,500.00)	(450,000.00)	(187,500.00)	(450,000.00)
10027	Transfer From Reserve	Asset Management Administration(O)	0.00	0.00	12,260.11	0.00	12,260.11
10041	Transfer From Reserve	Building Services(O)	0.00	0.00	9,938.47	0.00	9,938.47
10179	Transfer From Reserve	Youth Services(O)	0.00	0.00	16,684.91	0.00	16,684.91
10461	Transfer From Reserve	Geographe Leisure Centre(O)	0.00	0.00	27,411.93	0.00	27,411.93
10671	Transfer From Reserve	Development Compliance Services(O)	0.00	0.00	13,586.55	0.00	13,586.55
10678	Transfer From Reserve	Customer Services(O)	0.00	0.00	16,091.57	0.00	16,091.57
10704	Transfer From Reserve	Infrastructure and Environment Support(O)	0.00	0.00	31,653.94	0.00	31,653.94
10706	Transfer From Reserve	Engineering Services Design PWOH(O)	0.00	0.00	5,566.32	0.00	5,566.32
10710	Transfer From Reserve	Environmental Health Services Administration(O)	0.00	0.00	4,154.91	0.00	4,154.91
10711	Transfer From Reserve	Environmental Management Administration(O)	0.00	0.00	25,859.80	0.00	25,859.80
10733	Transfer From Reserve	Financial Services(O)	0.00	0.00	3,058.55	0.00	3,058.55
10790	Transfer From Reserve	Governance and Risk Services(O)	0.00	0.00	43,568.98	0.00	43,568.98
10824	Transfer From Reserve	Legal Services(O)	0.00	0.00	2,765.74	0.00	2,765.74
10901	Transfer From Reserve	Civil Infrastructure Management PWOH(O)	0.00	0.00	26,903.12	0.00	26,903.12
10905	Transfer From Reserve	Other Law, Order & Public Safety(O)	0.00	0.00	40,201.91	0.00	40,201.91
11026	Transfer From Reserve	Planning and Development Management(O)	0.00	0.00	10,138.26	0.00	10,138.26
11027	Transfer From Reserve	Community Planning Support(O)	0.00	0.00	16,151.22	0.00	16,151.22
11124	Transfer From Reserve	Stakeholder Relations(O)	0.00	0.00	4,927.23	0.00	4,927.23
11126	Transfer From Reserve	Rates Administration(O)	0.00	0.00	20,525.25	0.00	20,525.25
12258	Transfer From Reserve	Statutory Planning(O)	0.00	0.00	6,224.28	0.00	6,224.28
<i>Long Service Leave Reserve Closing Balance</i>			<b>(4,199,693.25)</b>	(4,203,299.95)	(4,045,853.59)	(4,203,299.95)	(4,045,853.59)
10904	Opening Balance	Other General Purpose Funding(O)	(1,574,755.62)	(1,574,755.62)	(1,574,755.62)	(1,574,755.62)	(1,574,755.62)
10904	Interest Earned	Other General Purpose Funding(O)	12,539.71	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	1,562,215.91	1,574,755.62	1,574,755.62	1,574,755.62	1,574,755.62
<i>Major Traffic Improvements Reserve Closing Balance</i>			<b>0.00</b>	0.00	0.00	0.00	0.00
10904	Opening Balance	Other General Purpose Funding(O)	(393,497.06)	(393,497.06)	(393,497.06)	(393,497.06)	(393,497.06)
10904	Interest Earned	Other General Purpose Funding(O)	(14,491.24)	(4,755.35)	(11,412.81)	(4,755.35)	(11,412.81)
10188	Transfer To Reserve	Events and Culture(O)	(187,975.45)	(610,975.45)	(1,466,341.00)	(610,975.45)	(1,466,341.00)
10022	Transfer From Reserve	Saltwater Administration(O)	0.00	0.00	100,000.00	0.00	100,000.00
10188	Transfer From Reserve	Events and Culture(O)	0.00	0.00	1,661,401.00	0.00	1,661,401.00
14729	Transfer From Reserve	Events Furniture & Equipment(C)	0.00	0.00	5,138.00	0.00	5,138.00
<i>Marketing &amp; Area Promotion Reserve Closing Balance</i>			<b>(1,018,963.75)</b>	(1,009,227.86)	(104,711.87)	(1,009,227.86)	(104,711.87)
10904	Opening Balance	Other General Purpose Funding(O)	(233,942.56)	(233,942.56)	(233,942.56)	(233,942.56)	(233,942.56)
10904	Interest Earned	Other General Purpose Funding(O)	1,975.57	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	231,966.99	233,942.56	233,942.56	233,942.56	233,942.56
<i>Naturaliste Community Centre Building (NCC) Reserve Closing Balance</i>			<b>0.00</b>	0.00	0.00	0.00	0.00
<b>1047 - New Infrastructure Development Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(756,383.73)	(756,383.73)	(756,383.73)	(756,383.73)	(756,383.73)
10904	Interest Earned	Other General Purpose Funding(O)	(50,718.96)	(4,381.70)	(10,516.10)	(4,381.70)	(10,516.10)
10904	Transfer To Reserve	Other General Purpose Funding(O)	(1,607,301.47)	(1,602,782.00)	(1,602,782.00)	(1,602,782.00)	(1,602,782.00)
10904	Transfer From Reserve	Other General Purpose Funding(O)	(250,000.00)	(250,000.00)	(250,000.00)	(250,000.00)	(250,000.00)
12407	Transfer From Reserve	Dunsborough Lakes Sporting Precinct (Stage 1)(C)	0.00	0.00	358,778.00	0.00	358,778.00
15073	Transfer From Reserve	Lot 9020 Dunsborough (C)	0.00	0.00	900,000.00	0.00	900,000.00
<i>New Infrastructure Development Reserve Closing Balance</i>			<b>(2,664,404.16)</b>	(2,613,547.43)	(1,360,903.83)	(2,613,547.43)	(1,360,903.83)
<b>1048 - Other Infrastructure (Drainage, Signage Etc) Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(721,867.25)	(721,867.25)	(721,867.25)	(721,867.25)	(721,867.25)
10904	Interest Earned	Other General Purpose Funding(O)	(29,720.08)	(12,327.60)	(29,586.24)	(12,327.60)	(29,586.24)
10027	Transfer To Reserve	Asset Management Administration(O)	(331,516.25)	(331,516.25)	(795,639.00)	(331,516.25)	(795,639.00)
10904	Transfer To Reserve	Other General Purpose Funding(O)	(403,782.22)	(496,980.00)	(496,980.00)	(496,980.00)	(496,980.00)
10027	Transfer From Reserve	Asset Management Administration(O)	0.00	0.00	21,600.00	0.00	21,600.00
10157	Transfer From Reserve	Busselton Cemetery(O)	0.00	0.00	72,303.00	0.00	72,303.00
10158	Transfer From Reserve	Dunsborough Cemetery(O)	0.00	0.00	25,620.00	0.00	25,620.00
10159	Transfer From Reserve	Pioneer Cemetery(O)	0.00	0.00	6,240.00	0.00	6,240.00
10700	Transfer From Reserve	Dunsborough Cemetery(C)	0.00	0.00	21,276.00	0.00	21,276.00
11024	Transfer From Reserve	Pioneer Cemetery - Implement Conservation Plan(C)	0.00	0.00	21,276.00	0.00	21,276.00
12815	Transfer From Reserve	North Street Drainage Works(C)	0.00	0.00	231,149.00	0.00	231,149.00
14811	Transfer From Reserve	RBFS Old Dunsborough Boat Ramp Scour Protection (C)	0.00	0.00	8,310.00	0.00	8,310.00
14879	Transfer From Reserve	Busselton LIA Drainage Investigation & Design(C)	0.00	0.00	50,000.00	0.00	50,000.00
14880	Transfer From Reserve	Chapman Street Mitigate private property inundation(C)	0.00	0.00	250,000.00	0.00	250,000.00
14881	Transfer From Reserve	Dunsborough Lakes Drive No 69/71(C)	0.00	0.00	112,215.00	0.00	112,215.00
14882	Transfer From Reserve	Dunsborough Lakes Condition inspection & Design(C)	0.00	0.00	50,000.00	0.00	50,000.00
14883	Transfer From Reserve	Carey Street Drainage Renewals(C)	0.00	0.00	252,893.58	0.00	252,893.58
14884	Transfer From Reserve	Depuch Close - Eagle Bay(C)	0.00	0.00	76,472.88	0.00	76,472.88
14885	Transfer From Reserve	Heppingstone & Hale Street intersection(C)	0.00	0.00	33,718.37	0.00	33,718.37
14886	Transfer From Reserve	Hester Steet Drainage Renewal SLK0.33 (C)	0.00	0.00	18,026.81	0.00	18,026.81
14900	Transfer From Reserve	Metricup Cemetery - Paths & Ashes itemrnt(C)	0.00	0.00	10,000.00	0.00	10,000.00
14945	Transfer From Reserve	WSUD Improv Drainage Outlets and Catchments(C)	0.00	0.00	30,000.00	0.00	30,000.00
<i>Other Infrastructure (Drainage, Signage Etc) Reserve Closing Balance</i>			<b>(1,486,885.80)</b>	(1,562,691.10)	(662,971.85)	(1,562,691.10)	(662,971.85)

**City of Busseton Reserve Movements**  
**For Period Ended 30 November 2023**

Project Number	Transaction Type	Project Description	YTD Actual	YTD Budget	Budget Original	YTD BudgetA	Budget Amended
<b>1049 - Parks, Gardens and Reserves Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(1,349,337.86)	(1,349,337.86)	(1,349,337.86)	(1,349,337.86)	(1,349,337.86)
10904	Interest Earned	Other General Purpose Funding(O)	(33,299.18)	(9,970.00)	(23,927.98)	(9,970.00)	(23,927.98)
10027	Transfer To Reserve	Asset Management Administration(O)	(572,618.75)	(572,618.75)	(1,374,285.00)	(572,618.75)	(1,374,285.00)
10904	Transfer To Reserve	Other General Purpose Funding(O)	(13,009.17)	(13,115.00)	(13,115.00)	(13,115.00)	(13,115.00)
10831	Transfer From Reserve	Rotary Park / War Memorial Relocation(C)	0.00	0.00	375,000.00	0.00	375,000.00
10953	Transfer From Reserve	Youth Skate Park(C)	0.00	0.00	20,000.00	0.00	20,000.00
10963	Transfer From Reserve	Minor Capital Improvements, Fencing, Seating, Lighting et(C)	0.00	0.00	40,000.00	0.00	40,000.00
10967	Transfer From Reserve	New Shade Sails – multiple locations (C)	0.00	0.00	265,165.00	0.00	265,165.00
11035	Transfer From Reserve	Park Furniture Renewals - Replace aged & unsafe Equip(C)	0.00	0.00	55,000.00	0.00	55,000.00
12251	Transfer From Reserve	Cricket Wicket Renewal(C)	0.00	0.00	30,000.00	0.00	30,000.00
12511	Transfer From Reserve	Dunsborough - BMX / Skatebowl(C)	0.00	0.00	10,000.00	0.00	10,000.00
12821	Transfer From Reserve	Churchill Park - Main Oval Renewal (C)	0.00	0.00	550,000.00	0.00	550,000.00
14904	Transfer From Reserve	Beach Shower/Water Fountain Ren Busseton Foreshore(C)	0.00	0.00	20,000.00	0.00	20,000.00
14906	Transfer From Reserve	Dunsborough Oval Skatepark Lighting Renewal(C)	0.00	0.00	70,000.00	0.00	70,000.00
14907	Transfer From Reserve	Busseton Foreshore - Annual Renewals (C)	0.00	0.00	50,000.00	0.00	50,000.00
14908	Transfer From Reserve	Dunsborough Town Centre & Foreshore - Annual Renewals (C)	0.00	0.00	30,000.00	0.00	30,000.00
14910	Transfer From Reserve	Seymour Park - Replacement of Decking(C)	0.00	0.00	50,600.00	0.00	50,600.00
14912	Transfer From Reserve	Newtown Oval New Playground Equipment(C)	0.00	0.00	15,000.00	0.00	15,000.00
14913	Transfer From Reserve	Heron Place - New New Playground Equipment(C)	0.00	0.00	10,000.00	0.00	10,000.00
14917	Transfer From Reserve	Meelup Regional Park - Capital Projects(C)	0.00	0.00	178,000.00	0.00	178,000.00
14918	Transfer From Reserve	BBO Placement and Replacement(C)	0.00	0.00	30,000.00	0.00	30,000.00
14919	Transfer From Reserve	Beach Access stairs (C)	0.00	0.00	55,000.00	0.00	55,000.00
14944	Transfer From Reserve	Busseton Works Depot Parks & Gardens Product Bays(C)	0.00	0.00	20,000.00	0.00	20,000.00
<i>Parks, Gardens and Reserves Reserve Closing Balance</i>			<b>(1,968,264.96)</b>	(1,945,041.61)	(820,900.84)	(1,945,041.61)	(820,900.84)
<b>1050 - Performing Arts and Convention Centre Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(2,467,332.55)	(2,467,332.55)	(2,467,332.55)	(2,467,332.55)	(2,467,332.55)
10904	Interest Earned	Other General Purpose Funding(O)	(46,441.03)	(48,641.20)	(116,738.87)	(48,641.20)	(116,738.87)
			0.00	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
<i>Performing Arts and Convention Centre Reserve Closing Balance</i>			<b>(2,513,773.58)</b>	(2,515,973.75)	(2,584,071.42)	(2,515,973.75)	(2,584,071.42)
<b>1051 - Plant Replacement Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(2,364,208.70)	(2,364,208.70)	(2,364,208.70)	(2,364,208.70)	(2,364,208.70)
10904	Interest Earned	Other General Purpose Funding(O)	(48,012.52)	(8,952.85)	(21,486.82)	(8,952.85)	(21,486.82)
10027	Transfer To Reserve	Asset Management Administration(O)	(467,384.05)	(467,384.05)	(1,121,721.71)	(467,384.05)	(1,121,721.71)
10448	Transfer From Reserve	Miscellaneous Plant & Equipment(O)	0.00	0.00	18,720.00	0.00	18,720.00
12881	Transfer From Reserve	HINO FD1024 Beavertail Truck (Turf) (C)	0.00	0.00	80,000.00	0.00	80,000.00
12882	Transfer From Reserve	Isuzu NPR65-190 Single Cab (Turf) (C)	0.00	0.00	80,000.00	0.00	80,000.00
12884	Transfer From Reserve	KUBOTA F3690 OUTFRONT MOWER (C)	0.00	0.00	21,200.00	0.00	21,200.00
12885	Transfer From Reserve	KUBOTA F3690 OUTFRONT MOWER (C)	0.00	0.00	21,200.00	0.00	21,200.00
12886	Transfer From Reserve	Brush / Tree Chipper (C)	0.00	0.00	100,000.00	0.00	100,000.00
12887	Transfer From Reserve	Plant Purchases (P11) - Redexim Easyspread 1600 Sand Spre(C)	0.00	0.00	19,200.00	0.00	19,200.00
12888	Transfer From Reserve	HINO FS2848 Tipper Truck (C&M) (C)	0.00	0.00	230,000.00	0.00	230,000.00
12890	Transfer From Reserve	Caterpillar CS56 Roller (C&M) (C)	0.00	0.00	130,000.00	0.00	130,000.00
12892	Transfer From Reserve	ISUZU FVZ1400 Tipper Truck (C&M) (C)	0.00	0.00	230,000.00	0.00	230,000.00
12894	Transfer From Reserve	Nissan Xtrail ST-L 2WD (Strategic Planning Coord) (C)	0.00	0.00	18,000.00	0.00	18,000.00
12895	Transfer From Reserve	NISSAN X-TRAIL ST-L 2WD SUV (ASSET MGMT COORD) (C)	0.00	0.00	18,000.00	0.00	18,000.00
12896	Transfer From Reserve	Facilities Operational Ute(C)	0.00	0.00	23,000.00	0.00	23,000.00
12897	Transfer From Reserve	ISUZU D-MAX SX 4X2 SINGLE CAB UTILITY (IRRIGATION) (C)	0.00	0.00	31,000.00	0.00	31,000.00
12898	Transfer From Reserve	mitsubishi triton 4x2 single cab ute (P&G) (C)	0.00	0.00	31,000.00	0.00	31,000.00
12899	Transfer From Reserve	Construction & Maintenance Ute (C)	0.00	0.00	27,000.00	0.00	27,000.00
12900	Transfer From Reserve	TOYOTA HILUX DUAL CAB 4X4 (CONST SUPERVISOR) (C)	0.00	0.00	25,000.00	0.00	25,000.00
12901	Transfer From Reserve	TOYOTA HILUX DUAL CAB 4X4 (MTC SUPERVISOR) (C)	0.00	0.00	25,000.00	0.00	25,000.00
12902	Transfer From Reserve	Manager Governance & Corp Services Vehicle (C)	0.00	0.00	20,000.00	0.00	20,000.00
12904	Transfer From Reserve	Plant Purchases (P11) - Minor Plant (P&G) (C)	0.00	0.00	13,000.00	0.00	13,000.00
12906	Transfer From Reserve	Plant Purchases (P12) - Minor Plant (C&M) (C)	0.00	0.00	18,500.00	0.00	18,500.00
12908	Transfer From Reserve	Program 12 - Minor Plant (Workshop) (C)	0.00	0.00	10,000.00	0.00	10,000.00
12948	Transfer From Reserve	TOYOTA HILUX SR 4X4 DUAL CAB UTILITY (BMO) (C)	0.00	0.00	25,000.00	0.00	25,000.00
14744	Transfer From Reserve	mitsubishi pajero glx 4x4 (MGR REG SERV) (C)	0.00	0.00	21,500.00	0.00	21,500.00
14818	Transfer From Reserve	Wheeled 20t excavator Inc. Accessories(C )	0.00	0.00	485,000.00	0.00	485,000.00
14819	Transfer From Reserve	Positrak (Cat 299) with Accessories (C)	0.00	0.00	250,000.00	0.00	250,000.00
14820	Transfer From Reserve	Trailer for Towing Positrak & Traffic Lights(C )	0.00	0.00	40,000.00	0.00	40,000.00
14821	Transfer From Reserve	Trailer Traffic Lights x 2 (and Sign Cones)(C )	0.00	0.00	45,000.00	0.00	45,000.00
14842	Transfer From Reserve	HINO CREW CAB TIPPER TRUCK (C)	0.00	0.00	77,540.00	0.00	77,540.00
14954	Transfer From Reserve	mitsubishi triton 4x2 single cab (C)	0.00	0.00	25,000.00	0.00	25,000.00
15004	Transfer From Reserve	HUSQVARNA RC318T RIDE ON MOWER (C)	0.00	0.00	15,000.00	0.00	15,000.00
15006	Transfer From Reserve	HINO 917 CREW CAB (C&M) (C)	0.00	0.00	80,000.00	0.00	80,000.00
15008	Transfer From Reserve	ISUZU NPR400 CREW CAB (C&M) (C)	0.00	0.00	80,000.00	0.00	80,000.00
15013	Transfer From Reserve	HINO GH1728 AUTO TRUCK (C&M) (C)	0.00	0.00	350,000.00	0.00	350,000.00
15016	Transfer From Reserve	HINO GF1628 REAR TIPPING TRUCK (P&G) (C)	0.00	0.00	100,000.00	0.00	100,000.00
15023	Transfer From Reserve	ISUZU DMAX 4X2 SINGLE CAB (P&G) FORESHORE IRRIGATION (C)	0.00	0.00	31,000.00	0.00	31,000.00
15025	Transfer From Reserve	TOYOTA HILUX 4X4 EXTRA CAB UTILITY (IRRIGATION) (C)	0.00	0.00	34,000.00	0.00	34,000.00
15028	Transfer From Reserve	FORD RANGER 4X4 DUAL CAB UTE (CESM) (C)	0.00	0.00	35,000.00	0.00	35,000.00
15032	Transfer From Reserve	SUBARU OUTBACK 2.5i PREMIUM WAGON (C)	0.00	0.00	20,000.00	0.00	20,000.00
15034	Transfer From Reserve	SUBARU OUTBACK AWD WAGON (RANGERS CO-ORDINATOR) (C)	0.00	0.00	20,000.00	0.00	20,000.00
15036	Transfer From Reserve	SUBARU OUTBACK 2.5i AWD PREMIUM (MGR CULTURE & EVENTS) (C)	0.00	0.00	20,000.00	0.00	20,000.00
15038	Transfer From Reserve	MAZDA CX9 FWD SUV (Director CCS) (C)	0.00	0.00	25,000.00	0.00	25,000.00
15040	Transfer From Reserve	SUBARU OUTBACK AWD (Director PDS) (C)	0.00	0.00	25,000.00	0.00	25,000.00
15045	Transfer From Reserve	Toyota Fortuner GX 4x4 SUV (Mgr Eng Tech Srv) (C)	0.00	0.00	20,000.00	0.00	20,000.00
15048	Transfer From Reserve	SUBARU FORESTER 2.5i-S AWD (MGR DEV SERV) (C)	0.00	0.00	20,000.00	0.00	20,000.00
15050	Transfer From Reserve	NISSAN X TRAIL STL 4X4 SUV (LIESURE CENTRES COORD) (C)	0.00	0.00	18,000.00	0.00	18,000.00
15052	Transfer From Reserve	mitsubishi outlander ls awd (HR COORD) (C)	0.00	0.00	18,000.00	0.00	18,000.00
15065	Transfer From Reserve	Differential GPS (C)	0.00	0.00	20,000.00	0.00	20,000.00
<i>Plant Replacement Reserve Closing Balance</i>			<b>(2,879,605.27)</b>	(2,840,545.60)	(267,557.23)	(2,840,545.60)	(267,557.23)

**City of Busselton Reserve Movements**  
**For Period Ended 30 November 2023**

Project Number	Transaction Type	Project Description	YTD Actual	YTD Budget	Budget Original	YTD BudgetA	Budget Amended
<b>1052 - Port Geographe Development Reserve (Council)</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(67,299.13)	(67,299.13)	(67,299.13)	(67,299.13)	(67,299.13)
10904	Interest Earned	Other General Purpose Funding(O)	(1,610.96)	(760.25)	(1,824.61)	(760.25)	(1,824.61)
10325	Transfer To Reserve	Port Geographe(O)	(43,734.35)	(43,734.35)	(104,962.40)	(43,734.35)	(104,962.40)
10086	Transfer From Reserve	Pedestrian Bridge (Port Geographe)(O)	0.00	38,937.90	93,451.00	38,937.90	93,451.00
14901	Transfer From Reserve	Port Geographe POS Retaining Wall Renewals(C)	0.00	0.00	53,000.00	0.00	53,000.00
<b>1053 - Port Geographe Waterways Management Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(3,051,048.60)	(3,051,048.60)	(3,051,048.60)	(3,051,048.60)	(3,051,048.60)
10904	Interest Earned	Other General Purpose Funding(O)	(56,281.53)	(57,031.65)	(136,875.96)	(57,031.65)	(136,875.96)
10325	Transfer To Reserve	Port Geographe(O)	(103,187.90)	(103,187.90)	(247,651.00)	(103,187.90)	(247,651.00)
10325	Transfer From Reserve	Port Geographe(O)	0.00	0.00	406,169.00	0.00	406,169.00
<i>Port Geographe Waterways Management Reserve Closing Balance</i>			<b>(3,210,518.03)</b>	(3,211,268.15)	(3,029,406.56)	(3,211,268.15)	(3,029,406.56)
<b>1054 - Professional Development Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(161,394.93)	(161,394.93)	(161,394.93)	(161,394.93)	(161,394.93)
10904	Interest Earned	Other General Purpose Funding(O)	(3,037.84)	(3,055.25)	(7,332.63)	(3,055.25)	(7,332.63)
10810	Transfer From Reserve	Human Resources & Payroll(O)	0.00	0.00	50,000.00	0.00	50,000.00
<i>Professional Development Reserve Closing Balance</i>			<b>(164,432.77)</b>	(164,450.18)	(118,727.56)	(164,450.18)	(118,727.56)
<b>1055 - Provence Landscape Maintenance Reserve (SAR)</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(1,524,829.18)	(1,524,829.18)	(1,524,829.18)	(1,524,829.18)	(1,524,829.18)
10904	Interest Earned	Other General Purpose Funding(O)	(29,838.22)	(26,723.30)	(64,135.96)	(26,723.30)	(64,135.96)
10289	Transfer From Reserve	Almond Green Park (Provence)(O)	0.00	0.00	43,168.00	0.00	43,168.00
10291	Transfer From Reserve	Avignon Park (Provence)(O)	0.00	0.00	15,203.00	0.00	15,203.00
10311	Transfer From Reserve	Lavender Park (Provence)(O)	0.00	0.00	11,949.00	0.00	11,949.00
10429	Transfer From Reserve	Streetscape Medians (Provence)(O)	0.00	0.00	26,544.00	0.00	26,544.00
10966	Transfer From Reserve	Provence SAR Area General Improvements to the Area(C)	0.00	0.00	160,000.00	0.00	160,000.00
11725	Transfer From Reserve	Yalyalup Half Court(O)	0.00	0.00	770.00	0.00	770.00
<b>1056 - Public Art Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(272,932.87)	(272,932.87)	(272,932.87)	(272,932.87)	(272,932.87)
10904	Interest Earned	Other General Purpose Funding(O)	75.71	(915.35)	(2,196.88)	(915.35)	(2,196.88)
10904	Transfer From Reserve	Other General Purpose Funding(O)	0.00	0.00	0.00	0.00	0.00
15059	Transfer From Reserve	Sculpture By The Bay (C)	272,857.16	0.00	0.00	273,848.22	275,129.75
			0.00	0.00	12,000.00	0.00	0.00
<i>Public Art Reserve Closing Balance</i>			<b>0.00</b>	(273,848.22)	(263,129.75)	0.00	0.00
<b>1057 - Railway House Building Reserve (50%)</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(89,584.83)	(89,584.83)	(89,584.83)	(89,584.83)	(89,584.83)
10904	Interest Earned	Other General Purpose Funding(O)	709.70	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	88,875.13	89,584.83	89,584.83	89,584.83	89,584.83
<i>Railway House Building Reserve (50%) Closing Balance</i>			<b>0.00</b>	0.00	0.00	0.00	0.00
<b>1058 - Road Asset Renewal Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(2,164,710.31)	(2,164,710.31)	(2,164,710.31)	(2,164,710.31)	(2,164,710.31)
10904	Interest Earned	Other General Purpose Funding(O)	(95,196.30)	(34,409.55)	(82,582.80)	(34,409.55)	(82,582.80)
10027	Transfer To Reserve	Asset Management Administration(O)	(1,537,029.15)	(1,537,029.15)	(3,688,870.00)	(1,537,029.15)	(3,688,870.00)
10904	Transfer To Reserve	Other General Purpose Funding(O)	(1,562,215.91)	(1,574,756.00)	(1,574,756.00)	(1,574,756.00)	(1,574,756.00)
11193	Transfer From Reserve	Wilyabrup Road Resheet(C)	0.00	0.00	80,643.00	0.00	80,643.00
11199	Transfer From Reserve	Bussell Highway(C)	150,000.00	0.00	678,321.00	0.00	678,321.00
11987	Transfer From Reserve	Caves Road - Median Crossing(C)	0.00	0.00	17,043.00	0.00	17,043.00
12005	Transfer From Reserve	Payne Road(C)	150,000.00	0.00	638,055.00	0.00	638,055.00
12611	Transfer From Reserve	North Jindong Road(C)	0.00	0.00	292,600.00	0.00	292,600.00
12827	Transfer From Reserve	Commonage Road(C)	0.00	0.00	269,500.00	0.00	269,500.00
12842	Transfer From Reserve	Nuttman Road (C)	0.00	0.00	54,724.00	0.00	54,724.00
12845	Transfer From Reserve	General Design / Modelling Works (O)	0.00	0.00	312,000.00	0.00	312,000.00
14817	Transfer From Reserve	Road Access Seal - Busselton Animal care facility(C)	0.00	0.00	49,766.00	0.00	49,766.00
14850	Transfer From Reserve	Cook Street - Asphalt Overlay SLK 0.39 to 1.25 (C)	0.00	0.00	351,573.55	0.00	291,573.55
14851	Transfer From Reserve	Strelly Street - Rehabilitation SLK 0.05 to 1.2 (C)	0.00	0.00	240,000.00	0.00	240,000.00
14852	Transfer From Reserve	North Jindong Road - Prelims and Land Acq SLK 0 to 0 (C)	0.00	0.00	12,000.00	0.00	12,000.00
14854	Transfer From Reserve	Boallia Road - Recon and Widening SLK 1.5 to 2.3 (C)	150,000.00	0.00	553,832.39	0.00	553,832.39
14855	Transfer From Reserve	Vasse Yallingup Siding Road - Design only SLK 0 to 0 (C)	0.00	0.00	16,500.00	0.00	16,500.00
14856	Transfer From Reserve	Peron Avenue - Asphalt Overlay SLK 0.17 to 0.76 (C)	0.00	0.00	450,030.00	0.00	450,030.00
14857	Transfer From Reserve	West Street - Rehabilitation SLK 1.02 to 1.15 (C)	0.00	0.00	181,000.00	0.00	0.00
14858	Transfer From Reserve	Artisan Street - Asphalt Overlay SLK 0.02 to 0.16 (C)	0.00	0.00	46,151.72	0.00	36,973.72
14859	Transfer From Reserve	Kershaw Street - Asphalt Overlay SLK 0 to 0.25 (C)	0.00	0.00	69,270.94	0.00	50,445.94
14860	Transfer From Reserve	Thornbill Court - Asphalt Overlay SLK 0 to 0.08 (C)	0.00	0.00	20,662.00	0.00	20,662.00
14862	Transfer From Reserve	Baudin Street - Asphalt Overlay SLK 0 to 0.1 (C)	0.00	0.00	95,132.00	0.00	95,132.00
14863	Transfer From Reserve	Cormorant Court - Asphalt Overlay SLK 0 to 0.08 (C)	0.00	0.00	23,616.00	0.00	23,616.00
14864	Transfer From Reserve	Sugarloaf Road - Second Coat Seal SLK 0 to 0 (C)	0.00	0.00	66,521.00	0.00	66,521.00
14865	Transfer From Reserve	Yallingup Beach Road - Reseal SLK 0 to 0.19 (C)	0.00	0.00	30,698.22	0.00	30,698.22
14866	Transfer From Reserve	Old Vasse Highway - Reseal SLK 0 to 1.83 (C)	0.00	0.00	13,535.16	0.00	13,535.16
15085	Transfer From Reserve	Jalbarragup Road New Shoulders (C)	0.00	0.00	200,000.00	0.00	200,000.00
15103	Transfer From Reserve	Prince Street Laneway(C)	0.00	0.00	0.00	0.00	28,003.00
<i>Road Asset Renewal Reserve Closing Balance</i>			<b>(4,759,151.67)</b>	(5,310,905.01)	(2,125,501.13)	(5,310,905.01)	(2,185,501.13)
<b>1059 - Sick Pay Incentive Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(79,316.37)	(79,316.37)	(79,316.37)	(79,316.37)	(79,316.37)
10904	Interest Earned	Other General Purpose Funding(O)	(1,469.78)	(1,250.00)	(3,000.02)	(1,250.00)	(3,000.02)
			0.00	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
<i>Sick Pay Incentive Reserve Closing Balance</i>			<b>(80,786.15)</b>	(80,566.37)	(82,316.39)	(80,566.37)	(82,316.39)

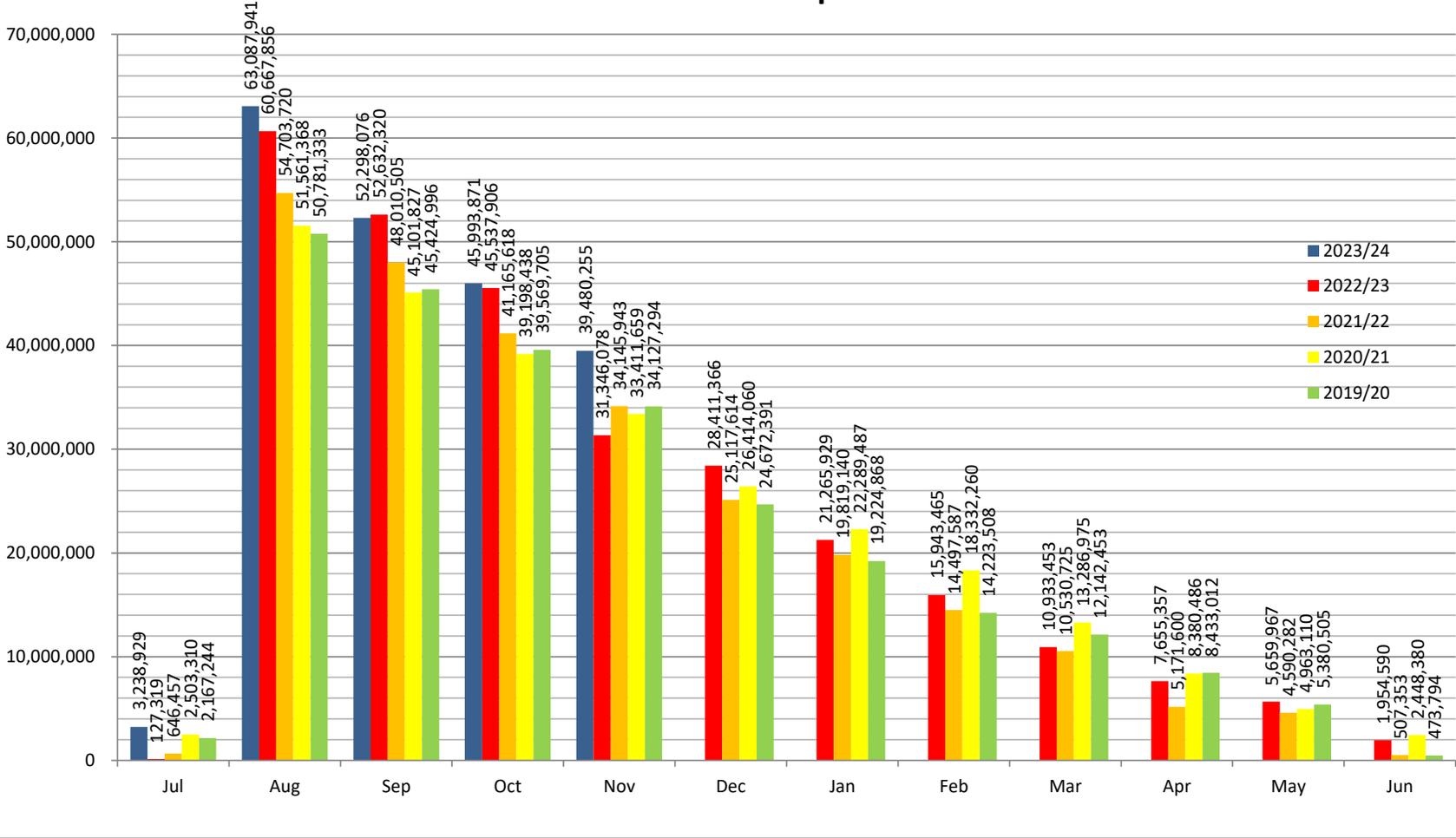
**City of Busseton Reserve Movements**  
**For Period Ended 30 November 2023**

Project Number	Transaction Type	Project Description	YTD Actual	YTD Budget	Budget Original	YTD BudgetA	Budget Amended
<b>1060 - Strategic Projects Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	<b>(3,032,416.03)</b>	(3,032,416.03)	(3,032,416.03)	(3,032,416.03)	(3,032,416.03)
10830	Transfer To Reserve	Members of Council(O)	<b>(10,416.65)</b>	(10,416.65)	(25,000.00)	(10,416.65)	(25,000.00)
12847	Transfer From Reserve	Purchase Sues Road(C)	<b>0.00</b>	0.00	500,000.00	0.00	500,000.00
<i>Strategic Projects Reserve Closing Balance</i>			<b>(3,100,091.08)</b>	(3,090,798.68)	(2,672,534.47)	(3,090,798.68)	(2,672,534.47)
<b>1061 - Vasse Newtown Landscape Maintenance Reserve (SAR)</b>							
10904	Opening Balance	Other General Purpose Funding(O)	<b>(615,196.44)</b>	(615,196.44)	(615,196.44)	(615,196.44)	(615,196.44)
10969	Transfer To Reserve	Vasse SAR Area General Improvements to the Area(C)	<b>(84,190.80)</b>	(84,190.80)	(202,058.00)	(84,190.80)	(202,058.00)
10286	Transfer From Reserve	Southern Wetland - Bush Area Lot 2007 (Newtown)(O)	<b>0.00</b>	0.00	3,353.00	0.00	3,353.00
10317	Transfer From Reserve	Southern Wetland - Heritage Park Lot 2013 (Newtown)(O)	<b>0.00</b>	0.00	4,836.00	0.00	4,836.00
10372	Transfer From Reserve	Northern POS (Newtown)(O)	<b>0.00</b>	0.00	24,318.00	0.00	24,318.00
10428	Transfer From Reserve	Streetscape Medians (Newtown)(O)	<b>0.00</b>	0.00	25,292.00	0.00	25,292.00
10969	Transfer From Reserve	Vasse SAR Area General Improvements to the Area(C)	<b>0.00</b>	0.00	70,000.00	0.00	70,000.00
11541	Transfer From Reserve	East Central (Wakeham Circle Newtown)(O)	<b>0.00</b>	0.00	12,645.00	0.00	12,645.00
11809	Transfer From Reserve	West Central (Elijah Circle Newtown)(O)	<b>0.00</b>	0.00	26,342.00	0.00	26,342.00
<i>Vasse Newtown Landscape Maintenance Reserve (SAR) Closing Balance</i>			<b>(711,005.75)</b>	(709,612.64)	(675,009.43)	(709,612.64)	(675,009.43)
<b>1062 - Vasse Sports Pavilion Building Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	<b>(2,405.91)</b>	(2,405.91)	(2,405.91)	(2,405.91)	(2,405.91)
10904	Interest Earned	Other General Purpose Funding(O)	<b>19.04</b>	0.00	0.00	0.00	0.00
			<b>0.00</b>	0.00	0.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	<b>2,386.87</b>	2,405.91	2,405.91	2,405.91	2,405.91
<i>Vasse Sports Pavilion Building Reserve Closing Balance</i>			<b>0.00</b>	0.00	0.00	0.00	0.00
<b>1063 - Waste Facilities and Plant Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	<b>(8,135,573.66)</b>	(8,135,573.66)	(8,135,573.66)	(8,135,573.66)	(8,135,573.66)
10904	Interest Earned	Other General Purpose Funding(O)	<b>(102,666.49)</b>	(85,773.40)	(205,856.16)	(85,773.40)	(205,856.16)
12419	Transfer To Reserve	Rubbish Sites Development(O)	<b>(731,297.10)</b>	(731,297.10)	(1,755,113.00)	(731,297.10)	(1,755,113.00)
10688	Transfer From Reserve	Depot Washdown Facility Upgrades(C)	<b>0.00</b>	0.00	76,700.00	0.00	76,700.00
12225	Transfer From Reserve	Waste and Fleet Management (PWOH)(O)	<b>0.00</b>	0.00	578,501.00	0.00	578,501.00
12421	Transfer From Reserve	City Lined Landfill Stage 2 - Preliminary Works(C)	<b>0.00</b>	0.00	80,000.00	0.00	80,000.00
12425	Transfer From Reserve	Busseton Landfill Post-closure Capping, Rehab & Remediat(C)	<b>4,244,250.00</b>	0.00	2,000,000.00	0.00	2,000,000.00
12428	Transfer From Reserve	Re-use shop Busseton Transfer Station(C)	<b>0.00</b>	0.00	125,000.00	0.00	125,000.00
12911	Transfer From Reserve	VOLVO FE320 WASTE TRUCK (C)	<b>0.00</b>	0.00	424,660.00	0.00	424,660.00
12914	Transfer From Reserve	Minor Plant - Program 10 (C)	<b>0.00</b>	0.00	10,000.00	0.00	10,000.00
13654	Transfer From Reserve	CATERPILLAR LANDFILL COMPACTOR REFURBISHED (DUNSB. TIP)(P)	<b>0.00</b>	0.00	130,000.00	0.00	130,000.00
14960	Transfer From Reserve	VOLVO FE320 WASTE TRUCK (C)	<b>0.00</b>	0.00	480,000.00	0.00	480,000.00
14962	Transfer From Reserve	WATER TANK FOR DUST SUPPRESSION (C)	<b>0.00</b>	0.00	25,000.00	0.00	25,000.00
14966	Transfer From Reserve	DIESEL TANK DWF (C)	<b>0.00</b>	0.00	50,000.00	0.00	50,000.00
14968	Transfer From Reserve	HOOKLIFT BIN BWTS (C)	<b>0.00</b>	0.00	18,000.00	0.00	18,000.00
14970	Transfer From Reserve	HOOKLIFT BIN BWTS (C)	<b>0.00</b>	0.00	18,000.00	0.00	18,000.00
14972	Transfer From Reserve	HOOKLIFT BIN BWTS (C)	<b>0.00</b>	0.00	18,000.00	0.00	18,000.00
14974	Transfer From Reserve	HOOKLIFT BIN (C)	<b>0.00</b>	0.00	18,000.00	0.00	18,000.00
14978	Transfer From Reserve	ABLE GL3013 GENERATOR (C)	<b>0.00</b>	0.00	50,000.00	0.00	50,000.00
14980	Transfer From Reserve	WASTE COORDINATOR VEHICLE (C)	<b>0.00</b>	0.00	35,000.00	0.00	35,000.00
14982	Transfer From Reserve	TOYOTA HILUX 4x2 SINGLE CAB (C)	<b>0.00</b>	0.00	26,000.00	0.00	26,000.00
14984	Transfer From Reserve	ISUZU D-MAX 4x4 DUAL CAB (C)	<b>0.00</b>	0.00	30,000.00	0.00	30,000.00
14986	Transfer From Reserve	HOLDEN COLORADO 4x4 DUAL CAB (C)	<b>0.00</b>	0.00	26,000.00	0.00	26,000.00
15066	Transfer From Reserve	Dunsborough Groundwater Investigation (O)	<b>0.00</b>	0.00	200,000.00	0.00	200,000.00
15077	Transfer From Reserve	Lunch / Meeting Room for Transfer Station(C)	<b>0.00</b>	0.00	50,000.00	0.00	50,000.00
<i>Waste Facilities and Plant Reserve Closing Balance</i>			<b>(4,725,287.25)</b>	(8,952,644.16)	(5,062,636.82)	(8,952,644.16)	(5,062,636.82)
<b>1064 - Winderlup Aged Housing Reserve (City Controlled)</b>							
10904	Opening Balance	Other General Purpose Funding(O)	<b>(463,207.57)</b>	(463,207.57)	(463,207.57)	(463,207.57)	(463,207.57)
10904	Interest Earned	Other General Purpose Funding(O)	<b>(9,012.06)</b>	(7,225.10)	(17,340.25)	(7,225.10)	(17,340.25)
11684	Transfer To Reserve	Winderlup Court Aged Housing(O)	<b>0.00</b>	0.00	(85,086.00)	0.00	(85,086.00)
<i>Winderlup Aged Housing Reserve (City Controlled) Closing Balance</i>			<b>(472,219.63)</b>	(470,432.67)	(516,633.82)	(470,432.67)	(516,633.82)
<b>1065 - Workers Compensation and Extended Sick Leave</b>							
10904	Opening Balance	Other General Purpose Funding(O)	<b>(4,281.48)</b>	(4,281.48)	(4,281.48)	(4,281.48)	(4,281.48)
10904	Interest Earned	Other General Purpose Funding(O)	<b>(80.41)</b>	(110.20)	(264.50)	(110.20)	(264.50)
			<b>0.00</b>	0.00	0.00	0.00	0.00
			<b>0.00</b>	0.00	0.00	0.00	0.00
<i>Workers Compensation and Extended Sick Leave Closing Balance</i>			<b>(4,361.89)</b>	(4,391.68)	(4,545.98)	(4,391.68)	(4,545.98)
<b>1066 - Youth and Community Activities Building Reserve</b>							
10904	Interest Earned	Other General Purpose Funding(O)	<b>1,720.11</b>	0.00	0.00	0.00	0.00
			<b>0.00</b>	0.00	0.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	<b>214,931.45</b>	216,651.56	216,651.56	216,651.56	216,651.56
<i>Youth and Community Activities Building Reserve Closing Balance</i>			<b>0.00</b>	0.00	0.00	0.00	0.00
<b>1067 - Prepaid Grants and Deferred Works &amp; Services Reserves</b>							
10904	Interest Earned	Other General Purpose Funding(O)	<b>(6,900.51)</b>	0.00	0.00	0.00	0.00
10904	Transfer To Reserve	Other General Purpose Funding(O)	<b>(351,807.98)</b>	(351,807.98)	(351,807.98)	(351,807.98)	(351,807.98)
10613	Transfer From Reserve	Meelup Coastal Parking & Landscaping(C)	<b>0.00</b>	0.00	178,237.98	0.00	178,237.98
10653	Transfer From Reserve	Community Assistance Program (Governance)(O)	<b>0.00</b>	0.00	50,000.00	0.00	50,000.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	<b>2,843,438.14</b>	2,843,444.58	2,843,444.58	2,843,444.58	2,843,444.58
11040	Transfer From Reserve	Economic and Business Development(O)	<b>0.00</b>	0.00	20,000.00	0.00	20,000.00
11128	Transfer From Reserve	Community and Recreation Management(O)	<b>0.00</b>	0.00	31,235.00	0.00	31,235.00
12648	Transfer From Reserve	Rural-Tree Pruning(O)	<b>0.00</b>	0.00	72,335.00	0.00	72,335.00
<i>Prepaid Grants and Deferred Works &amp; Services Reserves Closing Balance</i>			<b>(358,714.93)</b>	(351,807.98)	0.00	(351,807.98)	0.00

**City of Busseton Reserve Movements**  
**For Period Ended 30 November 2023**

Project Number	Transaction Type	Project Description	YTD Actual	YTD Budget	Budget Original	YTD BudgetA	Budget Amended
<b>1068 - Airport Development Reserve</b>							
10904	Interest Earned	Other General Purpose Funding(O)	0.00	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
<i>Airport Development Reserve Closing Balance</i>			0.00	0.00	0.00	0.00	0.00
<b>1069 - Airport Infrastructure Renewal and Replacement Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(3,210,167.16)	(3,210,167.16)	(3,210,167.16)	(3,210,167.16)	(3,210,167.16)
10904	Interest Earned	Other General Purpose Funding(O)	(256,242.95)	(110,889.30)	(266,134.43)	(110,889.30)	(266,134.43)
10594	Transfer To Reserve	Airport Operations(O)	(7,115,405.38)	(7,307,111.70)	(8,924,455.50)	(7,307,111.70)	(8,924,455.50)
10583	Transfer From Reserve	Airport Construction Stage 2, Airfield(C)	0.00	0.00	22,478.00	0.00	22,478.00
10589	Transfer From Reserve	Airport Construction, Existing Terminal Upgrade(C)	0.00	0.00	103,847.00	0.00	103,847.00
10594	Transfer From Reserve	Airport Operations(O)	0.00	0.00	2,500,000.00	0.00	2,500,000.00
12804	Transfer From Reserve	Airport Terminal Building(C)	0.00	0.00	28,481.00	0.00	28,481.00
12932	Transfer From Reserve	Airport Operations - Replacement Toro Groundmaster 7200 M(C)	0.00	0.00	22,000.00	0.00	22,000.00
12941	Transfer From Reserve	Airport - Other Buildings(C)	0.00	0.00	5,000.00	0.00	5,000.00
14836	Transfer From Reserve	RFDS relocation(C)	0.00	0.00	177,515.00	0.00	177,515.00
14838	Transfer From Reserve	BMRAP - Apron Lighting(C)	0.00	0.00	65,000.00	0.00	65,000.00
14946	Transfer From Reserve	PERUZZO BULL SLASHER (C)	0.00	0.00	57,000.00	0.00	57,000.00
14949	Transfer From Reserve	KUBOTA MULE (C)	0.00	0.00	10,000.00	0.00	10,000.00
14951	Transfer From Reserve	1500L SELF BUNDED DIESEL FUEL TANK (C)	0.00	0.00	14,500.00	0.00	14,500.00
15060	Transfer From Reserve	BMRA MAGS Improvements (C)	0.00	0.00	17,000.00	0.00	17,000.00
15061	Transfer From Reserve	BMRA Gate 1 Replacement (C)	0.00	0.00	10,000.00	0.00	10,000.00
15062	Transfer From Reserve	BMRA GA Precinct Reseal (C)	0.00	0.00	218,810.00	0.00	218,810.00
15083	Transfer From Reserve	New Airport Manager Vehicle (C)	0.00	0.00	40,000.00	0.00	40,000.00
<i>Airport Infrastructure Renewal and Replacement Reserve Closing Balance</i>			(10,581,815.49)	(10,628,168.16)	(8,688,336.09)	(10,628,168.16)	(8,688,336.09)
<b>1070 - Airport Noise Mitigation Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(661,139.59)	(661,139.59)	(661,139.59)	(661,139.59)	(661,139.59)
10904	Interest Earned	Other General Purpose Funding(O)	5,364.50	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
<i>Airport Noise Mitigation Reserve Closing Balance</i>			0.00	0.00	0.00	0.00	0.00
<b>1071 - LED Street Lighting Replacement Program Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(32,227.51)	(32,227.51)	(32,227.51)	(32,227.51)	(32,227.51)
10904	Interest Earned	Other General Purpose Funding(O)	227.10	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	32,000.41	32,227.51	32,227.51	32,227.51	32,227.51
<i>LED Street Lighting Replacement Program Reserve Closing Balance</i>			0.00	0.00	0.00	0.00	0.00
<b>1072 - Lou Weston Oval Pavilion Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(12,729.59)	(12,729.59)	(12,729.59)	(12,729.59)	(12,729.59)
10904	Interest Earned	Other General Purpose Funding(O)	94.67	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	12,634.92	12,729.59	12,729.59	12,729.59	12,729.59
<i>Lou Weston Oval Pavilion Reserve Closing Balance</i>			0.00	0.00	0.00	0.00	0.00
<b>1073 - Waterways Restoration Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(10.00)	(10.00)	(10.00)	(10.00)	(10.00)
10904	Interest Earned	Other General Purpose Funding(O)	(5,344.26)	(2,529.60)	(6,071.00)	(2,529.60)	(6,071.00)
10904	Transfer To Reserve	Other General Purpose Funding(O)	(133,468.93)	(134,559.00)	(134,559.00)	(134,559.00)	(134,559.00)
12848	Transfer To Reserve	Vasse River - Ongoing Restoration of River Habitat(O)	(238,530.50)	(238,530.50)	(572,473.20)	(238,530.50)	(572,473.20)
<i>Waterways Restoration Reserve Closing Balance</i>			(377,353.69)	(375,629.10)	(140,640.20)	(375,629.10)	(140,640.20)
<b>1078 - Post Office Tea Rooms Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(48,074.95)	(48,074.95)	(48,074.95)	(48,074.95)	(48,074.95)
10904	Interest Earned	Other General Purpose Funding(O)	(1,424.20)	(38.35)	(92.00)	(38.35)	(92.00)
10020	Transfer To Reserve	Art Geo Administration(O)	(7,589.60)	(7,589.60)	(18,215.00)	(7,589.60)	(18,215.00)
			0.00	0.00	0.00	0.00	0.00
<i>Post Office Tea Rooms Reserve Closing Balance</i>			(57,088.75)	(55,702.90)	(66,381.95)	(55,702.90)	(66,381.95)
<b>1079 - Peel Tce/Causeway Rd Building Reserve</b>							
10904	Opening Balance	Other General Purpose Funding(O)	(23,429.00)	(23,429.00)	(23,429.00)	(23,429.00)	(23,429.00)
			0.00	0.00	0.00	0.00	0.00
			0.00	0.00	0.00	0.00	0.00
10904	Transfer From Reserve	Other General Purpose Funding(O)	23,429.00	23,429.00	23,429.00	23,429.00	23,429.00
<i>Peel Tce/Causeway Rd Building Reserve Closing Balance</i>			0.00	0.00	0.00	0.00	0.00
<i>Grand Total</i>			(74,718,056.86)	(87,957,466.96)	(64,748,658.71)	(79,287,489.00)	(57,069,483.97)
	Opening Balance		(78,138,181.57)	(78,138,181.57)	(78,138,181.57)	(78,138,181.57)	(78,138,181.57)
	Interest Earned		(1,359,434.40)	(1,062,704.80)	(2,550,491.70)	(1,062,704.80)	(2,550,491.70)
	Transfer To Reserve		(22,334,307.68)	(22,726,880.02)	(36,452,245.68)	(22,726,880.02)	(36,452,245.68)
	Transfer From Reserve		27,113,866.79	13,970,299.43	52,392,260.24	22,640,277.39	60,071,434.98
<i>Grand Total</i>			(74,718,056.86)	(87,957,466.96)	(64,748,658.71)	(79,287,489.00)	(57,069,483.97)

### City of Busselton Net Current Position Year on Year Comparative





**CITY OF BUSSETTON - INVESTMENT PERFORMANCE REPORT**  
**For the month of November 2023**



11am Bank Account As at 30 November 2023

INSTITUTION	RATE	AMOUNT
ANZ 11am At Call Deposit	4.25%	\$ 7,500,000

Term Deposits - Miscellaneous Funds As at 30 November 2023

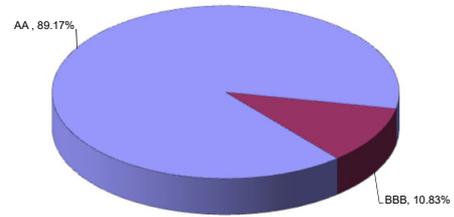
INSTITUTION	RATING	DAYS	MATURITY	RATE	AMOUNT
ANZ	AA	306	16-Feb-24	4.13%	\$ 3,500,000
WBC	AA	184	22-Jan-24	5.36%	\$ 1,500,000
WBC	AA	335	17-May-24	5.27%	\$ 1,500,000
Bendigo	BBB	366	22-Mar-24	4.50%	\$ 3,000,000
WBC	AA	245	06-Jun-24	5.07%	\$ 5,000,000
NAB	AA	301	15-Apr-24	5.45%	\$ 4,000,000
NAB	AA	179	22-Jan-24	5.45%	\$ 2,000,000
WBC	AA	244	24-Apr-24	4.91%	\$ 2,000,000
NAB	AA	182	12-Feb-24	5.15%	\$ 4,000,000
ANZ	AA	306	25-Jan-24	4.17%	\$ 3,000,000
ANZ	AA	305	25-Mar-24	4.56%	\$ 2,000,000
ANZ	AA	337	26-Feb-24	4.16%	\$ 4,000,000
WBC	AA	184	10-Jan-24	5.26%	\$ 4,000,000
NAB	AA	240	30-May-24	5.15%	\$ 3,000,000
NAB	AA	207	22-Dec-23	4.90%	\$ 3,000,000
NAB	AA	270	22-Apr-24	5.40%	\$ 4,000,000
ANZ	AA	244	10-Apr-24	4.95%	\$ 2,000,000
ANZ	AA	244	10-Mar-24	5.19%	\$ 3,000,000
CBA	AA	300	22-Feb-24	4.43%	\$ 5,000,000
BoQ/MeBank	BBB	365	23-Apr-24	4.75%	\$ 2,000,000
BoQ	BBB	366	02-May-24	4.55%	\$ 2,000,000
BoQ	BBB	366	30-May-24	4.95%	\$ 2,000,000
ANZ	AA	275	29-Apr-24	5.18%	\$ 4,000,000
CBA	AA	365	27-Jun-24	5.50%	\$ 3,000,000
WBC	AA	335	20-Mar-24	4.50%	\$ 4,000,000
WBC	AA	184	20-Nov-23	5.20%	\$ 4,000,000
WBC	AA	183	20-Dec-23	5.26%	\$ 4,000,000
CBA	AA	365	19-Jun-24	5.58%	\$ 5,000,000
CBA	AA	366	22-May-24	4.97%	\$ 5,000,000
NAB	AA	210	06-Jun-24	5.25%	\$ 2,500,000
NAB	AA	242	08-Jul-24	5.25%	\$ 3,000,000
BoQ	BBB	274	09-Aug-24	5.35%	\$ 4,000,000
CBA	AA	270	05-Aug-24	5.32%	\$ 5,000,000
CBA	AA	365	08-Nov-24	5.36%	\$ 6,000,000
WBC	AA	366	10-Nov-24	5.35%	\$ 5,000,000

Total of Term Deposits **120,000,000.00**

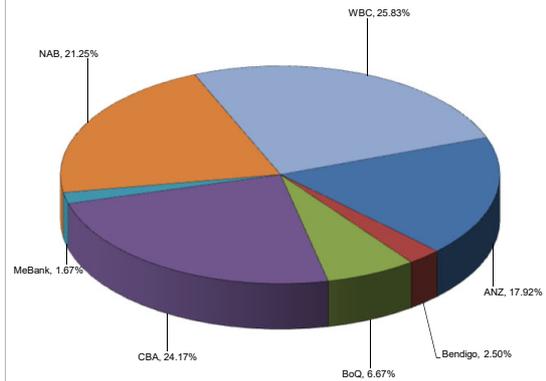
Weighted Average Annual Rate of Return **5.04%**

Investment Graphs

Summary of Term Deposits by S & P Rating (Excludes WATC and 11am Cash Account Funds)



Summary of Term Deposits by institution (Excludes WATC and 11am Cash Account Funds)



Airport Redevelopment Funds As at 30 November 2023

WA Treasury Corp. - Overnight Cash Deposit Facility	4.30%	\$ 625,343
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Total of Airport Redevelopment Funds - WATC \$ 625,343

Nil

Total of Airport Redevelopment Funds - Bank Term Deposits \$0

ANZ Cash Account	AA	NA	NA	4.25%	\$ 195,368
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Total of Airport Redevelopment Funds - Other \$ 195,368

Total of Airport Redevelopment Funds \$ 820,711

(Note: Funds held with the WATC are in accordance with the Airport Redevelopment Funding Contract and the Foreshore Development Contract and are not held within the requirements of the City's Investment Policy 218)

SUMMARY OF ALL INVESTMENTS HELD As at 1 year ago As at 30 June 2023 As at 30 November 2023

11am Bank Account	\$ 7,000,000	\$ 16,500,000	\$ 7,500,000
11am Bank Account Interest - not yet fully processed	\$ -	\$ -	\$ -
11am bank account - deposit not fully processed	\$ -	\$ -	\$ -
Term Deposits - Misc. Funds	\$ 103,500,000	\$ 94,500,000	\$ 120,000,000
Term Deposit Interest - not yet fully processed	\$ -	\$ -	\$ -
Airport Redevelopment - WATC Deposits	\$ 1,651,703	\$ 614,859	\$ 625,343
Airport Redevelopment - ANZ Cash A/c	\$ 784,422	\$ 191,770	\$ 195,368
Total of all Investments Held	\$ 112,936,125	\$ 111,806,629	\$ 128,320,711

TOTAL INTEREST RECEIVED AND ACCRUED \$ 884,602 \$ 3,625,530 \$ 2,361,995

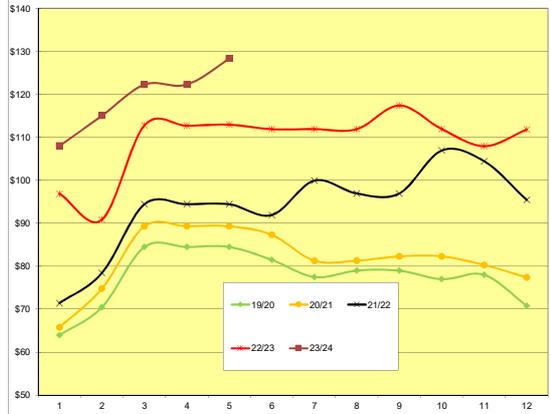
INTEREST BUDGET \$ 688,989 \$ 1,610,000 \$ 1,837,992

(Note: Interest figures relate to City general funds only and does not include interest allocated to specific areas such as the Airport Redevelopment)

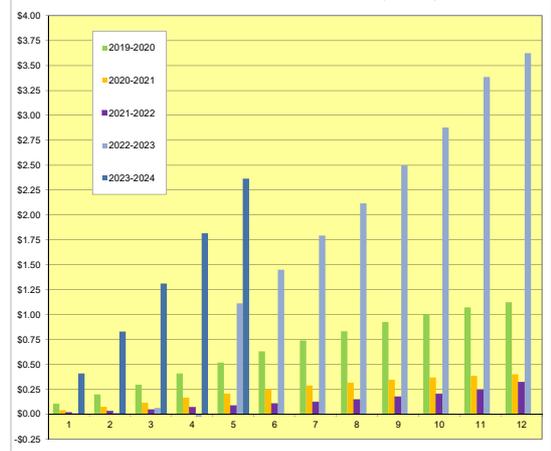
Statement of Compliance with Council's Investment Policy 218

1. All funds are to be invested within legislative limits. **Fully Compliant**
2. All individual funds held within the portfolio are not to exceed a set percentage of the total portfolio value. **Fully Compliant**
3. The amount invested based upon the Fund's Rating is not to exceed the set percentages of the total portfolio. **Fully Compliant**
4. The amount invested based upon the Investment Horizon is not to exceed the set percentages of the total portfolio. **Fully Compliant**

Balance of Investments (\$millions)



Interest Earnt on Investments (\$millions)



**City of Busselton**  
**Loan Schedule - as at 30th November 2023**

Purpose	Loan Number	Institution	Original	Budgeted	Term (Years)	Expiry	Actual	2023/24	2023/24 Actual	2023/24	2023/24 Actual	Budget	2023/24 Budget	2023/24 Budget	Budget	2023/24 Budget
			Borrowed Amount	Interest Rate			Interest Rate	Interest Rate	Balance of Principal Owing	Interest Repayments	Principal 1 July 2023	New Loans		Principal Repayments	Principal Outstanding 30 June 2024	Interest Repayments
			\$	%			%	\$	\$	\$	\$	\$	\$	\$	\$	\$
<b><u>Council Loans</u></b>																
<b>Governance</b>																
Civic and Administration Centre	207	WATC	18,000,000	4.51	20	Jun-34	4.51		208,344	11,576,293	132,872	11,784,637		847,578	10,937,059	517,286
<b>Recreation and Culture</b>																
Busselton Foreshore	204	WATC	1,100,000	4.36	15	Jun-29	4.36	19,407	509,697	5,767	529,104		78,907	450,197	21,790	
GLC Extensions	205	WATC	1,200,000	3.92	10	Jun-24	3.92	35,142	107,506	1,398	142,648		142,648	-	3,512	
Busselton Foreshore	209	WATC	6,500,000	3.45	12	Jun-27	3.45	145,590	2,345,983	22,175	2,491,572		590,179	1,901,393	80,879	
Busselton Foreshore	211	WATC	3,000,000	2.55	8	Oct-24	2.55	200,731	409,197	7,139	609,928		404,029	205,899	11,710	
Busselton Foreshore Jetty Precinct	215	WATC	2,500,000	3.25	10	Apr-28	3.25	125,469	1,225,060	21,946	1,350,529		252,977	1,097,552	41,853	
Tennis Club Facility	216	WATC	2,750,000	3.25	10	Apr-28	3.25	138,016	1,347,566	24,141	1,485,581		278,274	1,207,307	46,039	
Lot 10 Commonage Road	217	WATC	1,600,000	3.25	10	Apr-28	3.25	80,300	784,038	14,046	864,338		161,904	702,434	26,786	
Busselton Tennis Club	218	WATC	1,250,000	2.21	10	Jun-29	2.21	30,687	754,555	4,338	785,242		123,770	661,472	16,333	
BPACC	225	WATC	5,000,000	1.46	10	Dec-31	2.10	116,261	4,198,811	22,643	4,315,071		468,715	3,846,356	86,900	
BPACC	226	WATC	5,000,000	2.02	15	Dec-36	2.39	72,083	4,504,304	27,292	4,576,387		290,922	4,285,465	106,580	
BPACC	228	WATC	5,000,000	3.86	20	Jun-42	3.86	43,375	4,787,232	46,602	4,830,607		176,025	4,654,582	183,881	
BPACC	229	WATC	5,000,000	3.77	17	Jun-39	3.77	54,830	4,730,919	45,079	4,785,749		222,438	4,563,311	177,199	
BPACC	230	WATC	6,500,000	3.25	20	Dec-42	4.42	52,093	6,345,423	70,752	6,397,516		211,856	6,185,660	279,526	
<b>Transport</b>																
Airport Jet A1 Installation	206	WATC	350,000	3.92	10	Jun-24	3.92		10,250	31,356	408	41,606		41,606	-	1,024
Airport Freight Hub Stage 1	219	WATC	1,480,000	2.21	10	Jun-29	2.21	36,334	893,393	5,137	929,726		146,544	783,182	19,339	
Strategic Land Purchase	New	Unknown		4.29	10	New					-	1,750,000	106,946	1,643,054	55,137	
<b>Other Property and Services</b>																
Lot 40 Vasse Highway	210	WATC	850,000	3.61	10	Dec-25	3.61		850,000	7,671	850,000			850,000	30,685	
								-	1,368,911	45,401,332	459,406	46,770,244	1,750,000	4,545,318	43,974,925	1,706,459
<b><u>Self-Supporting Loans</u></b>																
<b>Recreation and Culture</b>																
Busselton Football and Sportsman's Club	208	WATC		2.93	10.25	Apr-25	2.93		800	5,764	48	6,564		3,234	3,330	157
Dunsborough and Districts Country Club	212	WATC		3.04	10	May-27	3.04	5,897	43,865	756	49,761		11,883	37,878	1,423	
Geopraphe Bay Yacht Club	213	WATC		3.04	10	May-27	3.04	5,173	38,477	663	43,650		10,424	33,226	1,248	
Dunsborough and Districts Country Club	214	WATC		3.19	10	Sep-27	3.19	5,609	48,213	858	53,822		11,307	42,515	1,627	
Busselton Tennis Club	220	WATC		1.37	7	Sep-26	1.37	1,792	21,990	81	23,782		7,205	16,577	289	
Busselton Hockey Club Stadium	221	WATC		1.31	10	Jun-30	1.31	1,099	31,060	105	32,158		4,416	27,742	400	
Busselton Golf Club	222	WATC		1.45	10	Jun-31	1.45	2,636	86,621	324	89,256		10,600	78,656	1,238	
Dunsborough Bay Yacht Club	223	WATC		2.77	5	Dec-26	1.57	1,233	16,471	69	17,704		4,960	12,744	248	
Geopraphe Bay Yacht Club	224	WATC		2.77	10	Dec-31	2.42	1,150	42,097	261	43,246		4,639	38,607	1,004	
MRBTA - Ancient Lands Discovery Park	227	WATC		2.77	10	Mar-32	2.77	28,183	1,083,782	7,704	1,111,965		113,910	998,055	29,639	
Community Groups 23/24 \$250K	New	Unknown		3.00	10	New			-	-	-	250,000	10,804	239,196	3,710	
								-	53,569	1,418,340	10,872	1,471,909	250,000	193,382	1,528,527	40,983
<b>Total - Council and Self-supporting Loans</b>								-	1,422,481	46,819,672	470,278	48,242,153	2,000,000	4,738,700	45,503,453	1,747,442



**LISTING OF PAYMENTS MADE  
UNDER DELEGATED AUTHORITY  
FOR THE MONTH OF NOVEMBER 2023**

<b>CHEQUE PAYMENTS</b>	119799 - 119817	44,842.05
<b>ELECTRONIC TRANSFER PAYMENTS</b>	99087 - 99776	8,565,634.79
<b>TRUST ACCOUNT</b>	EFT# TR000015 - TR000017	60,426.14
<b>PAYROLL</b>	01.11.2023 - 30.11.2023	1,772,286.78
<b>INTERNAL PAYMENT VOUCHERS</b>	DD #5500 - 5535	25,595,810.30
		<b>36,039,000.06</b>

CHEQUE PAYMENTS NOVEMBER 2023				
DATE	REF #	NAME	DESCRIPTION	AMOUNT \$
29/11/2023	119815	ARTGEO PETTY CASH	PETTY CASH REIMBURSEMENT	99.20
16/11/2023	119808	BOND ADMINISTRATOR	AGED HOUSING BONDS	240.00
8/11/2023	119801	BUSSELTON PUBLIC LIBRARY	PETTY CASH REIMBURSEMENT	150.75
29/11/2023	119816	BUSSELTON PUBLIC LIBRARY	PETTY CASH REIMBURSEMENT	212.90
16/11/2023	119804	CALLOWS CORNER NEWSAGENCY	STAFF SOCIAL CLUB - LOTTO	45.00
16/11/2023	119802	CITY OF BUSSELTON DEPOT	PETTY CASH RECoup	60.19
16/11/2023	119806	DEPARTMENT OF TRANSPORT	PLANT REGISTRATION	981.00
17/11/2023	119809	DEPARTMENT OF TRANSPORT	PLANT REGISTRATION	100.00
29/11/2023	119814	DEPARTMENT OF TRANSPORT	PLANT REGISTRATION	1,308.00
16/11/2023	119807	DUNSBOROUGH PUBLIC LIBRARY	PETTY CASH REIMBURSEMENT	201.95
16/11/2023	119803	GEOGRAPHE LEISURE CENTRE	PETTY CASH REIMBURSEMENT	283.60
8/11/2023	119800	VOIDED	VOIDED	-
29/11/2023	119810	VOIDED	VOIDED	-
29/11/2023	119811	VOIDED	VOIDED	-
29/11/2023	119812	VOIDED	VOIDED	-
29/11/2023	119813	VOIDED	VOIDED	-
29/11/2023	119817	VOIDED	VOIDED	160.00
8/11/2023	119799	WATER CORPORATION	WATER SERVICES	35,846.08
16/11/2023	119805	WATER CORPORATION	WATER SERVICES	5,153.38
				<b>44,842.05</b>

EFT PAYMENTS NOVEMBER 2023				
DATE	REF #	NAME	DESCRIPTION	AMOUNT \$
29/11/2023	99490	3E CONSULTING ENGINEERS PTY LTD	CONSULTANCY SERVICES	3,520.00
15/11/2023	99162	ABBAY TILING SERVICE	TLING SERVICES	1,193.50
29/11/2023	99509	ACE ANTENNAS	ANTENNA INSTALLATION	297.00
29/11/2023	99534	ACTION KERBING	KERBING SERVICES	5,427.00
15/11/2023	99389	ACTIV FOUNDATION INC	MAINTENANCE SERVICES	2,917.20
29/11/2023	99734	ACTIV FOUNDATION INC	MAINTENANCE SERVICES	57,229.41
29/11/2023	99498	ACURIX NETWORKS	INTERNET WIFI ACCESS	3,996.65
22/11/2023	99449	ADRIAN FISHER	STAFF REIMBURSEMENT	73.00
29/11/2023	99579	ADVAM PTY LTD	AIRPORT CARPARK CREDIT CARD TRANSACTIONS	723.58
29/11/2023	99602	ADVANCED HEARING WA PTY LTD	HEARING ASSESSMENT	105.00
15/11/2023	99201	ADVANCED SEALING TRUST	MAINTENANCE SERVICES	528.00
29/11/2023	99563	ADVANCED SEALING TRUST	MAINTENANCE SERVICES	11,522.80
15/11/2023	99337	AEROCO PTY LTD	AIRPORT SERVICES	2,097.80
29/11/2023	99673	AEROCO PTY LTD	AIRPORT SERVICES	2,022.40
15/11/2023	99212	AERODROME MANAGEMENT SERVICES PTY LTD	AIR SERVICES	34,611.65
15/11/2023	99257	AHOY MANAGEMENT	CABIN FEVER FESTIVAL ACTIVITIES	13,200.00
15/11/2023	99276	AIMEE CARTER	GLC REFUNDS	141.00
29/11/2023	99675	AL FORNO	CATERING	1,587.00
8/11/2023	99094	ALINTA ENERGY	ELECTRICITY	115.90
15/11/2023	99253	ALISON BURTON	ART SALES	471.10
22/11/2023	99471	ALLANNAH STAMMERS	STAFF REIMBURSEMENT	197.39
29/11/2023	99560	ALLFLOW INDUSTRIAL	MAINTENANCE SERVICES	1,309.00
29/11/2023	99546	ALLIANCE SECURITY PERSONNEL	SECURITY SERVICES	847.00
29/11/2023	99766	ALLOY & STAINLESS PRODUCTS PTY LTD	PLANT PURCHASES / SERVICES / PARTS	911.90
29/11/2023	99520	ALPHA PEST ANIMAL SOLUTIONS	FOX BAITING	5,874.00
29/11/2023	99567	ALPINE LAUNDRY PTY LTD	COMMERCIAL LAUNDRY	409.74
15/11/2023	99251	ALTUS GROUP CONSULTING PTY LTD	COST MANAGEMENT SERVICES	4,400.00
15/11/2023	99196	ANA MARISC	STAFF REIMBURSEMENT	74.48
15/11/2023	99280	ANDREW MCCARTHY	BJTP REFUND	57.00
29/11/2023	99624	ANDREW MCCARTHY	BJTP REFUND	57.00
15/11/2023	99248	ANTON BLUME	ART SALES	285.60
22/11/2023	99458	ANTONY COOK	STAFF REIMBURSEMENT	324.30
15/11/2023	99312	AQUAZONE MARINE	MARINE SUPPLIES AND REPAIRS	117.68
15/11/2023	99155	ARBOR GUY	TREE MAINTENANCE SERVICES	33,154.00
29/11/2023	99489	ARBOR GUY	TREE MAINTENANCE SERVICES	109,415.44
15/11/2023	99367	ARROW BRONZE	MEMORIAL PLAQUES SUPPLIER	731.32
15/11/2023	99359	ARROWES ROADING SAFETY PTY LTD	ROADING SAFETY SOLUTIONS	47,399.00
15/11/2023	99295	ASCOT SETTLEMENTS TRUST ACCOUNT	RATE REFUND	271.59
29/11/2023	99692	ASSET AVIATION INSTITUTE	AERODROME REPORTING OFFICER TRAINING	395.00
29/11/2023	99679	ASTRON ENVIRONMENTAL SERVICES PTY LTD	ENVIRONMENTAL SURVEY	47,109.70
15/11/2023	99324	ATTEKUS	ONLINE BOOKING, EVENT SOFTWARE	842.19
15/11/2023	99209	ATTURRA BUSINESS APPLICATIONS	SOFTWARE CONSULTANCY	1,870.00
15/11/2023	99166	AUSQ TRAINING	TRAINING SERVICES	1,896.00
29/11/2023	99503	AUSQ TRAINING	TRAINING SERVICES	1,500.00
15/11/2023	99235	AUSTRAFFIC WA	TRAFFIC DATA COLLECTION SERVICES	3,718.00
8/11/2023	99125	AUSTRALIA POST	POSTAL SERVICE	8,891.75
15/11/2023	99409	AUSTRALIAN INSTITUTE OF MANAGEMENT	MEMBERSHIP	606.00
29/11/2023	99757	AUSTRALIAN INSTITUTE OF MANAGEMENT	MEMBERSHIP	606.00
16/11/2023	99138	AUSTRALIAN TAXATION OFFICE	PAYG TAXATION	260,834.00
30/11/2023	99478	AUSTRALIAN TAXATION OFFICE	PAYG TAXATION	279,415.00
15/11/2023	99307	AUTO ONE TRADING PTY LTD	AUTOMOTIVE	365.15
29/11/2023	99642	AUTO ONE TRADING PTY LTD	AUTOMOTIVE	1,190.45
29/11/2023	99672	AVCRM PRODUCTS PTY LTD	IT SOFTWARE PROVIDER	7,700.00
29/11/2023	99609	AVIATION ID AUSTRALIA PTY LTD	SUPPLY AVIATION ASIC CARDS	280.00
29/11/2023	99515	AZILITY	IT SOFTWARE PROVIDER	1,375.00
15/11/2023	99393	B & B STREET SWEEPING	STREET SWEEPING SERVICE	10,487.99
22/11/2023	99467	B & B STREET SWEEPING	STREET SWEEPING SERVICE	20,818.33
29/11/2023	99739	B & B STREET SWEEPING	STREET SWEEPING SERVICE	22,821.21
15/11/2023	99229	BAKED BUSSELTON	CATERING SERVICES	410.00
15/11/2023	99182	BARRY ALLEN ELECTRICAL SERVICES PTY LTD	ELECTRICAL SERVICES - VARIOUS CITY OF BUSSELTON BUILDINGS	10,089.83
29/11/2023	99535	BARRY ALLEN ELECTRICAL SERVICES PTY LTD	ELECTRICAL SERVICES - VARIOUS CITY OF BUSSELTON BUILDINGS	33,858.09
29/11/2023	99504	BARTCO TRAFFIC EQUIPMENT PTY LTD	TRAFFIC MANAGEMENT EQUIPMENT	2,178.00
29/11/2023	99733	BAY SIGNS	SIGNAGE SERVICES	589.09
15/11/2023	99199	BAYTHE LINEN AND LAUNDRY	SUPPLY OF CLEAN LINEN	107.32
29/11/2023	99561	BAYTHE LINEN AND LAUNDRY	SUPPLY OF CLEAN LINEN	52.05



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<b>CHEQUE PAYMENTS</b>	119799 - 119817	44,842.05
<b>ELECTRONIC TRANSFER PAYMENTS</b>	99087 - 99776	8,565,634.79
<b>TRUST ACCOUNT</b>	EFT# TRO00015 - TRO00017	60,426.14
<b>PAYROLL</b>	01.11.2023 - 30.11.2023	1,772,286.78
<b>INTERNAL PAYMENT VOUCHERS</b>	DD #5500 - 5535	25,595,810.30
		<b>36,039,000.06</b>

29/11/2023	99711	BELL FIRE EQUIPMENT CO	FIRE EQUIPMENT SERVICES	1,045.00
15/11/2023	99398	BENARA NURSERY	NURSERY SUPPLIES	592.46
8/11/2023	99129	BG & E PTY LTD	CONSULTANCY SERVICES	2,286.90
15/11/2023	99414	BG & E PTY LTD	CONSULTANCY SERVICES	4,358.20
29/11/2023	99527	BINDI NUTRITION PTY LTD	ENERGY AND NUTRITION PRODUCTS	329.12
29/11/2023	99645	BING TECHNOLOGIES PTY LTD	MAILING SERVICES	1,675.44
15/11/2023	99271	BIO SOIL SOLUTIONS	LIQUID SOIL SOLUTIONS	2,288.00
29/11/2023	99616	BIO SOIL SOLUTIONS	LIQUID SOIL SOLUTIONS	2,460.00
15/11/2023	99387	BLACKWOODS	FLEET CONSUMABLES & MAINTENANCE PARTS	1,066.48
29/11/2023	99731	BLACKWOODS	FLEET CONSUMABLES & MAINTENANCE PARTS	3,107.68
15/11/2023	99372	BOC GASES AUSTRALIA LTD	GAS SERVICES	1,665.66
29/11/2023	99713	BOC GASES AUSTRALIA LTD	GAS SERVICES	2,005.93
15/11/2023	99273	BOLINDA DIGITAL PTY LTD	LIBRARY RESOURCES	1,650.00
15/11/2023	99245	BOOMERING	BASKETBALL RING SYSTEMS	18,205.00
22/11/2023	99434	BRAD AND TAM FISH	CROSSOVER CONTRIBUTION	416.30
15/11/2023	99279	BRADLEY & KAYLENE GOODSSELL	BT/ REFUND	57.00
22/11/2023	99447	BRENT & NICOLE MCDONALD	CROSSOVER CONTRIBUTION	167.90
8/11/2023	99105	BRETT TITTERTON ELECTRICAL AND AIR CONDITIONING	ELECTRICAL SERVICES	1,263.45
15/11/2023	99261	BRETT TITTERTON ELECTRICAL AND AIR CONDITIONING	ELECTRICAL SERVICES	241.06
29/11/2023	99613	BRETT TITTERTON ELECTRICAL AND AIR CONDITIONING	ELECTRICAL SERVICES	2,347.40
22/11/2023	99431	BRIAN EDWARD INGRAM	CARPENTRY SERVICES	3,500.00
15/11/2023	99160	BRIDGESTONE	TYRE SERVICES	16,199.48
29/11/2023	99496	BRIDGESTONE	TYRE SERVICES	30,948.36
24/11/2023	99472	BROAD CONSTRUCTION PTY LTD	CONSTRUCTION SERVICES	1,409,377.12
15/11/2023	99285	BROOKE GYSEN	BT/ REFUND	50.00
15/11/2023	99376	BUCHER MUNICIPAL PTY LTD	ENGINEERING - PLANT SPARES & SERVICING	252.37
29/11/2023	99644	BUILDING BUSSELTON TOWN TEAM INC	DONATION- MITCHELL PARK OPENING	5,610.00
15/11/2023	99303	BUILDING SUPPLIES AND HIRE	BUILDING SUPPLIES	96.01
29/11/2023	99754	BUNBURY MACHINERY	PLANT PURCHASES / SERVICES / PARTS	969.38
29/11/2023	99494	BUNBURY TELECOM SERVICE PTY LTD	MAINTENANCE SERVICES	893.75
15/11/2023	99270	BUNBURY TRUCKS	PLANT PURCHASES / SERVICES / PARTS	113,014.00
15/11/2023	99396	BUNNINGS BUILDING SUPPLIES	HARDWARE SUPPLIES	2,508.63
29/11/2023	99743	BUNNINGS BUILDING SUPPLIES	HARDWARE SUPPLIES	5,763.32
8/11/2023	99097	BUSSELTON ADVANCED DRIVER TRAINING	DRIVER TRAINING	3,620.00
29/11/2023	99545	BUSSELTON AGRICULTURAL SERVICES (WA) PTY LTD	RURAL SUPPLIES	644.00
15/11/2023	99164	BUSSELTON BEARING SERVICES	BEARING SUPPLIES	1,756.22
29/11/2023	99500	BUSSELTON BEARING SERVICES	BEARING SUPPLIES	2,803.81
22/11/2023	99468	BUSSELTON BITUMEN SERVICE	EARTHMOVING SERVICES	57,297.18
15/11/2023	99395	BUSSELTON BOWLING CLUB INC	COMMUNITY BID	5,111.70
15/11/2023	99304	BUSSELTON BUILDING PRODUCTS	BUILDING PRODUCT SUPPLIER	626.58
29/11/2023	99640	BUSSELTON BUILDING PRODUCTS	BUILDING PRODUCT SUPPLIER	824.23
15/11/2023	99403	BUSSELTON CHAMBER OF COMMERCE AND INDUSTRY	MEMBERSHIP	6,875.00
15/11/2023	99151	BUSSELTON CITY FC	WORKSHOP CATERING	2,640.00
15/11/2023	99148	BUSSELTON HYDRAULIC SERVICES INC	HYDRAULIC SERVICES	148.85
29/11/2023	99486	BUSSELTON HYDRAULIC SERVICES INC	HYDRAULIC SERVICES	2,176.36
15/11/2023	99237	BUSSELTON LOCAL ORGANISING COMMITTEE INC	IRONMAN 2023 - VOLUNTEER PROGRAM CONTRIBUTION	10,000.00
29/11/2023	99728	BUSSELTON MOTORS (MITSUBISHI/MAZDA)	VEHICLE PURCHASES / SERVICES / PARTS	217.81
29/11/2023	99735	BUSSELTON MULTI SERVICE	ENGRAVING SERVICES	426.25
15/11/2023	99327	BUSSELTON PEST & WEED CONTROL	PEST CONTROL	165.00
29/11/2023	99597	BUSSELTON POTTERY GROUP INC	COMMUNITY ASSISTANCE PROGRAM	11,893.50
15/11/2023	99381	BUSSELTON RETRAVISION	ELECTRICAL SERVICES	863.57
8/11/2023	99126	BUSSELTON STATE EMERGENCY SERVICE	PETTY CASH REIMBURSEMENT	464.49
29/11/2023	99538	BUSSELTON STOCKFEEDS & PET SUPPLIES	ANIMAL SUPPLIES	224.00
15/11/2023	99369	BUSSELTON TOYOTA	VEHICLE PURCHASES / SERVICES / PARTS	400.75
29/11/2023	99710	BUSSELTON TOYOTA	VEHICLE PURCHASES / SERVICES / PARTS	466.74
15/11/2023	99370	BUSSELTON WATER	WATER SERVICES	80,336.45
29/11/2023	99712	BUSSELTON WATER	WATER SERVICES	905.05
15/11/2023	99405	BUSSELTON WELDING SERVICES	WELDING SERVICES	5,082.00
29/11/2023	99750	BUSSELTON WELDING SERVICES	WELDING SERVICES	412.50
15/11/2023	99220	C & G NORRIS	INSTALLATION OF SCHEME WATER FOR IRRIGATION	14.25
15/11/2023	99361	CAPE CELLARS BUSSELTON	REFRESHMENTS	1,225.22
29/11/2023	99695	CAPE CELLARS BUSSELTON	REFRESHMENTS	1,728.87
15/11/2023	99419	CAPE RURAL CONTRACTING	FIRE CONTROL SERVICES	3,993.00
29/11/2023	99768	CAPE RURAL CONTRACTING	FIRE CONTROL SERVICES	20,707.50
29/11/2023	99548	CAPE SHADES	SHADE SAILS MAINTENANCE	20,583.00
22/11/2023	99459	CAPEL TRANSPORT	TRANSPORT SERVICES	471.90
29/11/2023	99703	CAPEL TRANSPORT	TRANSPORT SERVICES	94.38
29/11/2023	99646	CAR CARE (WA) BUSSELTON	CAR DETAILING	90.00
8/11/2023	99116	CAROL MULHEARN	STAFF REIMBURSEMENT	214.34
15/11/2023	99256	CAROL MULHEARN - ART SALES	ART SALES	40.00
15/11/2023	99228	CB TRAFFIC SOLUTIONS PTY LTD	TRAFFIC MANAGEMENT SERVICES - MEELUP BEACH RD, PAYNE RD	17,976.43
29/11/2023	99588	CB TRAFFIC SOLUTIONS PTY LTD	TRAFFIC MANAGEMENT SERVICES - BUSSELL HWY, RUABON RD, PAYNE RD	33,921.28
15/11/2023	99411	CBRE (GWSLA) PTY LTD	AIR CONDITIONING SERVICES	682.00
29/11/2023	99759	CBRE (GWSLA) PTY LTD	AIR CONDITIONING SERVICES	398.20
29/11/2023	99744	CEMETERIES & CREMATORIA ASSOCIATION OF WA INC.	MEMBERSHIP	750.00
15/11/2023	99226	CHOICECHEM	BITUMEN REMOVER	6,185.03
29/11/2023	99659	CHRIS GODDEN	LAWN MOWING	450.00
29/11/2023	99688	CHRIS TRANTHAM	GENERAL MAINTENANCE	1,510.00
15/11/2023	99239	CHRISEA DESIGNS	ART SALES	199.50
15/11/2023	99371	CHRISTIAN & CO ELECTRICAL CONTRACTORS	ELECTRICAL SERVICES	101.20
15/11/2023	99260	CHRISTINE CRESSWELL	ART SALES	8.40
8/11/2023	99128	CHRISTINE McGRATH	STAFF REIMBURSEMENT	264.31
29/11/2023	99631	CHRISTOPHER & JANINE PAGE	CROSSOVER CONTRIBUTION	351.50
15/11/2023	99399	CHUBB FIRE & SECURITY PTY LTD	FIRE EQUIPMENT SERVICES	3,497.19
29/11/2023	99746	CHUBB FIRE & SECURITY PTY LTD	FIRE EQUIPMENT SERVICES	2,000.86
16/11/2023	99133	CITY OF BUSSELTON CHRISTMAS CLUB	PAYROLL DEDUCTIONS	2,102.18
30/11/2023	99473	CITY OF BUSSELTON CHRISTMAS CLUB	PAYROLL DEDUCTIONS	2,102.18
16/11/2023	99139	CITY OF BUSSELTON PAYROLL	PAYROLL DEDUCTIONS REALLOCATION	5,108.55
30/11/2023	99479	CITY OF BUSSELTON PAYROLL	PAYROLL DEDUCTIONS REALLOCATION	4,861.15
16/11/2023	99134	CITY OF BUSSELTON STAFF LOTTO	STAFF LOTTO	174.00



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<b>ELECTRONIC TRANSFER PAYMENTS</b>	99087 - 99776	8,565,634.79
<b>TRUST ACCOUNT</b>	EFT# TR000015 - TR000017	60,426.14
<b>PAYROLL</b>	01.11.2023 - 30.11.2023	1,772,286.78
<b>INTERNAL PAYMENT VOUCHERS</b>	DD #5500 - 5535	25,595,810.30
		<b>36,039,000.06</b>

30/11/2023	99474	CITY OF BUSSELTON STAFF LOTTO	STAFF LOTTO	174.00
16/11/2023	99141	CITY OF BUSSELTON-SOCIAL CLUB	SOCIAL CLUB REIMBURSEMENT	202.00
30/11/2023	99481	CITY OF BUSSELTON-SOCIAL CLUB	SOCIAL CLUB REIMBURSEMENT	198.00
29/11/2023	99714	CJD EQUIPMENT PTY LTD	PLANT PURCHASES / SERVICES / PARTS	30,379.64
15/11/2023	99300	CLAIRE KASTELAN	ART SALES	3.15
22/11/2023	99451	CLAIRE SULLY	STAFF REIMBURSEMENT	69.95
29/11/2023	99705	CLAUDIA WILMOT	STAFF REIMBURSEMENT	155.82
15/11/2023	99407	CLEANAWAY	WASTE MANAGEMENT SERVICES	246.40
29/11/2023	99751	CLEANAWAY	WASTE MANAGEMENT SERVICES	117,641.70
29/11/2023	99547	CLEANAWAY CO PTY LTD	CHEMICAL DISPOSAL SERVICES	4,576.82
29/11/2023	99724	CLEANAWAY EQUIPMENT SERVICES	MAINTENANCE PARTS WASHER	1,020.80
29/11/2023	99551	CLEANAWAY SOLID WASTE PTY LTD	WASTE DISPOSAL SERVICES	130.20
15/11/2023	99147	CLEVERPATCH	VACATION CARE SUPPLIES	158.89
15/11/2023	99198	COATES CIVIL CONSULTING P/L	CIVIL ENGINEERING CONSULTING SERVICES	3,696.00
15/11/2023	99412	COCA COLA - AMATIL PTY LTD	GLC KIOSK PURCHASES	1,290.91
29/11/2023	99760	COCA COLA - AMATIL PTY LTD	GLC KIOSK PURCHASES	447.17
8/11/2023	99127	COLES	COUNCIL & STAFF REFRESHMENTS	263.59
		BUSSELTON LIBRARY - CARD #9186	FOOD AND BEVERAGE SUPPLIES	23.69
		GLC - CARD #8369	FOOD AND BEVERAGE SUPPLIES	85.40
		YOUTH SERVICES - CARD #8748	FOOD AND BEVERAGE SUPPLIES	48.60
		YOUTH SERVICES - CARD #8748	FOOD AND BEVERAGE SUPPLIES	11.65
		YOUTH SERVICES - CARD #8748	FOOD AND BEVERAGE SUPPLIES	94.25
15/11/2023	99401	COLES	COUNCIL & STAFF REFRESHMENTS	296.39
		BUSSELTON LIBRARY - CARD #9186	FOOD AND BEVERAGE SUPPLIES	222.10
		YOUTH SERVICES - CARD #8748	FOOD AND BEVERAGE SUPPLIES	74.29
22/11/2023	99469	COLES	COUNCIL & STAFF REFRESHMENTS	261.97
		YOUTH SERVICES - CARD #8748	FOOD AND BEVERAGE SUPPLIES	79.49
		YOUTH SERVICES - CARD #8748	FOOD AND BEVERAGE SUPPLIES	16.50
		YOUTH SERVICES - CARD #8748	FOOD AND BEVERAGE SUPPLIES	30.55
		YOUTH SERVICES - CARD #8748	FOOD AND BEVERAGE SUPPLIES	108.73
		YOUTH SERVICES - CARD #8748	FOOD AND BEVERAGE SUPPLIES	26.70
29/11/2023	99748	COLES	COUNCIL & STAFF REFRESHMENTS	1,281.01
		BUSSELTON LIBRARY - CARD #9186	FOOD AND BEVERAGE SUPPLIES	208.61
		BUSSELTON LIBRARY - CARD #9186	FOOD AND BEVERAGE SUPPLIES	196.25
		BUSSELTON LIBRARY - CARD #9186	FOOD AND BEVERAGE SUPPLIES	24.30
		GLC - CARD #8639	FOOD AND BEVERAGE SUPPLIES	276.43
		GLC - CARD #8639	FOOD AND BEVERAGE SUPPLIES	240.89
		GLC - CARD #8639	FOOD AND BEVERAGE SUPPLIES	242.59
		YOUTH SERVICES - CARD #8748	FOOD AND BEVERAGE SUPPLIES	49.15
		YOUTH SERVICES - CARD #8748	FOOD AND BEVERAGE SUPPLIES	35.79
		YOUTH SERVICES - CARD #8748	FOOD AND BEVERAGE SUPPLIES	7.00
15/11/2023	99176	COLES.COM.AU	COUNCIL & STAFF REFRESHMENTS	956.60
29/11/2023	99526	COLES.COM.AU	COUNCIL & STAFF REFRESHMENTS	602.60
29/11/2023	99621	COLIN & KERRY JOHNSON	BITP REFUND	144.00
22/11/2023	99426	COLIN WOODFORD CARPENTRY & FINE FURNITURE	CARPENTRY SERVICES	2,750.00
15/11/2023	99152	COMBINED TEAM SERVICES	TRAINING SERVICES	340.00
15/11/2023	99332	COMMERCIAL CHRISTMAS SERVICES	CHRISTMAS DECORATIONS	563.20
15/11/2023	99373	COMMERCIAL CLEANING EQUIPMENT	CLEANING EQUIPMENT SUPPLIER	485.96
15/11/2023	99352	COMMUNITY REFUGE INC	VENUE HIRE	500.00
29/11/2023	99690	COMMUNITY REFUGE INC	VENUE HIRE	190.00
15/11/2023	99207	COMPUTER WEST	INFORMATION TECHNOLOGY	1,020.70
8/11/2023	99107	CONFIDENTIAL LISTING	SEE ATTACHED	-
8/11/2023	99108	CONFIDENTIAL LISTING	SEE ATTACHED	-
10/11/2023	99131	CONFIDENTIAL LISTING	SEE ATTACHED	-
10/11/2023	99132	CONFIDENTIAL LISTING	SEE ATTACHED	-
22/11/2023	99441	CONFIDENTIAL LISTING	SEE ATTACHED	-
29/11/2023	99583	CONNECTIVA PTY LTD	INFORMATION TECHNOLOGY	2,420.00
15/11/2023	99262	CONSULT DIRECT	CLEANING EQUIPMENT AND SERVICES	710.60
29/11/2023	99518	CORNERSTONE CHURCH OF CHRIST INC	EVENT SPONSORSHIP PROGRAM	3,300.00
15/11/2023	99188	CORSIGN WA PTY LTD	SIGNAGE AND TRAFFIC ACCESSORIES	7,610.90
29/11/2023	99549	CORSIGN WA PTY LTD	SIGNAGE AND TRAFFIC ACCESSORIES	4,999.50
22/11/2023	99457	COURTNEY WILLIAMS	STAFF REIMBURSEMENT	33.08
29/11/2023	99491	CR KENNEDY & COMPANY PTY LTD	SURVEY SERVICES	19,041.50
15/11/2023	99366	CR. ANDREW MACNISH	COUNCILLOR PAYMENT	996.18
8/11/2023	99110	CR. ANNE RYAN	COUNCILLOR PAYMENT	3,534.63
15/11/2023	99310	CR. ANNE RYAN	COUNCILLOR PAYMENT	405.08
8/11/2023	99088	CR. G HENLEY	COUNCILLOR PAYMENT	8,397.99
8/11/2023	99120	CR. JARROD KENNEDY	COUNCILLOR PAYMENT	996.18
8/11/2023	99109	CR. JODIE RICHARDS	COUNCILLOR PAYMENT	3,088.14
8/11/2023	99102	CR. KATHERINE COX	COUNCILLOR PAYMENT	3,088.14
8/11/2023	99111	CR. MIKAYLA LOVE	COUNCILLOR PAYMENT	2,739.18
8/11/2023	99093	CR. P CARTER	COUNCILLOR PAYMENT	3,431.44
8/11/2023	99103	CR. P CRONIN	COUNCILLOR PAYMENT	6,091.02
15/11/2023	99247	CR. P CRONIN	COUNCILLOR PAYMENT	963.99
8/11/2023	99092	CR. R PAINE	COUNCILLOR PAYMENT	2,091.97
8/11/2023	99101	CR. SUSAN RICCELLI	COUNCILLOR PAYMENT	3,088.14
15/11/2023	99169	CRANEFORD PLUMBING PTY LTD	PLUMBING SERVICES	11,070.37
29/11/2023	99510	CRANEFORD PLUMBING PTY LTD	PLUMBING SERVICES	49,486.94
22/11/2023	99432	CREATIVE HEART COUNSELLING	WORKSHOP FOR YOUTH SERVICES	450.00
15/11/2023	99230	CS LEGAL - CLOISTERS SQUARE	LEGAL RECOVERY ON RATING DEBTS	805.00
29/11/2023	99591	CS LEGAL - CLOISTERS SQUARE	LEGAL RECOVERY ON RATING DEBTS	835.80
29/11/2023	99570	CURLY'S REMOVALS	FURNITURE TRANSPORT	120.00
15/11/2023	99338	C-WISE	SUPPLY OF SOILS AND MULCH	3,025.00
29/11/2023	99634	DALE HUNTINGTON	CROSSOVER CONTRIBUTION	134.00
15/11/2023	99219	DARRYL AND LEONIE STEAD	WATER CHARGES REIMBURSEMENT	137.75
29/11/2023	99702	DARYL SOMMERVILLE - DS SNAKE MANAGEMENT	SNAKE REMOVAL AND RELOCATION	80.00
15/11/2023	99143	DATA 3	COMPUTER SOFTWARE SUPPLIER	22,988.13
29/11/2023	99483	DATA 3	COMPUTER SOFTWARE SUPPLIER	53,425.76
22/11/2023	99466	DAVID GRAY & CO	GARBAGE BINS & PARTS SUPPLIER	4,708.00
29/11/2023	99745	DAVID MILDWATERS ELECTRICAL	MAINTENANCE SERVICES	1,472.88



**LISTING OF PAYMENTS MADE  
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<b>CHEQUE PAYMENTS</b>	119799 - 119817	44,842.05
<b>ELECTRONIC TRANSFER PAYMENTS</b>	99087 - 99776	8,565,634.79
<b>TRUST ACCOUNT</b>	EFT# TR000015 - TR000017	60,426.14
<b>PAYROLL</b>	01.11.2023 - 30.11.2023	1,772,286.78
<b>INTERNAL PAYMENT VOUCHERS</b>	DD #5500 - 5535	25,595,810.30
		<b>36,039,000.06</b>

8/11/2023	99099	DAVID NICHOLSON	STAFF REIMBURSEMENT	168.00
15/11/2023	99159	DEB HOLDEN	STAFF REIMBURSEMENT	73.48
29/11/2023	99773	DELL AUSTRALIA PTY LTD	COMPUTER EQUIPMENT SUPPLIER	11,147.40
29/11/2023	99704	DENISE ARNOLD	CONCUSSION PRESENTATION	1,650.00
15/11/2023	99146	DENNIS HADDON	ART SALES	52.50
29/11/2023	99741	DEPARTMENT OF FIRE AND EMERGENCY SERVICES	2023/2024 ANNUAL ESL	932,127.71
15/11/2023	99413	DEPARTMENT OF PREMIER & CABINET	ADVERTISING SERVICES	2,745.40
15/11/2023	99145	DEPARTMENT OF PREMIER & CABINET STATE LAW PUBLISHER	LEGAL PUBLICATIONS	852.00
29/11/2023	99485	DEPARTMENT OF PREMIER & CABINET STATE LAW PUBLISHER	LEGAL PUBLICATIONS	124.80
29/11/2023	99492	DEPARTMENT OF TRANSPORT	VEHICLE OWNERSHIP SEARCHES	30.80
15/11/2023	99197	DEPARTMENT OF WATER AND ENVIRONMENTAL REGULATION	ENVIRONMENTAL SERVICES	3,500.00
16/11/2023	99140	DEPUTY CHILD SUPPORT REGISTRAR	SALARY DEDUCTIONS	1,310.55
30/11/2023	99480	DEPUTY CHILD SUPPORT REGISTRAR	SALARY DEDUCTIONS	1,310.55
15/11/2023	99316	DIANE HEART	ART SALES	27.30
29/11/2023	99604	DIESEL AND HYDRAULIC SERVICES SOUTH WEST	PLANT REPAIR AND MAINTENANCE	594.44
29/11/2023	99575	DISCOVER DEADLY	SNK5 SAFETY AWARENESS	900.00
29/11/2023	99619	DONALD CANT WATTS CORKE (WA) PTY LTS	CONSULTANCY SERVICES	62,562.50
15/11/2023	99315	DOROTHY ROBINSON	ART SALES	78.00
15/11/2023	99161	DOROTHY SADDLETON	ART SALES	35.70
15/11/2023	99203	DORSOGNA LIMITED	ICE CREAM AND SMALL GOODS	1,030.45
15/11/2023	99185	DOUTH CONTRACTING	PROPERTY & GARDEN MAINTENANCE	375.72
29/11/2023	99544	DOUTH CONTRACTING	PROPERTY & GARDEN MAINTENANCE	1,187.65
29/11/2023	99720	DUNSBOROUGH VOLUNTEER BUSHFIRE BRIGADE	PETTY CASH REIMBURSEMENT	118.63
15/11/2023	99406	DUNSBOROUGH & DISTRICTS PROGRESS ASSOC INC.	EVENT SPONSORSHIP PROGRAM	7,500.00
29/11/2023	99556	DUNSBOROUGH & DISTRICTS WATER CARTAGE	WATER CARTAGE SERVICES	280.00
29/11/2023	99517	DUNSBOROUGH HARDWARE & HOME CENTRE	HARDWARE SERVICES	12.00
29/11/2023	99615	DUNSBOROUGH SHIPWRIGHTS	FIBREGLASS REPAIRS	951.50
15/11/2023	99173	DYMOCKS - BUSSELTON	LIBRARY RESOURCES	883.35
22/11/2023	99425	DYMOCKS - BUSSELTON	LIBRARY RESOURCES	651.31
29/11/2023	99523	DYMOCKS - BUSSELTON	LIBRARY RESOURCES	1,216.56
15/11/2023	99234	EARTH AND STONE WA	FOOTPATHS MAINTENANCE	88,151.80
8/11/2023	99113	EASI PACKAGING	SALARY PACKAGING SERVICE	12,930.37
16/11/2023	99135	EASI PACKAGING	SALARY PACKAGING SERVICE	21,313.90
30/11/2023	99475	EASI PACKAGING	SALARY PACKAGING SERVICE	23,372.41
15/11/2023	99211	EIS CONTROL PTY LTD	ELECTRICAL SERVICES	7,176.40
29/11/2023	99573	EIS CONTROL PTY LTD	ELECTRICAL SERVICES	17,131.55
15/11/2023	99192	ELAMOORE NATURAL SOAPS & COSMETICS PTY LTD	ART SALES	30.80
29/11/2023	99665	ELITE BUSINESS PERFORMANCE PTY LTD	CONSULTANCY SERVICES	2,433.75
15/11/2023	99215	ELIZABETH BINT	ART SALES	122.50
15/11/2023	99144	ELLIOTT'S FILTRATION	IRRIGATION & FILTRATION SERVICES	2,046.00
15/11/2023	99408	EMERGE OFFICE	OFFICE EQUIPMENT SERVICES - PHOTOCOPIING	92.40
29/11/2023	99755	EMERGE OFFICE	OFFICE EQUIPMENT SERVICES - PHOTOCOPIING	209.66
8/11/2023	99119	EMERGINATION	TIME MANAGEMENT WORKSHOPS	5,390.10
15/11/2023	99328	EMERSON RAINE	STRATA LEVY FEES & WATER CONSUMPTION	2,686.90
15/11/2023	99301	EMMA GEARY	ART SALES	5.60
15/11/2023	99168	ENVIRONEX INTERNATIONAL PTY LTD	POOL CHEMICALS FOR GLC	2,026.99
29/11/2023	99505	ENVIRONEX INTERNATIONAL PTY LTD	POOL CHEMICALS FOR GLC	2,358.64
15/11/2023	99204	ERGO LINK	ERGONOMIC OFFICE EQUIPMENT	1,249.49
15/11/2023	99178	EVERETT'S HOME AND YARD MAINTENANCE	MAINTENANCE SERVICES	3,347.00
15/11/2023	99255	FAIRTEL PTY LTD	TELECOMMUNICATION SERVICES	158.99
22/11/2023	99445	FEDERICO CARRERA	CROSSOVER CONTRIBUTION	310.20
15/11/2023	99342	FIONA FREMLIN	WATER CHARGES REIMBURSEMENT	117.65
29/11/2023	99508	FIRE RESCUE SAFETY AUSTRALIA	FIRE SAFETY EQUIPMENT	1,288.72
15/11/2023	99291	FRED MOSS AND IRMA LANGE	BJTP REFUND	57.00
15/11/2023	99200	FRESH AS	REFRESHMENTS	153.30
29/11/2023	99562	FRESH AS	REFRESHMENTS	472.75
15/11/2023	99180	FRONTLINE FIRE & RESCUE EQUIPMENT	EMERGENCY RESPONSE EQUIPMENT	2,983.66
29/11/2023	99533	FRONTLINE FIRE & RESCUE EQUIPMENT	EMERGENCY RESPONSE EQUIPMENT	7,931.25
15/11/2023	99374	FTE ENGINEERING	MAINTENANCE SERVICES	179.16
29/11/2023	99622	G A SHEPPARD	BJTP REFUND	23.00
29/11/2023	99605	GARMONY PROPERTY CONSULTANTS	PROPERTY EVALUATION	1,732.50
15/11/2023	99331	GARY LEE ELECTRICAL	ELECTRICAL SERVICES	906.75
29/11/2023	99666	GARY LEE ELECTRICAL	ELECTRICAL SERVICES	600.16
15/11/2023	99293	GAVIN AND BEV HARTLEY	BJTP REFUND	57.00
15/11/2023	99320	GENERATORS AUSTRALIA PTY LTD	HIRE AND SALES OF EQUIPMENT	4,822.68
8/11/2023	99087	GEO BAY SWIM	EVENT SPONSORSHIP PROGRAM	8,250.00
15/11/2023	99244	GEO CABINETS	CARPENTRY SERVICES	7,986.00
15/11/2023	99195	GEOBOX PTY LTD	VEHICLE CAMERAS	426.80
29/11/2023	99558	GEOBOX PTY LTD	VEHICLE CAMERAS	1,892.00
15/11/2023	99190	GEOGRAPHE COMMUNITY LANDCARE NURSERY	NURSERY SUPPLIES	124.00
29/11/2023	99550	GEOGRAPHE COMMUNITY LANDCARE NURSERY	NURSERY SUPPLIES	360.00
29/11/2023	99514	GEOGRAPHE INDUSTRIAL SUPPLIES	INDUSTRIAL SUPPLIES	78.54
15/11/2023	99416	GEOGRAPHE SAWS AND MOWERS	PLANT PURCHASES / SERVICES / PARTS	869.00
29/11/2023	99762	GEOGRAPHE SAWS AND MOWERS	PLANT PURCHASES / SERVICES / PARTS	822.00
29/11/2023	99715	GEOGRAPHE TIMBER & HARDWARE	HARDWARE SUPPLIES	196.15
15/11/2023	99242	GEOSPREAD	GREEN WASTE MULCHING	60,593.50
29/11/2023	99756	GHD PTY LTD	CONSULTANCY SERVICES - DUNSBOROUGH GROUNDWATER INVESTIGATION	32,814.65
15/11/2023	99311	GLOBAL SPILL CONTROL PTY LTD	SPILL, SITE SAFETY & TRAFFIC MANAGEMENT	2,637.40
29/11/2023	99647	GLOBAL SPILL CONTROL PTY LTD	SPILL, SITE SAFETY & TRAFFIC MANAGEMENT	1,190.79
15/11/2023	99402	GLOBE AUSTRALIA PTY LTD	PEST CONTROL	6,072.00
29/11/2023	99629	GOH SUM SING & HAIDEN RENDELL	CROSSOVER CONTRIBUTION	283.10
15/11/2023	99344	HALLY'S	CATERING	244.50
29/11/2023	99632	HAO-CHUN HSU & YU-YEN CHANG	CROSSOVER CONTRIBUTION	215.30
15/11/2023	99223	HARBECKS TRANSPORT	SAND AND GRAVEL SUPPLIES	62,079.36
29/11/2023	99581	HARBECKS TRANSPORT	SAND AND GRAVEL SUPPLIES	30,740.49
15/11/2023	99318	HARCHER SOUTHWEST FORMERLY PRESTIGE PRODUCTS	HOSPITALITY EQUIPMENT SUPPLIER	4,903.95
29/11/2023	99654	HARCHER SOUTHWEST FORMERLY PRESTIGE PRODUCTS	HOSPITALITY EQUIPMENT SUPPLIER	3,168.20
29/11/2023	99608	HARE & FORBES	WORKSHOP MACHINERY	292.00
29/11/2023	99694	HART SPORT	SPORTING EQUIPMENT	1,758.00
29/11/2023	99680	HARVEY NORMAN AV/IT BUSSELTON	COMPUTER AND ELECTRICAL	90.00



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<b>CHEQUE PAYMENTS</b>	119799 - 119817	44,842.05
<b>ELECTRONIC TRANSFER PAYMENTS</b>	99087 - 99776	8,565,634.79
<b>TRUST ACCOUNT</b>	EFT# TR000015 - TR000017	60,426.14
<b>PAYROLL</b>	01.11.2023 - 30.11.2023	1,772,286.78
<b>INTERNAL PAYMENT VOUCHERS</b>	DD #5500 - 5535	25,595,810.30
		<b>36,039,000.06</b>

22/11/2023	99437	HEATH ROSSITER	CROSSOVER CONTRIBUTION	201.80
15/11/2023	99335	HERRON TODD WHITE (WESTERN AUSTRALIA) PTY LTD	PROPERTY VALUATIONS	880.00
29/11/2023	99774	HIP POCKET WORK WEAR AND SAFETY	UNIFORMS & PROTECTIVE CLOTHING	2,546.32
29/11/2023	99664	HIRE A FENCE	TEMPORARY FENCING HIRE	356.40
15/11/2023	99157	HOLCIM (AUSTRALIA) PTY LTD	CONCRETE SERVICES	5,559.62
29/11/2023	99493	HOLCIM (AUSTRALIA) PTY LTD	CONCRETE SERVICES	2,185.70
29/11/2023	99722	HOLCIM (AUSTRALIA) PTY LTD	CONCRETE SERVICES	8,573.61
29/11/2023	99691	HOLLY STUBBS ( CRAIG)	AUTHOR TALK	374.00
15/11/2023	99296	HOSPITALITY ROADSHOW	BOND REFUND	1,000.00
29/11/2023	99582	ILLION AUSTRALIA PTY LTD	TENDER ADVERTISING AND MANAGEMENT	292.86
15/11/2023	99194	INDIAN OCEAN LONGBOARD CLUB INC	EQUIPMENT GRANT	4,000.00
29/11/2023	99699	INNOVATION CLUSTER	SOUTH WEST ANGELS & SUMMIT SPONSORSHIP - YOUTH SCHOLARSHIP PROGRAM	22,000.00
29/11/2023	99506	IRONMAN AUSTRALIA	PAYMENT - PARTNERSHIP OF 2023 IRONMAN WA	154,000.00
15/11/2023	99341	IRONMAN OCEANIA	BOND REFUND	5,500.00
15/11/2023	99238	ITR PACIFIC PTY LTD	EARTHMOVING MACHINERY SPARE PARTS	2,845.46
15/11/2023	99288	J BUTTERFIELD	BJTP REFUND	57.00
29/11/2023	99706	J D'AGOSTINO & S LUFF ARCHITECTS	ARCHITECTURAL SERVICES	4,510.00
8/11/2023	99091	JACQUELINE HAPP	STAFF REIMBURSEMENT	95.50
22/11/2023	99424	JACQUELINE HAPP	STAFF REIMBURSEMENT	83.49
15/11/2023	99241	JAMES BENNETT	SPECIALIST LIBRARY RESOURCES	167.62
29/11/2023	99600	JAMES BENNETT	SPECIALIST LIBRARY RESOURCES	292.22
15/11/2023	99205	JAN ROBERTS	ART SALES	160.00
15/11/2023	99375	JASON SIGNMAKERS	SIGNAGE SUPPLIES	3,642.06
29/11/2023	99717	JASON SIGNMAKERS	SIGNAGE SUPPLIES	53.13
29/11/2023	99566	JDM EARTHWORKS	STONWORK SERVICES	4,455.00
29/11/2023	99574	JDSI CONSULTING ENGINEERS PTY LTD	ENGINEERING CONSULTING SERVICES	2,475.00
15/11/2023	99191	JENNIFER BROWN	ART SALES	184.00
15/11/2023	99292	JEREMY & BROOK HAMILTON	BJTP REFUND	57.00
15/11/2023	99355	JESSICA WATTS	STAFF REIMBURSEMENT	131.03
29/11/2023	99700	JETCO ENGINEERING PTY LTD	BEACH RAKE MACHINE AND PARTS	3,248.74
15/11/2023	99240	JIGSAW SIGNS & PRINT	SIGNAGE SERVICES	363.00
29/11/2023	99598	JIGSAW SIGNS & PRINT	SIGNAGE SERVICES	253.00
15/11/2023	99174	JIMS FIRST	HARDWARE SUPPLIES	30.60
29/11/2023	99524	JIMS FIRST	HARDWARE SUPPLIES	334.80
16/11/2023	99136	JMW REAL ESTATE	T NOTTLE RENT	1,400.00
30/11/2023	99476	JMW REAL ESTATE	T NOTTLE RENT	1,400.00
29/11/2023	99620	JOANNE PIENING	BJTP REFUND	57.00
8/11/2023	99123	JODI KELSO	STAFF REIMBURSEMENT	144.00
22/11/2023	99456	JODI KELSO	STAFF REIMBURSEMENT	33.08
15/11/2023	99259	JOEL SMOKER	ART SALES	17.47
8/11/2023	99122	JOHN EVANS	STAFF REIMBURSEMENT	370.00
29/11/2023	99633	JOHNATHON AND CARMEN BERRY	CROSSOVER CONTRIBUTION	167.90
29/11/2023	99657	JOSEPH CASPERSZ-LONEY	STAFF REIMBURSEMENT	87.01
15/11/2023	99340	JOSEPHINE TAYLOR	STAFF REIMBURSEMENT	1,500.00
22/11/2023	99450	JOSEPHINE TAYLOR	STAFF REIMBURSEMENT	66.90
29/11/2023	99637	JOSHUA FOLEY	CARPENTRY SERVICES	4,995.83
29/11/2023	99771	JUICE PRINT	PRINTING SERVICES	156.75
15/11/2023	99236	JULIE GUTHRIDGE	ART SALES	141.40
22/11/2023	99433	KAREN AUSTIN	REFUND DOG REGISTRATION	150.00
15/11/2023	99252	KAREN TAYLOR-VIVIAN	WATER CHARGES REIMBURSEMENT	657.95
8/11/2023	99117	KATE JAMES	STAFF REIMBURSEMENT	51.98
29/11/2023	99667	KATE JAMES	STAFF REIMBURSEMENT	27.06
15/11/2023	99266	KATHLEEN NEELING	ART SALES	31.50
15/11/2023	99263	KAY MANOLAS	ART SALES	65.60
22/11/2023	99440	KAYLA LOUISE LAWRENCE	CROSSOVER CONTRIBUTION	289.90
29/11/2023	99594	KELLY MCGORLICK	WATER CHARGES REIMBURSEMENT	279.20
15/11/2023	99299	KENNETH ROWE	REFUND OF ANIMAL SURRENDER FEE	150.00
8/11/2023	99089	KERRY HILL ARCHITECTS	ARCHITECTURAL SERVICES	40,920.00
15/11/2023	99218	KEVIN NEWELL	WATER CHARGES REIMBURSEMENT	46.90
22/11/2023	99455	KIM WINFIELD	STAFF REIMBURSEMENT	22.18
29/11/2023	99697	KIM WINFIELD	STAFF REIMBURSEMENT	240.00
8/11/2023	99124	KING & WOOD MALLESONS	LAND PURCHASE DEPOSIT	500,000.00
8/11/2023	99095	KITCHEN TAKEOVERS	CATERING	360.00
22/11/2023	99427	KITCHEN TAKEOVERS	CATERING	1,758.80
29/11/2023	99742	KLEENHEAT GAS	GAS SERVICES	2,225.10
8/11/2023	99096	KMART BUSSELTON	RETAIL HOME WARES	678.50
15/11/2023	99214	KMART BUSSELTON	RETAIL HOME WARES	235.50
29/11/2023	99576	KMART BUSSELTON	RETAIL HOME WARES	164.55
15/11/2023	99343	KOMATSU AUSTRALIA	VEHICLE PURCHASES / SERVICES / PARTS	905.81
22/11/2023	99461	KYLIE FALCONER	STAFF REIMBURSEMENT	33.08
15/11/2023	99329	LAND & WATER CONSULTING	AUDIT SERVICES	3,575.00
15/11/2023	99382	LANDGATE (VALUER GENERAL'S OFFICE)	LAND VALUATIONS	137.40
29/11/2023	99725	LANDGATE (VALUER GENERAL'S OFFICE)	LAND VALUATIONS	4,580.18
15/11/2023	99378	LANDGATE CUSTOMER ACCOUNT	LAND INFORMATION AND TITLE SEARCHES	95.50
29/11/2023	99721	LANDGATE CUSTOMER ACCOUNT	LAND INFORMATION AND TITLE SEARCHES	122.00
29/11/2023	99686	LAUNDRY INVESTMENTS INVESTMENTS	DRYCLEANING AND LINEN HIRE SERVICES	351.95
15/11/2023	99302	LAWRENCE & HANSON	ELECTRICAL/PROTECTIVE CLOTHING SUPPLIES	3,848.00
29/11/2023	99639	LAWRENCE & HANSON	ELECTRICAL/PROTECTIVE CLOTHING SUPPLIES	1,561.24
15/11/2023	99333	LD TOTAL	LANDSCAPE MAINTENANCE	17,120.83
29/11/2023	99668	LD TOTAL	LANDSCAPE MAINTENANCE	11,430.88
8/11/2023	99121	LEE WILD	STAFF REIMBURSEMENT	67.90
29/11/2023	99698	LEE WILD	STAFF REIMBURSEMENT	230.80
29/11/2023	99607	LEEDERVILLE CAMERAS	CAMERA EQUIPMENT	550.00
15/11/2023	99150	LEEUWIN CIVIL PTY LTD	HIRE AND SALES OF EQUIPMENT	95,308.16
29/11/2023	99761	LEEUWIN TRANSPORT	COURIER SERVICES	3,039.95
22/11/2023	99430	LES MILLS ASIA PACIFIC INDUSTRIES	GYM PROGRAMME UPDATES	7,795.04
15/11/2023	99356	LESCHENAULT CATCHMENT COUNCIL INC	KEEPING IT LOCAL BOOKLET	2,750.00
22/11/2023	99460	LEZIA RAMSDEN	STAFF REIMBURSEMENT	172.88
29/11/2023	99519	LG CONNECT PTY LTD	BUSINESS SYSTEM CONSULTING SERVICES	2,860.00
15/11/2023	99186	LGISWA	WORKER COMPENSATION SERVICES	5,000.00



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<b>CHEQUE PAYMENTS</b>	119799 - 119817	44,842.05
<b>ELECTRONIC TRANSFER PAYMENTS</b>	99087 - 99776	8,565,634.79
<b>TRUST ACCOUNT</b>	EFT# TRO00015 - TRO00017	60,426.14
<b>PAYROLL</b>	01.11.2023 - 30.11.2023	1,772,286.78
<b>INTERNAL PAYMENT VOUCHERS</b>	DD #5500 - 5535	25,595,810.30
		<b>36,039,000.06</b>

8/11/2023	99115	LIAM JACKSON	EQUIPMENT HIRE	4,147.00
29/11/2023	99662	LIAM JACKSON	EQUIPMENT HIRE	10,587.50
29/11/2023	99585	LIFE READY PHYSIO & PILATES BUSSELTON	MUSCULOKELETAL FUNCTIONAL CAPACITY	195.00
15/11/2023	99283	LJ & JR HOWARD	BJTP REFUND	30.00
29/11/2023	99606	LOCK AROUND THE CLOCK	SECURITY SERVICES	312.50
29/11/2023	99612	LOT 1150 PTY LTD ATF LOT 1150 UNIT TRUST	RATE REFUND	443.82
29/11/2023	99530	LOTX FILTER CLEANING SERVICE	PLANT FILTER CLEANING SERVICE	489.31
15/11/2023	99275	LUCIE ATKINSON	GLC REFUNDS	572.65
15/11/2023	99323	MALATESTA ROAD PAVING & HOTMIX	ROAD HOTMIX / PAVING SERVICES	6,515.00
29/11/2023	99658	MALATESTA ROAD PAVING & HOTMIX	BUSSELTON HIGHWAY PROFILING & LINE MARKING	70,321.90
22/11/2023	99448	MANDY ANSON	STAFF REIMBURSEMENT	33.08
15/11/2023	99360	MANGLESHI CONTRACTING	LANDSCAPE MAINTENANCE	5,346.35
15/11/2023	99177	MARGARET RIVER BUSSELTON TOURISM ASSOCIATION	FINANCIAL ASSISTANCE TO THE LOCAL VISITOR CENTRE 23/24	42,201.73
29/11/2023	99528	MARGARET RIVER BUSSELTON TOURISM ASSOCIATION	IRONMAN WA FOC ROOM FUNDING - MILESTONE 1	11,000.00
29/11/2023	99512	MARGARET RIVER FENCING	MAINTENANCE SERVICES	1,155.00
22/11/2023	99443	MARK LANE	CROSSOVER CONTRIBUTION	398.10
15/11/2023	99187	MARTINE PERRET	ART SALES	83.79
22/11/2023	99438	MATTHEW HOOD	CROSSOVER CONTRIBUTION	389.00
15/11/2023	99290	MAUREEN O'CONNOR	BJTP REFUND	57.00
29/11/2023	99638	MAYDAY SERVICES	EQUIPMENT HIRE	36,841.15
29/11/2023	99678	MB TRAFFIC PLANNING & MANAGEMENT	TRAFFIC MANAGEMENT CONSULTANCY	462.00
8/11/2023	99130	MCGREGOR WJ & J	MANAGEMENT BUSSELTON JETTY TOURIST PARK	49,913.20
29/11/2023	99769	MCGREGOR WJ & J	MANAGEMENT BUSSELTON JETTY TOURIST PARK	270.93
29/11/2023	99502	MCLEODS BARRISTERS & SOLICITORS	LEGAL SERVICES	13,189.00
15/11/2023	99330	MDH LIQUID WASTE	LIQUID WASTE SERVICES	299.70
29/11/2023	99663	MDH LIQUID WASTE	LIQUID WASTE SERVICES	429.40
29/11/2023	99601	MDM ENTERTAINMENT PTY LTD	DVD AND CD SUPPLY TO LIBRARY	24.42
15/11/2023	99347	MERLE DAVIS	ART SALES	110.60
15/11/2023	99265	MESSAGE4U PTY LTD	BULK BUSINESS SMS	29.13
15/11/2023	99222	MICHAEL & KATE WILSON	WATER CHARGES REIMBURSEMENT	115.40
29/11/2023	99630	MICHAEL & LISA DEVLIN	CROSSOVER CONTRIBUTION	127.20
29/11/2023	99578	MICHAEL SHERRY PAINTING AND DECORATING PTY LTD	PAINTING SERVICES	1,793.00
15/11/2023	99217	MIKE MORGAN	WATER CHARGES REIMBURSEMENT	116.00
15/11/2023	99350	MILLS RECRUITMENT	RECRUITMENT SERVICES	4,620.00
29/11/2023	99687	MILLS RECRUITMENT	RECRUITMENT SERVICES	5,390.00
15/11/2023	99400	MINTER ELLISON LAWYERS	LEGAL SERVICES	4,794.57
29/11/2023	99747	MINTER ELLISON LAWYERS	LEGAL SERVICES	813.45
29/11/2023	99689	MIRANDA FREE	ART SALES	15.50
15/11/2023	99284	ML & LA SLOPER	BJTP REFUND	57.00
29/11/2023	99542	MOORE AUSTRALIA	TRAINING SERVICES	1,155.00
22/11/2023	99454	MR LACHLAN MILNE	MEDICAL REPORTS	440.00
8/11/2023	99114	MTD HOSPITALITY CONSULTING	KITCHEN AND LAUNDRY DESIGN	4,708.00
15/11/2023	99254	MUIR FORD BUSSELTON	VEHICLE MAINTENANCE	1,138.00
29/11/2023	99610	MUIR FORD BUSSELTON	VEHICLE MAINTENANCE	671.40
29/11/2023	99635	N MACKLIN & A BLACK	CROSSOVER CONTRIBUTION	166.10
15/11/2023	99277	NA & WJ TAYLOR	REFUND DOG REGISTRATION	150.00
15/11/2023	99170	NALDA HOSKINS	ART SALES	123.20
15/11/2023	99282	NATASCHA HEILMEIER	BJTP REFUND	7.00
29/11/2023	99571	NATIONAL ASSOCIATION FOR THE VISUAL ARTS	MEMBERSHIP	370.00
15/11/2023	99249	NATURAL EDGE FRAMING & PHOTOGRAPHY	ART SALES	52.85
29/11/2023	99532	NATURALISTE HYGIENE SERVICES	HYGIENE SERVICES	1,320.00
15/11/2023	99224	NEW IMAGE LANDSCAPING AND MAINTENANCE	LANDSCAPE MAINTENANCE	42,806.06
29/11/2023	99584	NEW IMAGE LANDSCAPING AND MAINTENANCE	LANDSCAPE MAINTENANCE	1,192.50
15/11/2023	99377	NICHOLLS MACHINERY	PLANT PURCHASES / SERVICES / PARTS	401.50
29/11/2023	99719	NICHOLLS MACHINERY	PLANT PURCHASES / SERVICES / PARTS	225.80
22/11/2023	99444	NICOLA MACKLIN & ANTHONY BLACK	CROSSOVER CONTRIBUTION	166.10
29/11/2023	99628	NICOLA MACKLIN & ANTHONY BLACK	CROSSOVER CONTRIBUTION	166.10
22/11/2023	99439	NICOLE BOWSER AND ALEC WILLIAMS	CROSSOVER CONTRIBUTION	148.10
15/11/2023	99289	NOELINE & LES MCCAMISH	BJTP REFUND	57.00
15/11/2023	99202	OFFICEWORKS	OFFICE EQUIPMENT SERVICES	57.61
29/11/2023	99565	OFFICEWORKS	OFFICE EQUIPMENT SERVICES	189.70
15/11/2023	99313	OMNICOM MEDIA GROUP AUSTRALIA PTY LTD	MEDIA AND ADVERTISING SERVICES	1,816.11
29/11/2023	99650	OMNICOM MEDIA GROUP AUSTRALIA PTY LTD	MEDIA AND ADVERTISING SERVICES	373.21
15/11/2023	99172	OPTUS BILLING SERVICES PTY LTD	FIXED INTERNET ACCESS	2,500.00
29/11/2023	99516	OPTUS BILLING SERVICES PTY LTD	FIXED INTERNET ACCESS	566.90
15/11/2023	99317	ORANA CONCRETE PTY LTD	CONCRETE SERVICES	12,705.00
29/11/2023	99653	ORANA CONCRETE PTY LTD	CONCRETE SERVICES	15,768.50
15/11/2023	99415	ORBIT HEALTH & FITNESS SOLUTIONS	GYM EQUIPMENT SUPPLIER	169.04
15/11/2023	99156	OUR COMMUNITY	ANNUAL SUBSCRIPTION	23,750.00
29/11/2023	99655	PAATSCH CONSULTING PTY LTD	CONSULTATION SERVICES GLC MASTERPLAN	25,917.10
29/11/2023	99568	PAINT INDUSTRIES PTY LTD	PAINTING SERVICES	4,001.08
29/11/2023	99617	PARA MOBILITY PTY LTD	POOL HOIST AND ACCESSORIES	715.00
22/11/2023	99462	PATRICIA MILLER	STAFF REIMBURSEMENT	33.08
15/11/2023	99232	PATRICK MARSHALL	STAFF REIMBURSEMENT	179.94
29/11/2023	99596	PAUL BREMAN	STAFF REIMBURSEMENT	516.46
29/11/2023	99726	PENDREY AGENCIES P/L	CHEMICAL/RURAL SUPPLIES	4,326.62
8/11/2023	99090	PENNY CROWLEY	STAFF REIMBURSEMENT	275.50
15/11/2023	99233	PERTH INTERNATIONAL JAZZ FESTIVAL INC	JAZZ FESTIVAL HOSTING	13,750.00
29/11/2023	99595	PERTH INTERNATIONAL JAZZ FESTIVAL INC	JAZZ FESTIVAL HOSTING	11,000.00
15/11/2023	99213	PETER & SHARYN BAKER	WATER CHARGES REIMBURSEMENT	1,604.35
15/11/2023	99286	PETER & SUSAN KOS	BJTP REFUND	118.00
15/11/2023	99268	PETER EVANS	ART SALES	147.00
15/11/2023	99321	PETTIT NOMINEES PTY LTD	MAINTENANCE SERVICES	489.50
15/11/2023	99421	PFD FOOD SERVICES PTY LTD	GLC KIOSK PURCHASES	1,544.90
29/11/2023	99775	PFD FOOD SERVICES PTY LTD	GLC KIOSK PURCHASES	1,509.80
15/11/2023	99149	PHIL HOLLETT PHOTOGRAPHY	ART SALES	4.20
15/11/2023	99278	PHILLIPA & IAN PARRY	BJTP REFUND	57.00
15/11/2023	99154	PHOENIX FOUNDRY PTY LTD	MEMORIAL PLAQUES SUPPLIER	601.70
29/11/2023	99708	PK COURIERS	COURIER SERVICES	566.50
29/11/2023	99648	PLAY CHECK	PLAYGROUND AUDITING SERVICES	9,735.00



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<b>TRUST ACCOUNT</b>	EFT# TR000015 - TR000017	60,426.14
<b>PAYROLL</b>	01.11.2023 - 30.11.2023	1,772,286.78
<b>INTERNAL PAYMENT VOUCHERS</b>	DD #5500 - 5535	25,595,810.30
		<b>36,039,000.06</b>

15/11/2023	99225	POWER MEDICAL CENTRE	PRE EMPLOYMENT MEDICAL	638.00
29/11/2023	99586	POWER MEDICAL CENTRE	PRE EMPLOYMENT MEDICAL	478.50
29/11/2023	99488	PRIME TELEVISION (SOUTHERN) PTY LIMITED	ADVERTISING SERVICES	2,355.10
15/11/2023	99206	PRO CRACK SEAL	CRACK SEALING SERVICES	4,455.00
15/11/2023	99319	PROGRAMMED PROPERTY SERVICES	MAINTENANCE SERVICES	6,468.00
15/11/2023	99221	PROJECT MILES TRUST	REPAIRS AND MAINTENANCE	1,402.50
22/11/2023	99428	PROJECT MILES TRUST	REPAIRS AND MAINTENANCE	13,561.90
29/11/2023	99580	PROJECT MILES TRUST	REPAIRS AND MAINTENANCE	5,564.90
29/11/2023	99539	PVR INDUSTRIAL PTY LTD	INDUSTRIAL PUMP REPAIRS	3,375.90
29/11/2023	99521	QUALITYPRESS	PRINTING SERVICES	1,244.10
29/11/2023	99677	QUEST KINGS PARK	ACCOMMODATION	552.00
29/11/2023	99696	QUEST MOUNTS BAY ROAD	ACCOMMODATION	585.00
29/11/2023	99626	NM & CC HOPKINS	RATE REFUND	1,132.47
29/11/2023	99627	NM & CC HOPKINS	RATE REFUND	444.95
29/11/2023	99770	RAY MCMILLAN	BOND REFUND	100.00
15/11/2023	99294	RC & EK SHORTLAND	BJTP REFUND	249.00
29/11/2023	99625	RC & EK SHORTLAND	BJTP REFUND	249.00
29/11/2023	99753	RECEC PTY LIMITED	PLUMBING SERVICES	49.82
15/11/2023	99388	REPCO AUTO PARTS	PLANT PURCHASES / SERVICES / PARTS	1,505.36
29/11/2023	99732	REPCO AUTO PARTS	PLANT PURCHASES / SERVICES / PARTS	2,229.59
29/11/2023	99684	RJ STRUCTURAL	STRUCTURAL ENGINEERING	4,987.40
22/11/2023	99429	RMC RAIL SERVICES PTY LTD	JETTY RAILWAY LINE INSPECTION	3,994.44
29/11/2023	99767	RMS (AUST) PTY LTD	SOFTWARE SERVICES	275.83
29/11/2023	99543	ROBERT'S TILT TRAY & HIAB SERVICE	FREIGHT	1,361.25
15/11/2023	99385	ROD'S AUTO ELECTRICS	AUTO ELECTRICAL SERVICES	1,063.39
29/11/2023	99729	ROD'S AUTO ELECTRICS	AUTO ELECTRICAL SERVICES	2,011.17
15/11/2023	99365	ROTARY CLUB OF BUSSELTON GEOGRAPHE BAY	SPONSORSHIP / ADVERTISING	14.00
15/11/2023	99363	RSPCA WA	COMMUNITY ASSISTANCE PROGRAM	3,465.00
22/11/2023	99453	RSPCA WA	COMMUNITY ASSISTANCE PROGRAM	3,465.00
15/11/2023	99397	RUBEK AUTOMATIC DOORS	AUTOMATIC DOOR SERVICES	8,163.27
29/11/2023	99599	RURAL PRESS PTY LTD	ADVERTISING SERVICES	355.01
15/11/2023	99189	RUTH BROOKS	ART SALES	28.80
15/11/2023	99345	SACHIKO PEARCE	CATERING	216.00
29/11/2023	99681	SACHIKO PEARCE	CATERING	1,005.00
15/11/2023	99165	SAFE FIRST TRAINING	STAFF TRAINING	220.00
29/11/2023	99501	SAFE FIRST TRAINING	STAFF TRAINING	134.00
15/11/2023	99258	SAGE CONSULTING ENGINEERS PTY LTD	ELECTRICAL SERVICES	5,368.00
15/11/2023	99308	SALENTO CATERING	CATERING	1,124.75
22/11/2023	99446	SALVATORE PETER MACCORA	CROSSOVER CONTRIBUTION	345.50
15/11/2023	99267	SANDRA WALKER	ART SALES	4.40
15/11/2023	99322	SAPIO PTY LTD	SECURITY INSTALLATIONS AND MAINTENANCE	21,334.80
29/11/2023	99656	SAPIO PTY LTD	SECURITY INSTALLATIONS AND MAINTENANCE	1,825.23
22/11/2023	99464	SARAH FRANCIS	STAFF REIMBURSEMENT	33.08
29/11/2023	99507	SCOPE BUSINESS IMAGING	ELECTRONIC EQUIPMENT	408.00
29/11/2023	99636	SCOTT PEARCE	SINGING PERFORMANCE	450.00
29/11/2023	99577	SEA CHANGE NEWS	STATIONERY AND OFFICE SUPPLIES	193.99
15/11/2023	99314	SEEK LIMITED	ADVERTISING SERVICES	759.00
29/11/2023	99651	SEEK LIMITED	ADVERTISING SERVICES	1,820.50
15/11/2023	99420	SHARON WILLIAMS	ART SALES	104.00
15/11/2023	99298	SHELLEY O'MALLEY	REFUND DOG REGISTRATION	30.00
15/11/2023	99362	SHELTER WA INC	HOMELESSNESS KNOWLEDGE TRAINING	440.00
29/11/2023	99772	SHORE COASTAL PTY LTD	COASTAL CONSULTANCY SERVICES	7,691.20
8/11/2023	99112	SHOREWATER MARINE PTY LTD	MARINE MAINTENANCE - BUSSELTON JETTY	107,868.86
29/11/2023	99649	SHOREWATER MARINE PTY LTD	MARINE MAINTENANCE - BUSSELTON JETTY	2,837.45
15/11/2023	99274	SIAN FENN	GLC REFUNDS	66.40
15/11/2023	99349	SIGMA CHEMICALS	CHEMICAL SUPPLIER	367.40
15/11/2023	99216	SIMONE NEWMAN	WATER CHARGES REIMBURSEMENT	320.00
8/11/2023	99100	SLIMLINE WAREHOUSE	DISPLAY MATERIALS	544.08
29/11/2023	99652	SMOKIN BULL BBQ	CATERING	4,537.50
15/11/2023	99336	SOILS AINT SOILS	NURSERY SUPPLIES	96.00
29/11/2023	99671	SOILS AINT SOILS	NURSERY SUPPLIES	296.00
8/11/2023	99118	SOLOMONS BUSSELTON	FLOORING AND BLIND SERVICES	9,500.00
29/11/2023	99564	SONGFEST INCORPORATED	ADVOCATES FOR COMMUNITY SINGING	5,000.00
29/11/2023	99513	SONIC HEALTH PLUS PTY LTD	OCCUPATIONAL HEALTH SERVICES	161.70
29/11/2023	99611	SOS OFFICE EQUIPMENT	OFFICE EQUIPMENT SERVICES	2,875.88
29/11/2023	99541	SOUTH REGIONAL TAFE	EDUCATION & TRAINING	245.10
29/11/2023	99661	SOUTH WEST CLEANING	BBQ CLEANING	6,040.11
15/11/2023	99380	SOUTH WEST MACHINING CENTRE	PLANT MAINTENANCE SERVICES	9,157.50
29/11/2023	99723	SOUTH WEST MACHINING CENTRE	PLANT MAINTENANCE SERVICES	903.71
15/11/2023	99227	SOUTH WEST OFFICE NATIONAL	STATIONERY	1,918.90
29/11/2023	99587	SOUTH WEST OFFICE NATIONAL	STATIONERY	466.49
29/11/2023	99511	SOUTH WEST PRINTING & PUBLISHING	ADVERTISING SERVICES	2,900.00
15/11/2023	99379	SOUTH WEST STEEL PRODUCTS	STEEL PRODUCTS SUPPLIER	1,090.39
29/11/2023	99559	SOUTHERN ATU SERVICES PTY LTD	WASTE MANAGEMENT SERVICES	415.00
29/11/2023	99552	SOUTHERN CROSS AUSTEREO	ADVERTISING SERVICES	1,821.60
29/11/2023	99716	SOUTHERN LOCK & SECURITY	SECURITY SERVICES	1,036.20
15/11/2023	99404	SOUTHERN MACHINING & MAINTENANCE	PLANT MAINTENANCE SERVICES	1,320.00
29/11/2023	99749	SOUTHERN MACHINING & MAINTENANCE	PLANT MAINTENANCE SERVICES	198.00
15/11/2023	99334	SOUTHWEST TYRE SERVICE	PLANT TYRE SUPPLIER / REPAIRER	3,444.00
29/11/2023	99669	SOUTHWEST TYRE SERVICE	PLANT TYRE SUPPLIER / REPAIRER	19,759.50
29/11/2023	99623	SP & RG MARRON	BJTP REFUND	49.00
29/11/2023	99569	SPECTUR LTD	SOLAR SECURITY CAMERAS	1,240.80
29/11/2023	99531	SPORTFIRST BUSSELTON	SPORTING EQUIPMENT	1,466.30
29/11/2023	99709	SPOTLIGHT	VACATION CARE SUPPLIES	208.90
15/11/2023	99183	SPRAYMOW SERVICES	MAINTENANCE SERVICES	4,845.29
29/11/2023	99536	SPRAYMOW SERVICES	MAINTENANCE SERVICES	17,160.34
15/11/2023	99158	ST JOHN AMBULANCE	TRAINING SERVICES	340.00
29/11/2023	99495	ST JOHN AMBULANCE	TRAINING SERVICES	319.05
22/11/2023	99463	STACEY ROUSE	STAFF REIMBURSEMENT	33.08
15/11/2023	99305	STATEWIDE DOORS BUSSELTON	MAINTENANCE SERVICES	1,575.00



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<b>TRUST ACCOUNT</b>	EFT# TR000015 - TR000017	60,426.14
<b>PAYROLL</b>	01.11.2023 - 30.11.2023	1,772,286.78
<b>INTERNAL PAYMENT VOUCHERS</b>	DD #5500 - 5535	25,595,810.30
		<b>36,039,000.06</b>

29/11/2023	99525	STAX CHAIRS	OUTDOOR FURNITURE	477.40
29/11/2023	99693	STEPHANIE VAUGHAN	AUTHOR TALK	344.30
22/11/2023	99442	STEPHEN AND ANN HUBBARD	CROSSOVER CONTRIBUTION	154.10
15/11/2023	99368	STEWART & HEATON CLOTHING CO PTY LTD	PROTECTIVE CLOTHING SUPPLIER	2,090.55
29/11/2023	99707	STEWART & HEATON CLOTHING CO PTY LTD	PROTECTIVE CLOTHING SUPPLIER	724.25
29/11/2023	99590	STINSON AIR SOUTH WEST	AIR CONDITIONING SERVICES	374.20
29/11/2023	99763	STRATAGREEN	NURSERY SUPPLIES	87.32
16/11/2023	99142	SUPERCHOICE	SUPERANNUATION	198,908.11
30/11/2023	99482	SUPERCHOICE	SUPERANNUATION	200,795.08
15/11/2023	99354	SUSAN GRAF	ART SALES	48.00
22/11/2023	99470	SUSSEX BUSHFIRE BRIGADE	PETTY CASH REIMBURSEMENT	136.69
15/11/2023	99386	SW PRECISION PRINT	PRINTING SERVICES	1,932.15
29/11/2023	99730	SW PRECISION PRINT	PRINTING SERVICES	1,222.00
29/11/2023	99553	SWAN RIVER HOTEL	ACCOMMODATION	760.00
29/11/2023	99572	SWEET & UNIQUE (WA) PTY LTD T/AS LUV A LOLLY	CONFECTIONERY	259.88
15/11/2023	99269	SWITCHED ONTO SAFETY	CHEMICAL MANAGEMENT SYSTEM	4,880.70
8/11/2023	99098	SYNERGY	ELECTRICITY	66,926.47
15/11/2023	99231	SYNERGY	ELECTRICITY	186,399.91
29/11/2023	99592	SYNERGY	ELECTRICITY	53,480.80
29/11/2023	99718	T J DEPIAZZI & SONS	LAWN SUPPLIES	10,829.94
22/11/2023	99465	TAHLIA MUELLER	STAFF REIMBURSEMENT	4,000.00
15/11/2023	99287	TAMARA BURKE	BJTP REFUND	57.00
29/11/2023	99487	TARVIA PTY LTD	ENGINEERING SERVICES	1,759.15
29/11/2023	99557	TAS'S BAKERY	CATERING	176.76
15/11/2023	99171	TEAM DIGITAL	DIGITAL IMAGING	1,035.00
15/11/2023	99309	TEAM GLOBAL EXPRESS	COURIER SERVICES	963.63
29/11/2023	99643	TEAM GLOBAL EXPRESS	COURIER SERVICES	309.46
15/11/2023	99250	TECH WINDOW GAVIN KENNETH BARNES t/as	IT SUPPORT AND RETAIL SALES	123.60
29/11/2023	99676	TELSTRA LIMITED	COMMUNICATION SERVICES	17,967.54
15/11/2023	99353	THE AUSTRALIAN INSTITUTE OF ARCHITECTS	DONATION WA REGIONAL ARCHITECTURE SYMPOS	2,200.00
29/11/2023	99701	THE BUTCHERS SHOP WA PTY LTD	ART SUPPLIES	1,326.20
15/11/2023	99351	THE COLLAB EFFECT	COMMUNITY ENGAGEMENT	16,597.15
15/11/2023	99339	THE MARGARET RIVER NATURAL SPRING WATER	BOTTLED WATER	395.00
29/11/2023	99540	THE PLANT SUPPLY CO	NURSERY SERVICES	3,350.27
15/11/2023	99272	THE SUGAR CHEF PATISSERIE	CATERING	4,174.00
29/11/2023	99618	THE SUGAR CHEF PATISSERIE	CATERING	240.00
29/11/2023	99529	THE URBAN COFFEE HOUSE	CATERING	345.00
16/11/2023	99137	THE WESTERN AUSTRALIAN MUNICIPAL ROAD BOARD PARKS	UNION FEES	198.00
30/11/2023	99477	THE WESTERN AUSTRALIAN MUNICIPAL ROAD BOARD PARKS	UNION FEES	198.00
15/11/2023	99243	THINK WATER DUNSBOROUGH	RETICULATION SERVICES	3,293.68
29/11/2023	99603	THINK WATER DUNSBOROUGH	RETICULATION SERVICES	3,737.80
8/11/2023	99104	THOMAS ECKERT	CARPENTRY SERVICES	2,310.00
15/11/2023	99418	THOMAS MAZEY	ART SALES	122.50
29/11/2023	99674	TIFFANY BOON	CLEANING AND MAINTENANCE SERVICES	455.00
15/11/2023	99358	TIM WILSON	CARPENTRY SERVICES	14,963.08
22/11/2023	99452	TIM WILSON	CARPENTRY SERVICES	7,676.65
15/11/2023	99153	TINT A CAR BUSSELTON	MAINTENANCE SERVICES	120.00
15/11/2023	99417	TOTAL HORTICULTURAL SERVICES	LANDSCAPE MAINTENANCE	13,023.63
29/11/2023	99764	TOTAL HORTICULTURAL SERVICES	LANDSCAPE MAINTENANCE	9,653.40
15/11/2023	99422	T-QUIP	MOWER PARTS & SERVICE	254.20
29/11/2023	99776	T-QUIP	MOWER PARTS & SERVICE	331.15
29/11/2023	99765	TRADE HIRE	PLANT HIRE & EQUIPMENT	3,562.40
15/11/2023	99163	TRANEN REVEGETATION SOUTHWEST	VEGETATION REPORT	24,558.60
29/11/2023	99499	TRANEN REVEGETATION SOUTHWEST	VEGETATION REPORT	3,154.09
15/11/2023	99179	TROPHIES ON TIME	NAME BADGE SUPPLIER	462.00
15/11/2023	99193	TRUCK CENTRE (WA) PTY LTD	NEW VEHICLE PARTS & SERVICE	3,985.00
29/11/2023	99555	TRUCK CENTRE (WA) PTY LTD	NEW VEHICLE PARTS & SERVICE	3,262.21
29/11/2023	99752	TYREPOWER BUSSELTON	PLANT TYRE SUPPLIER / REPAIRER	55.00
15/11/2023	99210	TYRES & MORE DUNSBOROUGH	TYRE SALES AND SERVICE	60.00
29/11/2023	99554	UNDALUP ASSOCIATION INC	CANVAS SIGNS	2,750.00
15/11/2023	99325	UNITING GLOBAL PTY LTD	CLEANING AND MAINTENANCE SERVICES	90,861.22
29/11/2023	99660	UNITING GLOBAL PTY LTD	CLEANING AND MAINTENANCE SERVICES	17,494.68
15/11/2023	99246	URBIS PTY LTD	PROPERTY ECONOMIC & RESEARCH CONSULTING	10,780.00
15/11/2023	99297	USM EVENTS T/A IRONMAN GROUP	BOND REFUND	100.00
29/11/2023	99682	VASSE PANEL AND PAINT	PANEL BEATING AND SPRAY PAINTING	1,000.00
15/11/2023	99392	VASSE VOLUNTEER BUSH FIRE BRIGADE INC	PETTY CASH REIMBURSEMENT	75.65
15/11/2023	99208	VENDORPANEL PTY LTD	MULTI PARTY EVALUATIONS	1,877.50
15/11/2023	99364	VERITAS ENGINEERING PTY LTD	AVIATION SECURITY IDENTIFICATION CARD	338.80
22/11/2023	99435	VINCENZO & BROOKE SERAFINO	CROSSOVER CONTRIBUTION	303.60
15/11/2023	99357	VIVA ENERGY AUSTRALIA (SHELL)	FUELS, OILS AND GREASES	111,098.07
8/11/2023	99106	VOIDED	VOIDED	-
15/11/2023	99167	VORGEE PTY LTD	GYM EQUIPMENT SUPPLIER	165.00
15/11/2023	99391	W.A. HINO SALES & SERVICE	PLANT PURCHASES / SERVICES / PARTS	508.34
29/11/2023	99737	W.A. HINO SALES & SERVICE	PLANT PURCHASES / SERVICES / PARTS	893.24
29/11/2023	99497	WA EXTERNAL SOLUTIONS	GUTTER MAINTENANCE	2,970.00
29/11/2023	99484	WALGA	WALGA TRAINING SERVICES	2,420.00
29/11/2023	99522	WELARM PTY LTD	BUSSELTON GROUNDWATER INVESTIGATION	4,466.75
15/11/2023	99175	WELCOME SITE	PORTABLE TOILET HIRE	1,985.50
22/11/2023	99436	WENDY PROCTOR & WAYNE FLOOD	CROSSOVER CONTRIBUTION	418.70
15/11/2023	99306	WEST COAST WASTE	SKIP BIN HIRE	8,195.00
29/11/2023	99641	WEST COAST WASTE	SKIP BIN HIRE	4,654.32
15/11/2023	99348	WEST OZ LINEMARKING PTY LTD	LINE MARKING SERVICES	1,047.20
29/11/2023	99685	WEST OZ LINEMARKING PTY LTD	LINE MARKING SERVICES	518.10
29/11/2023	99593	WESTBOOKS	LIBRARY RESOURCES	666.13
15/11/2023	99264	WESTERN GROWERS FRESH	CATERING	220.00
29/11/2023	99614	WESTERN GROWERS FRESH	CATERING	1,508.16
15/11/2023	99181	WESTERN IRRIGATION PTY LTD	BORE AND IRRIGATION SERVICES	4,125.00
15/11/2023	99390	WESTRAC EQUIPMENT P/L	PLANT PURCHASES / SERVICES / PARTS	2,390.18
29/11/2023	99736	WESTRAC EQUIPMENT P/L	PLANT PURCHASES / SERVICES / PARTS	6,178.39
29/11/2023	99589	WHO'S ON LOCATION LIMITED	ELECTRONIC VISITOR MANAGEMENT SYSTEM	140.00



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<b>CHEQUE PAYMENTS</b>	119799 - 119817	44,842.05
<b>ELECTRONIC TRANSFER PAYMENTS</b>	99087 - 99776	8,565,634.79
<b>TRUST ACCOUNT</b>	EFT# TR000015 - TR000017	60,426.14
<b>PAYROLL</b>	01.11.2023 - 30.11.2023	1,772,286.78
<b>INTERNAL PAYMENT VOUCHERS</b>	DD #5500 - 5535	25,595,810.30
		<b>36,039,000.06</b>

29/11/2023	99670	WICKED STRATEGIES PTY LTD	COMMUNITY ENGAGEMENT	1,320.00
22/11/2023	99423	WILL OLDFIELD	STAFF REIMBURSEMENT	5,305.13
15/11/2023	99281	WILLIAM & ANNA CRERAR	BTJP REFUND	57.00
15/11/2023	99383	WINC AUSTRALIA PTY LTD	STATIONERY SUPPLIER	76.29
29/11/2023	99727	WINC AUSTRALIA PTY LTD	STATIONERY SUPPLIER	16.63
15/11/2023	99326	WOOLWORTHS GROUP LIMITED	GROCERIES	228.46
15/11/2023	99394	WORK CLOBBER	PROTECTIVE CLOTHING SUPPLIER	1,721.00
29/11/2023	99740	WORK CLOBBER	PROTECTIVE CLOTHING SUPPLIER	3,298.00
15/11/2023	99346	WORKPOWER INCORPORATED	WASTE MANAGEMENT AND RECYCLING ACTIVITIES	2,106.50
29/11/2023	99683	WORKPOWER INCORPORATED	WASTE MANAGEMENT AND RECYCLING ACTIVITIES	762.30
15/11/2023	99410	WREN OIL	WASTE OIL SERVICES	33.00
29/11/2023	99758	WREN OIL	WASTE OIL SERVICES	16.50
15/11/2023	99184	YAHAVA KOFFEE WORKS WHOLESALE	CATERING	1,542.30
29/11/2023	99537	YAHAVA KOFFEE WORKS WHOLESALE	CATERING	480.20
29/11/2023	99738	YALLINGUP RURAL BUSHFIRE BRIGADE	PETTY CASH REIMBURSEMENT	541.30
15/11/2023	99384	ZIPFORM	PRINTING SERVICES	3,048.25
				<b>7,832,634.79</b>
			<i>Plus payments under separate confidential listing:</i>	<b>733,000.00</b>
			<b>Total EFT Payments for the month:</b>	<b>8,565,634.79</b>

TRUST PAYMENTS NOVEMBER 2023				
DATE	REF #	NAME	DESCRIPTION	AMOUNT \$
16/11/2023	17	BUILDING & CONSTRUCTION INDUSTRY TRAINING	CTF LEVY	13,887.83
16/11/2023	16	CITY OF BUSSELTON - EFT	INTERNAL PAYMENTS / COMMISSIONS	944.50
16/11/2023	15	DEPARTMENT OF MINES, INDUSTRY REGULATION	BUILDING SERVICES LEVY	45,593.81
				<b>60,426.14</b>

INTERNAL PAYMENT VOUCHERS (DIRECT DEBITS) NOVEMBER 2023				
DATE	REF #	NAME	DESCRIPTION	AMOUNT \$
7/11/2023	5531	AMPOL AUSTRALIA PETROLEUM PTY LTD	FUEL SERVICES	27,877.70
		ADMINISTRATION - BSN1 - CARD #3410	UNLEADED ONLY	245.38
		ADMINISTRATION - BSN1 - CARD #1840	UNLEADED AND PREMIUM UNLEADED ONLY	305.83
		AIRPORT OPERATIONS - BSN16AV - CARD #7320	PREMIUM DIESEL ONLY	361.05
		AIRPORT OPERATIONS - BSN30BT - CARD #8470	PREMIUM DIESEL ONLY	566.92
		AIRPORT OPERATIONS - BSN55DJ - CARD #3310	PREMIUM DIESEL ONLY	77.79
		AIRPORT SMALL PLANT - CARD #1640	UNLEADED ONLY	36.95
		CEO - BSN66DG - CARD #8430	PREMIUM DIESEL ONLY	506.81
		COMMUNITY & COMMERCIAL SERVICES - BSN22AO - CARD #3030	UNLEADED ONLY	245.10
		COMMUNITY & COMMERCIAL SERVICES - BSN254L - CARD #9760	PREMIUM DIESEL ONLY	117.58
		COMMUNITY & COMMERCIAL SERVICES - BSN26CM - CARD #0920	UNLEADED ONLY	483.73
		COMMUNITY & COMMERCIAL SERVICES - BSN28AU - CARD #2011	UNLEADED ONLY	684.84
		COMMUNITY & COMMERCIAL SERVICES - BSN45CT - CARD #8880	PREMIUM DIESEL ONLY	418.09
		COMMUNITY & COMMERCIAL SERVICES - BSN45DA - CARD #7520	UNLEADED ONLY	411.48
		COMMUNITY & COMMERCIAL SERVICES - BSN53BK - CARD #3520	UNLEADED ONLY	93.05
		COMMUNITY & COMMERCIAL SERVICES - BSN57AV - CARD #9790	UNLEADED ONLY	235.02
		ENGINEERING/FACILITIES - BSN47CT - CARD #8930	PREMIUM DIESEL ONLY	128.19
		ENGINEERING/FACILITIES - BSN23AM - CARD #3500	PREMIUM DIESEL ONLY	395.45
		ENGINEERING/FACILITIES - BSN28CO - CARD #0500	PREMIUM DIESEL ONLY	575.73
		ENGINEERING/FACILITIES - BSN97CY - CARD #2450	PREMIUM DIESEL ONLY	531.10
		ENGINEERING/FACILITIES - BSN05BH - CARD #1180	PREMIUM DIESEL ONLY	228.96
		ENGINEERING/FACILITIES - BSN07BL - CARD #7150	PREMIUM DIESEL ONLY	114.55
		ENGINEERING/FACILITIES - BSN24BA - CARD #0270	PREMIUM DIESEL ONLY	245.79
		ENGINEERING/FACILITIES - BSN41AS - CARD #1700	PREMIUM DIESEL ONLY	566.26
		ENGINEERING/FACILITIES - BSN42AS - CARD #1720	PREMIUM DIESEL ONLY	573.72
		ENGINEERING/FACILITIES - BSN47BE - CARD #1660	UNLEADED ONLY	248.13
		ENGINEERING/FACILITIES - BSN47CM - CARD #0080	UNLEADED ONLY	209.61
		ENGINEERING/FACILITIES - BSN48CM - CARD #0160	UNLEADED ONLY	564.46
		ENGINEERING/FACILITIES - BSN54BG - CARD #0700	UNLEADED ONLY	223.40
		ENGINEERING/FACILITIES - BSN693T - CARD #5750	PREMIUM DIESEL ONLY	80.52
		ENGINEERING/FACILITIES - BSN89CY - CARD #2050	UNLEADED ONLY	472.95
		FINANCE/IT SERVICES - BSN20BL - CARD #5770	PREMIUM DIESEL ONLY	203.98
		FINANCE/IT SERVICES - BSN21BA - CARD #1140	UNLEADED ONLY	243.18
		FINANCE/IT SERVICES - BSN56CY - CARD #9210	PREMIUM DIESEL ONLY	1,001.34
		FINANCE/IT SERVICES - BSN75BL - CARD #2440	UNLEADED ONLY	82.19
		FINANCE/IT SERVICES - BSN75DI - CARD #1200	UNLEADED ONLY	402.01
		FINANCE/IT SERVICES - BSN82BT - CARD #3060	PREMIUM DIESEL AND DIESEL ONLY	440.89
		FLEET MANAGEMENT - BSN74AE - CARD #1820	PREMIUM DIESEL ONLY	134.57
		FLEET MANAGEMENT - BSN91CR - CARD #8890	PREMIUM UNLEADED ONLY	183.66
		GENERAL CONSTRUCTION - BSN12AH - CARD #0140	UNLEADED ONLY	99.62
		GENERAL CONSTRUCTION - BSN68BE - CARD #5580	PREMIUM DIESEL ONLY	132.16
		GENERAL CONSTRUCTION - BSN75BA - CARD #3170	PREMIUM DIESEL ONLY	495.66
		PARKS, GARDENS & RESERVES - BSN022S - CARD #7030	PREMIUM DIESEL ONLY	484.04
		PARKS, GARDENS & RESERVES - BSN02DG - CARD #8130	PREMIUM DIESEL ONLY	231.32
		PARKS, GARDENS & RESERVES - BSN17AY - CARD #4340	PREMIUM DIESEL ONLY	555.34
		PARKS, GARDENS & RESERVES - BSN18AY - CARD #5950	PREMIUM DIESEL ONLY	283.09
		PARKS, GARDENS & RESERVES - BSN32CJ - CARD #5760	PREMIUM DIESEL ONLY	274.75
		PARKS, GARDENS & RESERVES - BSN353S - CARD #2760	PREMIUM DIESEL ONLY	181.73
		PARKS, GARDENS & RESERVES - BSN51AU - CARD #3080	PREMIUM DIESEL ONLY	774.04
		PARKS, GARDENS & RESERVES - BSN614Q - CARD #4550	PREMIUM DIESEL ONLY	575.57
		PARKS, GARDENS & RESERVES - BSN61BF - CARD #3370	PREMIUM DIESEL ONLY	256.07
		PARKS, GARDENS & RESERVES - BSN64BC - CARD #8490	PREMIUM DIESEL ONLY	296.68
		PARKS, GARDENS & RESERVES - BSN694T - CARD #7990	PREMIUM DIESEL ONLY	220.58
		PARKS, GARDENS & RESERVES - BSN762T - CARD #7240	PREMIUM DIESEL ONLY	329.45
		PARKS, GARDENS & RESERVES - BSN77BC - CARD #8160	PREMIUM DIESEL ONLY	385.82
		PARKS, GARDENS & RESERVES - BSN86BX - CARD #2930	PREMIUM DIESEL ONLY	443.71
		PARKS, GARDENS & RESERVES - BSN87AV - CARD #7840	PREMIUM DIESEL ONLY	416.38
		PARKS, GARDENS & RESERVES - BSN993T - CARD #5120	PREMIUM DIESEL ONLY	294.36
		PARKS, GARDENS & RESERVES - LAWNMOWER - CARD #0240	UNLEADED ONLY	29.24
		PARKS, GARDENS & RESERVES - TRACTOR - CARD #4150	UNLEADED ONLY	85.55
		PLANNING & BUILDING - BSN00CK - CARD #1440	PREMIUM UNLEADED AND UNLEADED ONLY	435.99



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<b>CHEQUE PAYMENTS</b>	119799 - 119817	44,842.05
<b>ELECTRONIC TRANSFER PAYMENTS</b>	99087 - 99776	8,565,634.79
<b>TRUST ACCOUNT</b>	EFT# TR000015 - TR000017	60,426.14
<b>PAYROLL</b>	01.11.2023 - 30.11.2023	1,772,286.78
<b>INTERNAL PAYMENT VOUCHERS</b>	DD #5500 - 5535	25,595,810.30
		<b>36,039,000.06</b>

		PLANNING & BUILDING - BSN01BE - CARD #8790	UNLEADED ONLY	365.46
		PLANNING & BUILDING - BSN08AO - CARD #5830	UNLEADED ONLY	32.29
		PLANNING & BUILDING - BSN20AO - CARD #2970	UNLEADED ONLY	337.25
		PLANNING & BUILDING - BSN53BG - CARD #0410	UNLEADED ONLY	356.51
		PLANNING & BUILDING - BSN56BX - CARD #2130	PREMIUM UNLEADED ONLY	339.34
		PLANNING & BUILDING - BSN61BG - CARD #0770	PREMIUM UNLEADED AND UNLEADED ONLY	414.55
		PROPERTY & LEGAL SERVICES - BSN64CT - CARD #7040	UNLEADED ONLY	255.03
		PROPERTY & LEGAL SERVICES - BSN75DG - CARD #7700	UNLEADED ONLY	222.57
		RANGER & FIRE SERVICES - BSN76BC - CARD #5760	PREMIUM DIESEL ONLY	366.46
		RANGER & FIRE SERVICES - BSN91BO - CARD #9320	PREMIUM DIESEL ONLY	415.97
		RANGER & FIRE SERVICES - 1CRZ008 - CARD #8300	DIESEL ONLY	75.62
		RANGER & FIRE SERVICES - 1EIO341 - CARD #9420	DIESEL ONLY	285.31
		RANGER & FIRE SERVICES - 1EZK373 - CARD #2000	PREMIUM DIESEL AND DIESEL ONLY	383.33
		RANGER & FIRE SERVICES - BSN01AO - CARD #5220	UNLEADED ONLY	323.20
		RANGER & FIRE SERVICES - BSN70BZ - CARD #6100	PREMIUM DIESEL ONLY	361.72
		RANGER & FIRE SERVICES - BSN71OK - CARD #0020	PREMIUM DIESEL ONLY	367.37
		RANGER & FIRE SERVICES - BSN75BC - CARD #5750	PREMIUM DIESEL ONLY	228.94
		RANGER & FIRE SERVICES - BSN90BO - CARD #9300	PREMIUM DIESEL ONLY	735.20
		REGULATORY SERVICES - BSN25CM - CARD # 4830	UNLEADED ONLY	213.21
		REGULATORY SERVICES - BSN91DA - CARD #1680	PREMIUM DIESEL ONLY	263.50
		REGULATORY SERVICES - BSN97BT - CARD #1690	UNLEADED ONLY	327.54
		WASTE SERVICES - 1GUG884 - CARD #6770	UNLEADED ONLY	149.94
		WASTE SERVICES - BSN09AQ - CARD #1300	PREMIUM DIESEL ONLY	405.20
		WASTE SERVICES - BSN34BG - CARD #3370	PREMIUM DIESEL ONLY	564.43
		WASTE SERVICES - BSN85CC - CARD #3630	PREMIUM DIESEL ONLY	166.35
6/11/2023	5513	ANZ BANK	BANK FEES	938.60
6/11/2023	5514	ANZ BANK	BANK FEES	2,583.06
6/11/2023	5515	ANZ BANK	BANK FEES	192.39
6/11/2024	5516	ANZ BANK	BANK FEES	281.33
1/11/2023	5517	ANZ BANK	BANK FEES	15.00
1/11/2023	5518	ANZ BANK	BANK FEES	10,005.17
6/11/2023	5519	ANZ BANK	BANK FEES	0.30
9/10/2023	5535	ANZ BANK	VISA PAYMENT	<b>19,652.72</b>
		<b>CEO</b>		
		BAKED BUSSELTON	BEVERAGES FOR ELECTION STAFF COUNT	118.30
		BANKSIA TAVERN	DINNER FOR COUNCILLORS AND ELT	501.50
		DAN MURPHY'S	BEVERAGES FOR MAYOR'S FINAL COUNCIL DINNER	70.99
		DOMINOS	CATERING FOR ELECTION STAFF COUNT	86.57
		KMART PHOTO CENTRE	MAYOR FAREWELL GIFT	32.20
		LG PROFESSIONALS	COMMTELLIGENCE FORUM 2023 REGISTRATIONS	760.00
		MAILCHIMP	ELECTRONIC NEWSLETTER PR	387.04
		SLIMLINE WAREHOUSE	MOBILE LAPTOP STAND FOR TRAINING PURPOSES	385.35
		WEST AUSTRALIAN	DIGITAL SUBSCRIPTION	28.00
		<b>COMMUNITY SERVICES MANAGER</b>		
		BUNNINGS	STOWAGE AND BATTERIES FOR CESM	56.92
		DOMINOS	CATERING- YALLINGUP BRASH RD FIRE	270.00
		DOMINOS	CATERING- BFB VEHICLE TRAINING	380.00
		DOMINOS	CATERING- YALLINGUP BRASH RD FIRE	600.00
		<b>ECONOMIC &amp; BUSINESS DEVELOPMENT DIRECTOR</b>		
		ALIA-ONLINE CONFERENCE	GREENING LIBRARY CONFERENCE	175.00
		AUSTRALIAN FINANCIAL SECURITY AUTHORITY	BANKRUPTCY SEARCH	15.00
		AUSTRALIAN FINANCIAL SECURITY AUTHORITY	BANKRUPTCY SEARCH	15.00
		CALLOWS	STAFF FAREWELL CARD	6.99
		DOUBLE TREE PERTH	TRAINING ACCOMODATION	569.00
		FITNESS AUST	AUSACTIVE ANNUAL MEMBERSHIP	699.00
		KMART	PURCHASES FOR BJTP	269.00
		KMART	PURCHASES FOR BJTP	97.00
		KMART	PURCHASES FOR BJTP	92.00
		KMART	PURCHASES FOR BJTP	72.50
		RINGTAILS MOTEL BUSSELTON	ACCOMODATION FOR AIRPOT REPORTING OFFICER	484.80
		RINGTAILS MOTEL BUSSELTON	ACCOMODATION FOR AIRPOT REPORTING OFFICER	120.00
		VIVA BOOKS	STAFF FAREWELL GIFT	140.00
		WESTERN GROWERS	STAFF FAREWELL CATERING	49.39
		<b>ENGINEERING DIRECTOR</b>		
		DWER - WATER PERTH	TRANSFER FEE ASSOCIATED WITH AGR 157168 ( PROVENCE ESTATE)	200.00
		DWER - WATER PERTH	238 NATURALISTE TCE	440.00
		LAW SOCIETY OF WA PERTH	FORMS	10.00
		MAILCHIMP	ARTGEO NEWSLETTER AND MARKETING	112.85
		RAW HABITAT MARGARET RIVER	SYMPATHY FLOWERS	112.00
		SOFAR OCEAN	SUBSCRIPTION OF THE SOFAR - SPOT BUSSELTON JETTY	1,644.09
		THE INSTITUTION OF ENGINEERS AUST.	CHARTERED APPLICATION & ASSESSMENT FEE	1,760.00
		WA GOVERNMENT - DMIRS CANNINGTON	APPLICATION TO ADD A CLASS TO HIGH RISK LICENCE- FORKLIFT	43.00
		<b>FINANCE DIRECTOR</b>		
		BENCHMARK ENGRAVING AN PENROSE	GAVEL AND DISPLAY STAND	143.00
		DEPARTMENT OF JUSTICE BUSSELTON	APPLICATION FOR LIQUOR LICENCE- MAYOR HENLEY'S FAREWELL	57.00
		DYNAMIC METHODS PTY LTD	REIWA- CONTRACT OD FAE DOCUMENTS	30.00
		LG PROFESSIONALS	PEOPLE AND CULTURE NETWORK- A LEADERSHIP PERSPECTIVE	70.00
		LOCAL GOVERNMENT PROFESSIONALS	ANNUAL STATE CONFERENCE	1,450.00
		LOCAL GOVERNMENT PROFESSIONALS	ANNUAL STATE CONFERENCE	1,450.00
		MAILCHIMP	ECONOMIC AND BUSINESS DEVELOPMENT NEWSLETTER	43.51
		MRBTA	STAFF AT MAYORS LEAVING FUNCTION	120.00
		PULLMAN ALBERT PARK	ACCOMMODATION -ACCA ANNUAL CONFERENCE	1,216.80
		THE FLOWER PLACE	STAFF CARE PACKAGE	75.00
		THE FLOWER PLACE	POLICE REMEMBRANCE DAY WREATH	90.00
		<b>HUMAN RESOURCES MANAGER</b>		
		MAILCHIMP	ELECTRONIC NEWLETTER BUSSELTON LIBRARY	122.99



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		<b>36,039,000.06</b>

		SENDGRID	EMAIL PROGRAM FOR CUSTOMER RETENTION AND CONTACT-GLC	32.69
		SNAPPFISH	FARWELL GIFT- P.NEEDHAM	160.92
		SPOTIFY	YOUTH SERVICE MUSIC	20.99
		<b>IT MANAGER</b>		
		ALIBABA	USB TESTER -TECH KIT	141.78
		ALIEXPRESS	USB ADAPTER	24.87
		ALIEXPRESS	WIFI POWER METERS	52.57
		ALIEXPRESS	PHONE CASES & SCREEN PROTECTORS	82.34
		AMAZON MARKETPLACE	USB CORDS	26.98
		AMAZON MARKETPLACE	GAN CHARGER AND DOCKING STATION	172.72
		AMAZON MARKETPLACE	ITEMS FOR TECH KITS	180.48
		AMAZON MARKETPLACE	USB ADAPTER	27.54
		AMAZON MARKETPLACE	USB ADAPTER	42.09
		AMAZON MARKETPLACE	TOUCHPAD & KEYBOARD	195.26
		AMAZON MARKETPLACE	USB CABLES	25.98
		AMAZON MARKETPLACE	ANTI GLARE SCREEN PROTECTOR	47.82
		AMAZON MARKETPLACE	AIR TAG KEYCHAIN & PROTECTIVE CASE COVER	115.20
		AMAZON MARKETPLACE	USB TO ETHERNET ADAPTER	74.05
		AMAZON MARKETPLACE	BATTERIE PACK & VELCRO TAPE	34.98
		AMAZON MARKETPLACE	HDMI CABLES	11.89
		AMAZON MARKETPLACE	USB EXTENTIONS	223.86
		AMAZON MARKETPLACE	ITEMS FOR TECH KITS-REFUND	-
		BUNNINGS	CABLING ACCESSORIES AND POWERBORDS	49.62
		CRAZY DOMAINS	DOMIAIN RENEWALS - BJTP	168.81
		DEPUTY	DEPUTY ROSTERING SYSTEM GLC	997.48
		GO DADDY	DOMAIN RENEWALS	407.68
		GO DADDY	DOMAIN RENEWALS	279.75
		ZOOM	MONTHLY SUBSCRIPTION	69.19
		<b>DEVELOPMENT MANAGER</b>		
		CALLOWS CORNER NEWS BUSSELTON	STAFF FAREWELL CARDS	54.95
		THE BUTCHER SHOP	YOUTH URBAN ART ACTIVITY SUPPLIES	277.70
		BOFFINS BOOKSHOP	STAFF FAREWELL GIFT VOUCHER	50.00
		BAKERS DELIGHT	STAFF FAREWELL MORNING TEA	14.90
		COLES	STAFF FAREWELL MORNING TEA	75.00
		TRYBOOKING-WALGA	TRAINING CANCELLATION	-
		TRYBOOKING-WALGA	TRAINING CANCELLATION	-
		TRYBOOKING-WALGA	TRAINING CANCELLATION	-
8/11/2023	5512	AUSTRALIAN TAXATION OFFICE	PAYG TAXATION	106.08
24/11/2023	5532	B HUTCHINSON	REFUND OF BOND	200.00
24/11/2023	5527	B SIMON	REFUND OF RATE OVERPAYMENT	2,338.05
9/11/2023	5508	BANK OF QUEENSLAND	OPENING OF TERM DEPOSIT - BOQ	4,000,000.00
6/11/2023	5500	COBU DESIGNS	REFUND OF APPLICATION FEE	1,440.00
9/11/2023	5507	COMMONWEALTH BANK OF AUSTRALIA	OPENING OF TERM DEPOSIT - CBA	5,000,000.00
9/11/2023	5507	COMMONWEALTH BANK OF AUSTRALIA	OPENING OF TERM DEPOSIT - CBA	6,000,000.00
15/11/2023	5520	COMMONWEALTH BANK OF AUSTRALIA	BANK FEES	26.81
3/11/2023	5521	COMMONWEALTH BANK OF AUSTRALIA	BANK FEES	849.41
13/11/2023	5522	CR. MIKAYLA LOVE	SUPERANNUATION	348.96
6/11/2023	5501	CULTURE COUNTS	REFUND OF BOND	1,000.00
7/11/2023	5502	D & K CURNUCK	REFUND OF APPLICATION FEE	147.00
30/11/2023	5534	D COWAN	REFUND OF RATE OVERPAYMENT	124.71
28/11/2023	5530	D RUMMER & K BRADEN	REFUND OF RATE OVERPAYMENT	1,196.48
24/11/2023	5532	DALE ALCOCK HOMES SOUTH WEST	REFUND OF APPLICATION FEE	1,748.15
6/11/2023	5500	E & J WATTS	REFUND OF RATE OVERPAYMENT	900.92
10/11/2023	5511	G ROWE	REFUND OF RATE OVERPAYMENT	1,996.67
22/11/2023	5526	GEO BAY WILDLIFE RESCUE	REFUND OF ANIMAL TRAP BOND	100.00
29/11/2023	5533	H CEAMS	REFUND OF RATE OVERPAYMENT	47.52
8/11/2023	5503	J & A D'ESEISSIS	REFUND OF RATE OVERPAYMENT	438.35
6/11/2023	5501	J & S PHILLIPS	REFUND OF RATE OVERPAYMENT	3,342.35
10/11/2023	5511	K BADGER	REFUND OF APPLICATION FEE	1,804.65
17/11/2023	5524	KULD CREAMERY	REFUND OF APPLICATION FEE	3,471.00
1/11/2023	5504	LES MILLS ASIA PACIFIC	CONTRACT FEES	534.94
1/11/2023	5505	LES MILLS ASIA PACIFIC	CONTRACT FEES	641.93
6/11/2023	5500	M ALDRIDGE & A MCDONALD	REFUND OF RATE OVERPAYMENT	200.00
10/11/2023	5510	M COOK	REFUND OF APPLICATION FEE	118.00
6/11/2023	5500	M NORTJE	REFUND OF RATE OVERPAYMENT	1,959.03
17/11/2023	5524	M SIMS	REFUND OF RATE OVERPAYMENT	1,702.50
27/11/2023	5529	N LUDLOW	REFUND OF RATE OVERPAYMENT	2,636.94
9/11/2023	5506	NATIONAL AUSTRALIA BANK	OPENING OF TERM DEPOSIT - NAB	2,500,000.00
9/11/2023	5506	NATIONAL AUSTRALIA BANK	OPENING OF TERM DEPOSIT - NAB	3,000,000.00
24/11/2023	5527	PARWISE CORP	REFUND OF RATE OVERPAYMENT	2,609.27
17/11/2023	5525	R CHAMBERS	REFUND OF ANIMAL TRAP BOND	100.00
6/11/2023	5501	RIVER & VINES	REFUND OF BOND	500.00
16/11/2023	5523	S BROWNE	REFUND OF RATE OVERPAYMENT	393.31
21/11/2023	5528	SHEDFORCE	REFUND OF APPLICATION FEE	147.00
24/11/2023	5532	SHEDFORCE	REFUND OF APPLICATION FEE	147.00
24/11/2023	5527	SURFER PRODUCTIONS	REFUND OF APPLICATION FEE	75.00
21/11/2023	5528	T MAHON	REFUND OF RATE OVERPAYMENT	872.00
10/11/2023	5509	WESTPAC BANKING CORPORATION	OPENING OF TERM DEPOSIT - WBC	5,000,000.00
				<b>25,595,810.30</b>

PAYROLL PAYMENTS NOVEMBER 2023				
DATE	REF #	NAME	DESCRIPTION	AMOUNT \$
14/11/2023	14/11/2023	CITY OF BUSSELTON	PAYROLL & SALARIES	869,228.74
28/11/2023	28/11/2023	CITY OF BUSSELTON	PAYROLL & SALARIES	903,058.04
				<b>1,772,286.78</b>



## Terms of Reference

**Committee:** Audit and Risk  
**Responsible Directorate:** Corporate Strategy and Performance **Version:** Adopted

### 1. INTRODUCTION

- 1.1. The Council of the City of Busselton has established an Audit and Risk Committee (the Committee) pursuant to Part 7 of the *Local Government Act 1995* (the Act).
- 1.2. The Committee is established to fulfil the requirements of Part 6, Financial Management and Part 7, Audit of the Act.
- 1.3. The Committee provides appropriate advice and recommendations to the Council on matters relevant to its terms of reference.
- 1.4. The Committee shall act in accordance with the provisions of the Act, the local laws and policies of the City of Busselton and these Terms of Reference.

### 2. OBJECTIVES, DUTIES AND RESPONSIBILITIES

- 2.1 The Committee operates to assist Council to fulfil its corporate governance, stewardship, leadership and control responsibilities in relation to financial reporting and audit, internal audit and risk management.
- 2.2 The primary objective of the Committee is to accept responsibility for the annual external audit and liaise with the local government's auditor so that Council can be satisfied with the local government's compliance with the relevant sections of the Act, in managing its financial affairs.
- 2.3 The specific duties and responsibilities of the Committee are to:
  - a. Provide guidance and assistance to the Council as to the carrying out the functions of the local government in relation to audits and financial management;
  - b. Meet with the external auditor at least once in each year.
  - c. Liaise with the CEO to ensure that the local government does everything in its power to:
    - i. assist the auditor to conduct the audit and carry out his or her other duties under the Local Government Act 1995; and
    - ii. ensure that audits are conducted successfully and expeditiously;
  - d. Examine the reports of the auditor after receiving a report from the CEO to:
    - i. determine if any matters raised require action to be taken by the local government; and
    - ii. be satisfied that the CEO has taken appropriate action in respect of those matters;
  - e. Review the report prepared by the CEO on any actions taken in respect of any matters raised in the report of the auditor and presenting the report to council for adoption prior to the end of the next financial year or 6 months after the last report prepared by the auditor is received, whichever is the latest in time;
  - f. Consider and recommend adoption of the annual financial report to the Council;
  - g. Address issues brought to the attention of the Committee in relation to the external audit, including responding to requests from the Council for advice that are within the parameters of the committee's terms of reference;

- h. Review the scope of audit plans and programs and their effectiveness as well as the appropriateness of special internal audit assignments undertaken by internal audit at the request of the Council or CEO;
- i. Review reports of internal audits and monitor the implementation of recommendations made by internal audit;
- j. Review the annual Compliance Audit Return, consider the report from the CEO and recommend to the Council the results of that review;
- k. Review the scope of internal and external audit to promote compatibility, to the extent appropriate, between each audit program;
- l. Assist the Council to ensure that the City is taking a sound approach to risk management, by monitoring and assuring that the City has implemented an appropriate risk management framework;
- m. Oversee periodic reviews of the City's risk management framework;
- n. Review and provide advice to the Council on the management of the City's strategic risk profile as required;
- o. Receive and consider reports from officers relating to significant strategic risks;
- p. Review the level of resources allocated to internal audit and risk management;
- q. Assist Council in ensuring that the City is taking a sound approach to fraud and corruption detection, by monitoring and assuring there are appropriate processes and systems in place to detect, capture and effectively respond to fraud and improper activities;
- r. Consider the CEO's triennial reviews of the appropriateness and effectiveness of the local government's financial management systems and procedures and report to the Council the results of those reviews;
- s. Oversee the implementation of any action required following the receipt of a review into the appropriateness and effectiveness of the local government's financial management systems and procedures;
- t. Consider the CEO's triennial reviews of the appropriateness and effectiveness of the local government's systems and procedures in regard to risk management, internal control and legislative compliance, and report to the Council the results of those reviews; and
- u. Oversee the implementation of any action required following the receipt of a review into the appropriateness and effectiveness of the local government's systems and procedures in regard to risk management, internal control and legislative compliance.

### **3. POWERS OF THE AUDIT AND RISK COMMITTEE**

- 3.1. The Committee is to report to the Council and provide appropriate advice and recommendations on matters relevant to its terms of reference to facilitate informed decision making by the Council.
- 3.2. The Committee is a formally appointed committee of the Council.
- 3.3. The Committee does not have executive powers or authority to implement actions in areas over which the CEO has legislative responsibility and does not have delegated financial responsibility.
- 3.4. The Committee does not have any management functions and cannot involve itself in management processes and procedures.

### **4. MEMBERSHIP**

- 4.1. The Committee will consist of:
  - a. between three and nine elected members; and
  - b. one independent external member.
- 4.2. The independent external member will be selected based on criteria which includes the following:
  - a. A suitably qualified person with demonstrated expertise and knowledge in financial management, risk management, governance and audit (internal and external);
  - b. Understanding of the duties and responsibilities of the position; ideally with respect to local government financial reporting and auditing requirements;
  - c. Strong communication skills; and
  - d. Relevant skills and experience in providing independent expert advice.

- 4.3. An independent external member will be a person with no operational responsibilities with the City nor will that person provide any other paid services to the City either directly or indirectly.
- 4.4. Appointment and re-appointment of independent external members will be made by Council after consideration of the CEO's recommendation. The applications of independent external members will be sought through an open and transparent process. The evaluation of potential members will be reviewed by the CEO and appointments will be approved by the Council.
- 4.5. An independent external member may only be appointed for a maximum of three consecutive terms.
- 4.6. Independent external members will be required to complete a confidentiality agreement and confirm that they will operate in accordance with the City's code of conduct. They will also be required to follow any City policies pertaining to the Committee operations.
- 4.7. Meeting attendance fees and reimbursement of approved expenses may be paid to an independent external member in accordance with s 5.100 of the Act.
- 4.8. Meeting attendance fees for an independent external member are to be the top of the relevant range for a band 1 local government in the most current Salaries and Allowances Tribunal determination for Local Government Chief Executive Officers and Elected Members
- 4.9. Council may appoint up to two elected members as deputy members of the Committee.
- 4.10. Membership of the Committee will, unless determined otherwise, cease on the day of the next ordinary Council election.
- 4.11. The membership of a member may be ceased in accordance with the Act.

## 5. PRESIDING MEMBER

- 5.1. The Committee will appoint a Presiding Member and Deputy Presiding Member to conduct its business.

## 6. MEETINGS

- 6.1. The Committee will meet at least four times per year.
- 6.2. The Committee will report to Council in accordance with clause 2.10 of the *City of Busselton Standing Orders Local Law 2018*.
- 6.3. Notice of meetings will be given to members at least five days prior to each meeting, with the agenda papers to be provided to members not less than 72 hours prior to the meeting.
- 6.4. The Presiding Member will ensure that detailed minutes of all meetings are kept in accordance with the Part 14 of the *City of Busselton Standing Orders Local Law 2018*.
- 6.5. All members of the Committee will have one vote. If the vote of the members present is equally divided, the Presiding Member will cast a second vote.
- 6.6. Where the Committee makes a recommendation to the Council, the Chief Executive Officer will ensure that the recommendation is on the agenda of the next practicable ordinary Council meeting.

## 7. QUORUM

- 7.1. The quorum for a meeting will be at least 50% of the number of offices of membership, whether vacant or not.

**8. DELEGATED POWERS**

- 8.1. Pursuant to section 5.17 of the Act, the Committee is delegated the powers to conduct the formal meeting with the City’s external Auditor required by section 7.12(A)(2) on behalf of the local government.
- 8.2. In all other matters, Committee recommendations are not binding on Council and must be endorsed by Council to take effect.

**9. EVALUATION OF COMMITTEE’S EFFECTIVENESS**

- 9.1. At the meeting of the Council prior to the ordinary Council elections, officers will present for Council’s consideration a review as to the effectiveness and ongoing relevance of the Committee.

**10. APPROVAL**

<b>Council Adoption</b>	<b>DATE</b>	31 January 2024	<b>Resolution #</b>	
<b>Previous Adoption</b>	<b>DATE</b>	21 June 2023	<b>Resolution #</b>	C2306/110



# Expression of Interest

Independent External Member – Audit and Risk Committee

**Expressions of Interest close 4:00pm (AWST) 20 February 2024**



## Background

The City of Busselton ('the City') is a local government situated in the lower south west region of Western Australia covering an area of 1,454 square kilometres. The City is part of the Noongar country that is called the Wadandi Boodja. The City has approximately 43,000 residents who are represented by the Council.

The Council comprises the Mayor and eight other elected members with council meetings held monthly. The *Local Government Act 1995* requires that all local governments have an audit committee. The City has established an Audit and Risk Committee whose membership currently consists of all elected members.

Having at least one external member on an audit committee has been a long-established practice in other states and local governments in Western Australia have increasingly included independent members on their audit committees.

At its meeting on 29 November 2023, Council resolved to amend the terms of reference for the Audit and Risk Committee to provide for one independent external member to be appointed to the committee, which will take the total number of the committee members to ten. The independent external member will be appointed for a period of up to two years with the term ending on the day of the next ordinary Council election.

## Legislative Framework

The principal legislation is the Local Government Act 1995 (the Act). There are several subsidiary regulations which apply to the purpose and function of audit committees. The annual budgeting process, financial accounting, management and reporting of municipal and trust funds, and the requirements for rates setting and land valuation are set out in Part 6 of the Act.

The requirement to have an audit committee is set out in Part 7 of the Act which also covers the essential requirements for appointment of auditors and conducting audits. This part was expanded significantly in 2017 to allow for financial and performance audits to be done by the Office of Auditor General (OAG). The detailed requirements for appointing auditors, developing an audit plan and conducting and reviewing audits are dealt with under the Local Government (Audit) Regulations 1996.

## Obligations and Entitlements

It is essential that nominees can commit to the full term of the Committee.

### Meeting schedule

The Committee meets at least four times a year under its Terms of Reference, and each meeting lasts about two hours. A schedule of meetings is adopted for each calendar year. As a guide, at least two hours should be allowed to consider the matters in the agenda, prior to the meeting.

### Term of office

Pursuant to the Local Government Act an independent committee member should be appointed for a term expiring on the next ordinary election day. This means that the term will cease on the election date in October 2025.

### Code of Conduct

All committee members must abide by the Code of Conduct for Elected Members, Committee Members and Candidates.



### **Terms of Reference**

Independent committee members must work within the scope of the Audit and Risk Committee's Terms of Reference.

### **Fees and expenses**

The Local Government Act 1995 allows for payment of an attendance fee to Committee members. For a Band 1 local government the permitted attendance fee range is from \$0 up to \$415. Reimbursement of reasonable expenses is also permissible in accordance with the Local Government (Administration) Regulations 1996 and the applicable Salaries and Allowances Tribunal determination. It is recommended that attendance fees be set at the top of the range (subject to Council approval).

## **Expression of Interest and Selection Criteria**

The City of Busselton is seeking nominations for one independent external member to join our Audit and Risk Committee.

Nominations from independent external members to join the City's Audit Committee will be for a term commencing from the date of appointment until 18 October 2025 (being the date of the next local government ordinary election).

Nominations open on 1 February 2024 and close at 4:00pm (AWST) 20 February 2024.

The selection criteria for the independent external member:

1. A suitably qualified person with demonstrated expertise and knowledge in financial management, risk management, governance and audit (internal and external);
2. Understanding of the duties and responsibilities of the position; ideally with respect to local government financial reporting and auditing requirements;
3. An appreciation of the values of the City, its core activities and the strategic direction as set out in the Strategic Community Plan;
4. Strong communication skills; and
5. Relevant skills and experience in providing independent expert advice.

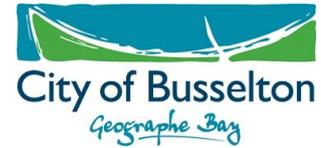
Nominees for the independent external member must have no operational responsibilities with the City and must not provide paid services to the City either directly or indirectly.

The successful respondent will be required to complete a confidentiality agreement and confirm that they will adhere to the City of Busselton Code of Conduct for Elected Members, Committee Members and Candidates.

## **Evaluation Process**

After the Expression of Interest period closes, nominations will be reviewed internally. The internal review may require the City to see clarifications from the nominee. An assessment will be made by the CEO and a report will be prepared for the Council recommending an appointment to the Audit and Risk Committee. This is likely to be in March 2024.

Everyone who nominates will be notified when the report is due to be considered by Council. Members of the public are welcome to attend the Council briefing and Council meeting to ask questions or comment.



## How to lodge your Expression of Interest

Applicants are requested to submit:

1. An expression of interest addressing the selection criteria and outlining relevant skills and experience; and
2. a current CV.

Responses to the Expression of Interest must be received by **4.00 pm (AWST) Tuesday 20 February 2024**. All responses should be marked for the attention of **Sarah Pierson, Director Corporate Strategy and Performance**.

Electronic responses may be lodged via the City's corporate email address [city@busselton.wa.gov.au](mailto:city@busselton.wa.gov.au). Responses may also be hand delivered to the City Administration Building at 2 Southern Drive, Busselton or posted to City of Busselton, Locked Bag 1 Busselton WA 6280.

## Contact for further information

For further information about this Expression of Interest please contact:

**Ben Whitehill**  
**Manager Legal and Governance**  
(08) 9781 1752  
[ben.whitehill@busselton.wa.gov.au](mailto:ben.whitehill@busselton.wa.gov.au)



## Terms of Reference

**Committee:** Audit and Risk  
**Responsible Directorate:** Finance and Corporate Services  
**Version:** Adopted

### 1. INTRODUCTION

- 1.1. The Council of the City of Busselton has established an Audit and Risk Committee (the Committee) pursuant to Part 7 of the *Local Government Act 1995* (the Act).
- 1.2. The Committee is established to fulfil the requirements of Part 6, Financial Management and Part 7, Audit of the Act.
- 1.3. The Committee operates to assist Council to fulfil its corporate governance, stewardship, leadership and control responsibilities in relation to financial reporting and audit, internal audit and risk management.
- 1.4. The Committee shall act in accordance with the provisions of the Act, the local laws and policies of the City of Busselton and these Terms of Reference.

### 2. OBJECTIVE

- 2.1 The objectives of the Committee are:
  - a. to monitor the annual external audit and to liaise with the local government's auditor so that Council can be satisfied with the performance of the local government in managing its financial affairs;
  - b. to ensure openness in the local government's financial reporting and to liaise with the Chief Executive Officer (or their representative) to ensure the effective and efficient management of the local government's financial accounting systems and compliance with legislation;
  - c. to facilitate:
    - i. the enhancement of the credibility and objectivity of external financial reporting;
    - ii. compliance with laws and regulations as well as use of best practice guidelines relative to auditing;
    - iii. the provision of an effective means of communication between the external auditor, the Chief Executive Officer (or their representative) and the Council; and
    - iv. the effective conduct of internal audit and risk management activities.
  - d. to monitor the implementation of an effective and appropriate risk management system and framework.
- 2.2 The following specific duties and responsibilities have been given to the Committee in order to facilitate the achievement of its objectives:
  - a. to provide guidance and assistance to Council as to the carrying out of the functions of the local government in relation to audits;
  - b. to review the report prepared by the Chief Executive Officer on any actions taken in respect of any matters raised in the report of the auditor and to present the report to Council for adoption prior to the end of the next financial year or six months after the last report prepared by the auditor is received, whichever is the latter;
  - c. to formally meet with the auditor in accordance with the requirements of Section 7.12(A)(2) of the Act on behalf of the local government;

- d. to review the compliance audit return and report to the Council the results of that review in accordance with Audit Regulation 14(3A);
- e. to receive reports from the CEO relating to the appropriateness and effectiveness of the local government's systems and procedures in relation to risk management, internal control and legislative compliance and to report to Council the results of the review in accordance with Audit Regulation 16(c); and
- f. to periodically receive reports from officers relating to significant strategic and/or medium rated risks.

### 3. MEMBERSHIP

- 3.1. The Council shall appoint four elected members to the Committee.
- 3.2. Council shall appoint two elected members as deputy members of the Committee.
- 3.3. Membership of the Committee shall, unless determined otherwise, cease on the day of the next ordinary Council election.
- 3.4. Council shall appoint new members to the Committee, in accordance with paragraphs 3.1 and 3.2, at the next ordinary Council meeting following the Council election.
- 3.5. The membership of a member may be ceased in accordance with the Act.

### 4. PRESIDING MEMBER

- 4.1. The Committee shall appoint a Presiding Member and Deputy Presiding Member to conduct its business.

### 5. MEETINGS

- 5.1. The Committee shall meet at least two times per year.
- 5.2. The Committee shall report to Council in accordance with clause 2.10 of the *City of Busselton Standing Orders Local Law 2018*.
- 5.3. Notice of meetings shall be given to members at least five days prior to each meeting, with the agenda papers to be provided to members not less than 72 hours prior to the meeting.
- 5.4. The Presiding Member shall ensure that detailed minutes of all meetings are kept in accordance with the Part 14 of the *City of Busselton Standing Orders Local Law 2018*.
- 5.5. All members of the Committee shall have one vote. If the vote of the members present is equally divided, the Presiding Member shall cast a second vote.
- 5.6. Where the Committee makes a recommendation to the Council, the Chief Executive Officer shall ensure that the recommendation is on the agenda of the next practicable ordinary Council meeting.

### 6. QUORUM

- 6.1. The quorum for a meeting shall be at least 50% of the number of offices of membership, whether vacant or not.

### 7. DELEGATED POWERS

- 7.1. Pursuant to section 5.17 of the Act, the Committee is delegated the powers to:
  - a. conduct the formal meeting with the City's external Auditor required by section 7.12(A)(2) on behalf of the local government; and
  - b. schedule and endorse the dates, times and locations of the meetings held by the Committee.

7.2. In all other matters, Committee recommendations are not binding on Council and must be endorsed by Council to take effect.

**8. EVALUATION OF COMMITTEE'S EFFECTIVENESS**

- 8.1. At the meeting of the Committee prior to the ordinary Council elections, the Committee shall undertake a review to evaluate its effectiveness, and as a guide the following should be determined:
- a. are the Committee's purpose and objectives being met?
  - b. should the purpose and objectives be amended?
  - c. is the Committee functioning effectively and as per the requirements of the Act?
  - d. are the Committee members regularly attending meetings?
  - e. review the Terms of Reference, including membership.

**9. APPROVAL**

<b>Council Adoption</b>	<b>DATE</b>	8 September 2021	<b>Resolution #</b>	C2109/201
<b>Previous Adoption</b>	<b>DATE</b>	25 September 2019	<b>Resolution #</b>	C1909/191



## Terms of Reference

**Committee:** Audit and Risk  
**Responsible Directorate:** Corporate Strategy and Performance **Version:** Adopted

### 1. INTRODUCTION

- 1.1. The Council of the City of Busselton has established an Audit and Risk Committee (the Committee) pursuant to Part 7 of the *Local Government Act 1995* (the Act).
- 1.2. The Committee is established to fulfil the requirements of Part 6, Financial Management and Part 7, Audit of the Act.
- 1.3. The Committee provides appropriate advice and recommendations to the Council on matters relevant to its terms of reference.
- 1.4. The Committee shall act in accordance with the provisions of the Act, the local laws and policies of the City of Busselton and these Terms of Reference.

### 2. OBJECTIVES, DUTIES AND RESPONSIBILITIES

- 2.1 The Committee operates to assist Council to fulfil its corporate governance, stewardship, leadership and control responsibilities in relation to financial reporting and audit, internal audit and risk management.
- 2.2 The primary objective of the Committee is to accept responsibility for the annual external audit and liaise with the local government's auditor so that Council can be satisfied with the local government's compliance with the relevant sections of the Act, in managing its financial affairs.
- 2.3 The specific duties and responsibilities of the Committee are to:
  - a. Provide guidance and assistance to the Council as to the carrying out the functions of the local government in relation to audits and financial management;
  - b. Meet with the external auditor at least once in each year.
  - c. Liaise with the CEO to ensure that the local government does everything in its power to:
    - i. assist the auditor to conduct the audit and carry out his or her other duties under the Local Government Act 1995; and
    - ii. ensure that audits are conducted successfully and expeditiously;
  - d. Examine the reports of the auditor after receiving a report from the CEO to:
    - i. determine if any matters raised require action to be taken by the local government; and
    - ii. be satisfied that the CEO has taken appropriate action in respect of those matters;
  - e. Review the report prepared by the CEO on any actions taken in respect of any matters raised in the report of the auditor and presenting the report to council for adoption prior to the end of the next financial year or 6 months after the last report prepared by the auditor is received, whichever is the latest in time;
  - f. Consider and recommend adoption of the annual financial report to the Council;
  - g. Address issues brought to the attention of the Committee in relation to the external audit, including responding to requests from the Council for advice that are within the parameters of the committee's terms of reference;

- h. Review the scope of audit plans and programs and their effectiveness as well as the appropriateness of special internal audit assignments undertaken by internal audit at the request of the Council or CEO;
- i. Review reports of internal audits and monitor the implementation of recommendations made by internal audit;
- j. Review the annual Compliance Audit Return, consider the report from the CEO and recommend to the Council the results of that review;
- k. Review the scope of internal and external audit to promote compatibility, to the extent appropriate, between each audit program;
- l. Assist the Council to ensure that the City is taking a sound approach to risk management, by monitoring and assuring that the City has implemented an appropriate risk management framework;
- m. Oversee periodic reviews of the City's risk management framework;
- n. Review and provide advice to the Council on the management of the City's strategic risk profile as required;
- o. Receive and consider reports from officers relating to significant strategic risks;
- p. Review the level of resources allocated to internal audit and risk management;
- q. Assist Council in ensuring that the City is taking a sound approach to fraud and corruption detection, by monitoring and assuring there are appropriate processes and systems in place to detect, capture and effectively respond to fraud and improper activities;
- r. Consider the CEO's triennial reviews of the appropriateness and effectiveness of the local government's financial management systems and procedures and report to the Council the results of those reviews;
- s. Oversee the implementation of any action required following the receipt of a review into the appropriateness and effectiveness of the local government's financial management systems and procedures;
- t. Consider the CEO's triennial reviews of the appropriateness and effectiveness of the local government's systems and procedures in regard to risk management, internal control and legislative compliance, and report to the Council the results of those reviews; and
- u. Oversee the implementation of any action required following the receipt of a review into the appropriateness and effectiveness of the local government's systems and procedures in regard to risk management, internal control and legislative compliance.

### **3. POWERS OF THE AUDIT AND RISK COMMITTEE**

- 3.1. The Committee is to report to the Council and provide appropriate advice and recommendations on matters relevant to its terms of reference to facilitate informed decision making by the Council.
- 3.2. The Committee is a formally appointed committee of the Council.
- 3.3. The Committee does not have executive powers or authority to implement actions in areas over which the CEO has legislative responsibility and does not have delegated financial responsibility.
- 3.4. The Committee does not have any management functions and cannot involve itself in management processes and procedures.

### **4. MEMBERSHIP**

- 4.1. The Committee will consist of:
  - a. between three and nine elected members; and
  - b. one independent external member.
- 4.2. The independent external member will be selected based on criteria which includes the following:
  - a. A suitably qualified person with demonstrated expertise and knowledge in financial management, risk management, governance and audit (internal and external);
  - b. Understanding of the duties and responsibilities of the position; ideally with respect to local government financial reporting and auditing requirements;
  - c. Strong communication skills; and
  - d. Relevant skills and experience in providing independent expert advice.

- 4.3. An independent external member will be a person with no operational responsibilities with the City nor will that person provide any other paid services to the City either directly or indirectly.
- 4.4. Appointment and re-appointment of independent external members will be made by Council after consideration of the CEO's recommendation. The applications of independent external members will be sought through an open and transparent process. The evaluation of potential members will be reviewed by the CEO and appointments will be approved by the Council.
- 4.5. An independent external member may only be appointed for a maximum of three consecutive terms.
- 4.6. Independent external members will be required to complete a confidentiality agreement and confirm that they will operate in accordance with the City's code of conduct. They will also be required to follow any City policies pertaining to the Committee operations.
- 4.7. Meeting attendance fees and reimbursement of approved expenses may be paid to an independent external member in accordance with s 5.100 of the Act.
- 4.8. Meeting attendance fees for an independent external member are to be the top of the relevant range for a band 1 local government in the most current Salaries and Allowances Tribunal determination for Local Government Chief Executive Officers and Elected Members
- 4.9. Council may appoint up to two elected members as deputy members of the Committee.
- 4.10. Membership of the Committee will, unless determined otherwise, cease on the day of the next ordinary Council election.
- 4.11. The membership of a member may be ceased in accordance with the Act.

## **5. PRESIDING MEMBER**

- 5.1. The Committee will appoint a Presiding Member and Deputy Presiding Member to conduct its business.

## **6. MEETINGS**

- 6.1. The Committee will meet at least four times per year.
- 6.2. The Committee will report to Council in accordance with clause 2.10 of the *City of Busselton Standing Orders Local Law 2018*.
- 6.3. Notice of meetings will be given to members at least five days prior to each meeting, with the agenda papers to be provided to members not less than 72 hours prior to the meeting.
- 6.4. The Presiding Member will ensure that detailed minutes of all meetings are kept in accordance with the Part 14 of the *City of Busselton Standing Orders Local Law 2018*.
- 6.5. All members of the Committee will have one vote. If the vote of the members present is equally divided, the Presiding Member will cast a second vote.
- 6.6. Where the Committee makes a recommendation to the Council, the Chief Executive Officer will ensure that the recommendation is on the agenda of the next practicable ordinary Council meeting.

## **7. QUORUM**

- 7.1. The quorum for a meeting will be at least 50% of the number of offices of membership, whether vacant or not.

**8. DELEGATED POWERS**

- 8.1. Pursuant to section 5.17 of the Act, the Committee is delegated the powers to conduct the formal meeting with the City’s external Auditor required by section 7.12(A)(2) on behalf of the local government.
- 8.2. In all other matters, Committee recommendations are not binding on Council and must be endorsed by Council to take effect.

**9. EVALUATION OF COMMITTEE’S EFFECTIVENESS**

- 9.1. At the meeting of the Council prior to the ordinary Council elections, officers will present for Council’s consideration a review as to the effectiveness and ongoing relevance of the Committee.

**10. APPROVAL**

<b>Council Adoption</b>	<b>DATE</b>	31 January 2024	<b>Resolution #</b>	C2401/13
<b>Previous Adoption</b>	<b>DATE</b>	21 June 2023	<b>Resolution #</b>	C2306/110

As at 31 December 2023

<b>TENDER INFORMATION</b>	<b>COUNCIL OR CEO DECISION</b>	<b>STATUS</b>
<p><b>RFT11/23 CHURCHILL PARK TURF</b> Requirements – a suitable contractor for the installation and maintenance of turf to Churchill Park.</p>	CEO Delegated Authority	Letter of award issued 8 December 2023
<p><b>RFT13/23 – CHURCHILL PARK CIVIL WORKS</b> Requirements – a suitable contractor to upgrade the Churchill Park main oval (including drainage, irrigation and resurfacing works)</p>	CEO Delegated Authority	Letter of award issued 30 November 2023
<p><b>RFT14/23 DEPOT FLOOR RESURFACING</b> Requirements - a suitable contractor to resurface the City of Busselton's mechanical workshop floor</p>	CEO Delegated Authority	Letter of Award issued 21 November 2023
<p><b>RFT15/23 – MARYBROOK SIESTA PARK EROSION STRATEGY</b> Requirements – a suitable consultant to design a coastal erosion protection strategy for Marybrook and Siesta Park coastline (including an implementation plan and funding model)</p>	CEO Delegated Authority	Advertised 11 October 2023, closed 7 November 2023 (2 Submissions received)
<p><b>RFT16/23 – Bussell Highway Upgrade Stage 2</b> Requirements - suitably experienced contractors to provide as Stage 2 of the Bussell Highway Upgrade Project asphalt overlay and widening works on for purposes of a cycleway.</p>	Council	Advertised on 9 December 2023, closing on 16 January 2024
<p><b>RFT17/23 – Changing Places Design and Construction</b> Requirement - design and construction of a new Changing Place facility at Signal Park on the Busselton foreshore.</p>	CEO Delegated Authority	Advertised on 9 December, closing on 23 January 2024

**As at 11 January 2024**

APPLICATION (Name, No. and City File Reference)	PROPERTY	DATE COMMENCED	DECISION BEING REVIEWED	STAGE COMPLETED	NEXT ACTION AND DATE OF ACTION AS PER SAT ORDERS	DATE COMPLETED / CLOSED
<b>CITY OF BUSSELTON</b>						
Amber Cloud Pty Ltd v City of Busselton (DR 231/2021)	Lot 101 Wilyabrup Road, Wilyabrup	November 2021	Review of a decision to refuse or conditionally grant an application under a planning scheme  (Single House)	<ul style="list-style-type: none"> <li>Refer to previous versions of this report for historical information.</li> <li>Directions hearing on 29 September 2023 was vacated and the matter is listed for a directions hearing on 20 October 2023.</li> <li>Directions hearing on 20 October 2023 was vacated and the matter is listed for a directions hearing on 2 February 2024.</li> </ul>	Listed for a directions hearing on 2 February 2024	
Silverlodge Pty Ltd v City of Busselton (CC 1225/2023)	585 Caves Rd Marybrook	September 2023	Review of a decision under the Caravan Parks and Camping Grounds Act 1995.	<ul style="list-style-type: none"> <li>Directions hearing on 20 October 2023.</li> <li>Applicant to submit an amended application to the City and the City to reconsider its decision.</li> <li>Directions hearing on 28 November 2023 was vacated and the matter is listed for a directions hearing on 16 January 2024.</li> </ul>	Directions hearing on 16 January 2024	
Happ & Anor v City of Busselton (DR 159/2022) (DR 197/2022) (DR 152/2023)	575 Commonage Road Quindalup	November 2022	<ol style="list-style-type: none"> <li>Review of directions under s214(2) and s214(3) by the City</li> <li>Review of decision to refuse application under a planning scheme</li> </ol>	<ul style="list-style-type: none"> <li>Refer to previous versions of this report for historical information</li> <li>Directions hearing on 6 October 2023 it was ordered: <ul style="list-style-type: none"> <li>SIFC to be filed by Respondent by 27 October 2023;</li> <li>SIFC to be filed by Applicant by 17 November 2023; and</li> <li>Directions hearing listed for 24 November 2023.</li> </ul> </li> <li>Directions hearing on 24 November 2023, it was ordered that: <ul style="list-style-type: none"> <li>The Parties file witness statements by 25 January 2024;</li> <li>Directions hearing listed for 2 February 2024.</li> </ul> </li> </ul>	Directions Hearing on 2 February 2024	
Forrest v City of Busselton (DR 101/2023)	Ludlow Park Road, Ludlow	June 2023	Review of a decision to refuse or conditionally grant an application under a planning scheme  (Industry - Extractive)	<ul style="list-style-type: none"> <li>Directions hearing listed for 7 July 2023</li> <li>Listed for Mediation on 9 August 2023</li> <li>Mediation on 9 August 2023 was vacated and re-listed for 20 September 2023.</li> <li>Mediation on 20 September 2023 and re-listed for 15 November 2023.</li> <li>Mediation on 15 November 2023, it was ordered to proceed to a hearing and listed for a directions hearing on 1 December 2023.</li> <li>Directions hearing listed for 1 December 2023 was vacated and the following was ordered: <ul style="list-style-type: none"> <li>SIFC to be filed by Respondent by 21 December 2023;</li> <li>SIFC to be filed by Applicant by 25 January 2024 and</li> <li>Directions hearing listed for 2 February 2024.</li> </ul> </li> </ul>	Directions Hearing on 2 February 2024	
Leeuwin Civil Pty Ltd v City of Busselton (DR 139/2023)	Lot 1801 Banksia Road, Hithergreen	August 2023	Deemed refusal under Clause 75 (2) of the Regulations 2015.	<ul style="list-style-type: none"> <li>Directions hearing listed for 1 September 2023: <ul style="list-style-type: none"> <li>By 8 September 2023 the applicant is to provide additional information to the respondent.</li> </ul> </li> <li>Mediation held on 21 September 2023.</li> </ul>	Mediation scheduled for 1 November 2023 was vacated as the City agreed to	

APPLICATION (Name, No. and City File Reference)	PROPERTY	DATE COMMENCED	DECISION BEING REVIEWED	STAGE COMPLETED	NEXT ACTION AND DATE OF ACTION AS PER SAT ORDERS	DATE COMPLETED / CLOSED
				<ul style="list-style-type: none"> <li>- Applicant to provide the City with additional information for the City to review and forward to agencies (where required) and further mediation on 10 October 2023.</li> <li>• Mediation held on 10 October 2023.</li> <li>- Additional information was required to be provided to the City by the Applicant. Applicant has since provided this information to the City. Further mediation was scheduled for 1 November 2023 however this has been vacated as the City has agreed to reconsider its decision on, or before, 10 November 2023 pursuant to Section 31.</li> </ul>	reconsider its decision by 10 November 2023.	
Smith v City of Busselton (DR 161/2023)	Carnarvon Castle Drive, Eagle Bay	October 2023	Review of a decision to refuse or conditionally grant an application under a planning scheme	<ul style="list-style-type: none"> <li>• Directions hearing listed for 16 February 2024</li> </ul>	Directions hearing listed for 16 February 2024	
La Terre (WA) Pty Ltd v City of Busselton (DR 167/2023)	Bina Place, Quedjinup	November 2023	Review of a decision to refuse or conditionally grant an application under a planning scheme	<ul style="list-style-type: none"> <li>• Directions hearing listed for 19 December 2023 was vacated.</li> <li>• Directions hearing listed for 2 February 2024.</li> </ul>	Directions hearing listed for 2 February 2024	
Sims v City of Busselton (DR 174/2023)	Forrest Beach Road, Wonnerup	November 2023	Review of a decision to refuse or conditionally grant an application under a planning scheme	<ul style="list-style-type: none"> <li>• Directions hearing listed for 22 December 2023</li> <li>• Directions hearing listed for 22 December 2023 was vacated and the proceedings were withdrawn.</li> </ul>		
<b>REGIONAL JOINT DEVELOPMENT ASSESSMENT PANEL</b>						
NIL						

# Action Register

## Search Criteria

**Showing Completed Items:** Yes  
**Include Items Completed From:** 01/07/2023  
**Generated By:** Jo Barrett-Lennard  
**Generated On:** 18/01/2024 at 9:27am

Meeting Date	Item	Status	Action Required	Action Taken	Completed (Overdue)								
26/07/2023	Airport Advisory Committee - 14/06/2023 - REVIEW OF THE AIRPORT ADVISORY COMMITTEE	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 10px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2307/61</b> Moved Cr Paul Carter, seconded Cr Phill Cronin</p> <p>That the Council:</p> <ol style="list-style-type: none"> <li>1. notes the outcome of the evaluation of the Airport Advisory Committee's (the Committee) effectiveness, in accordance with clause 8 of the Committee's Terms of Reference;</li> <li>2. acknowledge that while the Committee is operating effectively, increased information and training for members and more strategic involvement in LTFP and budget preparation would further improve its effectiveness and requests the CEO to consider how these can be actioned;</li> <li>3. endorses the amendments proposed to the Committee's Terms of Reference as per Attachment 1.</li> </ol> <p style="text-align: right;"><b>CARRIED 7 / 0</b> <b>EN BLOC</b></p> </div>	<p><b>03/08/2023 Jo Barrett-Lennard</b></p> <p>ToR updated on City Website - 3 August 2023</p>	06/09/2023								
26/07/2023	AMENDMENT NO 59 TO LOCAL PLANNING SCHEME 21 LOT 200 (NO 1676) CAVES ROAD DUNSBOROUGH - CONSIDERATION FOR ADOPTION	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 10px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2307/65</b> Moved Cr Paul Carter, seconded Cr Phill Cronin</p> <p>That the Council:</p> <ol style="list-style-type: none"> <li>1. In pursuance of the <i>Planning and Development Act (Local Planning Schemes) Regulations 2015</i> (the Regulations) adopts Amendment 59 to the City of Busselton Local Planning Scheme 21 (the Scheme) for final approval, for the purposes of: <ul style="list-style-type: none"> <li>a) Inserting the following particulars in 'Schedule 3 - Special Provisions Areas' of the Scheme; <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>No.</th> <th>Particulars of land</th> <th>Zone</th> <th>Special Provisions</th> </tr> </thead> <tbody> <tr> <td>SP77</td> <td>Lot 200 (No 1676) Caves Road Dunsborough</td> <td>Tourism</td> <td>Notwithstanding any other provisions of the Scheme, unrestricted length of stay shall be permitted for a 'chalet' on strata lots 6-13 and 48-51 as shown on Strata Plan 50936.</td> </tr> </tbody> </table> </li> <li>2. Advises the Western Australian Planning Commission (WAPC) that Amendment 59 is considered a 'standard amendment' pursuant to the Regulations for the following reasons: <ul style="list-style-type: none"> <li>a) An amendment that is consistent with the objectives of the 'Tourism' zone as set out within Local Planning Scheme 21.</li> <li>b) An amendment that is consistent with a Local Planning Strategy for the Scheme that has been endorsed by the Commission.</li> </ul> </li> <li>3. Pursuant to r. 53 of the Regulations, endorses the Schedule of Submissions at Attachment 5, which has been prepared in response to the public consultation process.</li> </ul></li></ol> </div>	No.	Particulars of land	Zone	Special Provisions	SP77	Lot 200 (No 1676) Caves Road Dunsborough	Tourism	Notwithstanding any other provisions of the Scheme, unrestricted length of stay shall be permitted for a 'chalet' on strata lots 6-13 and 48-51 as shown on Strata Plan 50936.	<p><b>01/09/2023 Justin Biggar</b></p> <p>Referred to the Minister for final approval on 8 August 2023</p>	01/09/2023
No.	Particulars of land	Zone	Special Provisions										
SP77	Lot 200 (No 1676) Caves Road Dunsborough	Tourism	Notwithstanding any other provisions of the Scheme, unrestricted length of stay shall be permitted for a 'chalet' on strata lots 6-13 and 48-51 as shown on Strata Plan 50936.										

Meeting Date	Item	Status	Action Required	Action Taken	Completed (Overdue)
			<p>4. Upon preparation of the necessary documentation, refers the adopted Amendment 59 to the WAPC for consideration and determination in accordance with the <i>Planning and Development Act 2005</i>.</p> <p>5. Pursuant to r. 56 of the Regulations, should directions be given that modifications to Amendment 59 are required, direct these modifications to be undertaken accordingly, on behalf of the Council, unless they are considered by officers to significantly affect the purpose and intent of the Amendment, in which case the matter shall be formally referred back to the Council for assessment and determination</p> <p style="text-align: right;"><b>CARRIED 7 / 0</b> <b>EN BLOC</b></p>		
26/07/2023	RFT 09/23 PARKS AND RESERVES	Completed	<p><b>COUNCIL DECISION</b></p> <p><b>C2307/66</b> Moved Cr Paul Carter, seconded Cr Sue Riccelli</p> <p>That the Council endorses the outcome of the evaluation panel's assessment of RFT 09/23 Garden Maintenance Services and accepts the tender from Activ Foundation Inc as most advantageous.</p> <p style="text-align: right;"><b>CARRIED 7 / 0</b> <b>EN BLOC</b></p> <p>Please action as per resolution.</p>	<p><b>07/09/2023</b> Iain Ferry</p> <p>Tender has been awarded</p>	07/09/2023
26/07/2023	BUSINESS DEVELOPMENT, EVENTS AND MARKETING PROGRAM OUTCOMES - JUNE 2023	Completed	<p>Please action as per resolution.</p> <p><b>COUNCIL DECISION</b></p> <p><b>C2307/67</b> Moved Cr Jodie Richards, seconded Cr Phill Cronin</p> <p>That the Council:</p> <p>1. Endorses the following multi year funding allocation, to be funded from the 2024/2025 - 2026/2027 Events Budgets:</p> <p><i>*removed for brevity</i></p> <p style="text-align: right;"><b>CARRIED 7 / 0</b> <b>BY ABSOLUTE MAJORITY</b></p>	<p><b>05/09/2023</b> Peta Pulford</p> <p>Two streams of Economic Development and Marketing funding have been developed. All associated application forms and guidelines have been produced. First round of funding for these streams are currently open for application, with closing date of 21 September 2023. Successful funding recipients of the Events Sponsorship (IRONMAN WA, BCCI Awards) have been advised of outcomes. Funding agreements are in progress, with full execution expected by end of September.</p>	05/09/2023

16/08/2023	Policy and Legislation Committee - 26/07/2023 - Dogs Local Law	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 10px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2308/78</b> Moved Cr Paul Carter, seconded Cr Phill Cronin</p> <p>That the Council:</p> <ol style="list-style-type: none"> <li>1. Commences the law-making process for the <i>City of Busselton Dogs Local Law 2023</i>; the purpose and effect of the local law being as follows: <ul style="list-style-type: none"> <li>Purpose: To repeal the Dogs Local Law of 2014 and make provisions about dogs which are permitted under the <i>Dog Act 1976</i>.</li> <li>Effect: To provide Council with controls and regulatory measures in relation to matters such as the impounding of dogs, to control the number of dogs that can be kept on premises and the manner of keeping those dogs.</li> </ul> </li> <li>2. Authorises the CEO to carry out the law-making procedure under section 3.12(3) of the <i>Local Government Act 1995</i>, by: <ol style="list-style-type: none"> <li>(a) Giving local public notice of the Proposed Local Law; and</li> <li>(b) Giving a copy of the Proposed Local Law and public notice to the Minister for Local Government.</li> </ol> </li> <li>3. Notes that the CEO, after the close of the public consultation period, will submit a report to the Council on any submissions received on the Proposed Local Law to enable the Council to consider the submissions made and to determine whether to make the local law in accordance with section 3.12(4) of the Act.</li> </ol> <p style="text-align: right;"><b>CARRIED 7 / 0</b> EN BLOC</p> </div>	<p><b>06/09/2023 Briony McGinty</b></p> <p>Local Law advertised and sent to Minister.</p>	06/09/2023
16/08/2023	Policy and Legislation Committee - 26/07/2023- Delegation of Power to Dispose of Residential Property by Lease	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 10px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2308/79</b> Moved Cr Anne Ryan, seconded Cr Kate Cox</p> <p>That the Council adopts the instrument of delegation at Attachment 1.</p> <p style="text-align: right;"><b>CARRIED 7 / 0</b> <b>BY ABSOLUTE MAJORITY</b></p> </div>	<p><b>31/08/2023 Ann Strang</b></p> <p>No action required by Property Services, forwarded to Governance to amend delegation register.</p> <p><b>06/09/2023 Sarah Pierson</b></p> <p>Delegation register amended</p>	06/09/2023

16/08/2023	Policy and Legislation Committee - 26/07/2023 - Review of the Policy and Legislation Committee	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2308/80</b> Moved Cr Paul Carter, seconded Cr Phill Cronin</p> <p>That the Council</p> <ol style="list-style-type: none"> <li>1. notes the outcome of the evaluation of the Policy and Legislation Committee's (the Committee) effectiveness, in accordance with clause 8 of the Committee's Terms of Reference;</li> <li>2. acknowledge that while the Committee is operating effectively, increased information and training for members would further improve its effectiveness and requests the CEO consider how this can be actioned;</li> <li>3. Endorses the amendments proposed to the Committee's Terms of Reference as per Attachment 1.</li> </ol> <p style="text-align: right;"><b>CARRIED 7 / 0</b> <b>EN BLOC</b></p> </div>	<p><b>04/09/2023 Jo Barrett-Lennard</b></p> <p>Terms of Reference updated to website 31 August 2023.</p>	04/09/2023
16/08/2023	Policy and Legislation Committee - 26/07/2023 - RESCISSION OF COUNCIL POLICY 'SWEARING IN OF ELECTED MEMBERS'	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2308/81</b> Moved Cr Paul Carter, seconded Cr Phill Cronin</p> <p>That the Council rescind Council Policy 'Swearing in of Elected Members' (Attachment 1), effective immediately.</p> <p style="text-align: right;"><b>CARRIED 7 / 0</b> <b>EN BLOC</b></p> </div>	<p><b>31/08/2023 Jo Barrett-Lennard</b></p> <p>Policy removed from website and moved to 'Rescinded' folder in ECM</p>	31/08/2023
16/08/2023	Policy and Legislation Committee - 26/07/2023 - REVIEW OF COUNCIL POLICY 'FEES, ALLOWANCES AND EXPENSES FOR ELECTED MEMBERS'	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2308/82</b> Moved Cr Paul Carter, seconded Cr Phill Cronin</p> <p>That the Council adopts the amended Council Policy 'Elected Member Fees, Allowances and Expenses' (Attachment 1), to replace the current policy titled 'Fees, Allowances and Expenses for Elected Members' (Attachment 2), inclusive of Committee amendments so that hte following points read as follows, with subsequent renumbering as required:</p> <ol style="list-style-type: none"> <li>5.5 As per Section 5.99 of the Act, Elected Members will be paid an annual attendance fee in lieu of fees for attending meetings referred to in Section 5.98(1) of the Act. Elected Members will be paid an annual fee within the relevant band set out in the Determination, as set each year by Council through the adoption of its budget.</li> <li>5.8 Any equipment provided in accordance with this policy must be returned to the City at the expiry of an Elected Members term. After having served a minimum of 24 months, an Elected Member may on retiring choose to keep the equipment, subject to it being cleared by the City's IT department.</li> </ol> </div>	<p><b>31/08/2023 Jo Barrett-Lennard</b></p> <p>Policy updated, updated version on website</p>	31/08/2023

			<p>5.18 Spouse/ partner attendance on City sanctioned activities must be approved by the Mayor and CEO and will be at the Elected Member's expense. <i>(new)</i></p> <p>5.19 The City will also meet the following expenses:</p> <ul style="list-style-type: none"> <li>a. cost of laundry for Elected Member for greater than 5 nights;</li> <li>b. reimbursement of taxi fares or other public transport where these directly relate to the activity and no other transport is provided;</li> <li>c. reimbursement of a daily sustenance allowance per day, in accordance with the Public Service Award 1992;</li> <li>d. specific conference / event related meals.</li> </ul> <p>5.27 In addition to their entitlements as an Elected Member under this Policy, the Mayor is eligible for a Mayoral allowance in accordance with the Act. The Mayor will be paid an allowance within the relevant Band set out in the Determination, as set each year by Council through adoption of its budget. <i>(previously 5.26)</i></p> <p style="text-align: right;"><b>CARRIED 7 / 0</b> <b>EN BLOC</b></p>		
16/08/2023	Policy and Legislation Committee - 26/07/2023 - REVIEW OF COUNCIL POLICY 'DESIGNATION OF SENIOR EMPLOYEE AND ACTING CEO'	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2308/83</b> Moved Cr Paul Carter, seconded Cr Phill Cronin</p> <p>That the Council adopts the amended Council Policy 'Designation of Senior Employees and Acting CEO' (Attachment 1), to replace the current policy (Attachment 2).</p> <p style="text-align: right;"><b>CARRIED 7 / 0</b> <b>EN BLOC</b></p> </div>	31/08/2023 Jo Barrett-Lennard	06/09/2023
16/08/2023	Policy and Legislation Committee - 26/07/2023 - REVIEW OF COUNCIL POLICY 'CLOSED CIRCUIT TELEVISION SYSTEMS'	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2308/84</b> Moved Cr Paul Carter, seconded Cr Phill Cronin</p> <p>That the Council adopts the amended Council policy: Closed Circuit Television Systems (the Policy) (Attachment 1) to replace the current policy (Attachment 2).</p> <p style="text-align: right;"><b>CARRIED 7 / 0</b> <b>EN BLOC</b></p> </div>	31/08/2023 Jo Barrett-Lennard	06/09/2023

16/08/2023	Finance Committee - 09/08/2023 - Rate Exemption - Lamp Inc.	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2308/87</b> Moved Cr Paul Carter, seconded Cr Phill Cronin</p> <p>That the Council:</p> <ol style="list-style-type: none"> <li>1. Grant rate exemption to Lamp Inc. for 224 Bussell Highway WEST BUSSELTON, effective from the 27th of June 2023 application date, under section 6.26(2)(d) of the <i>Local Government Act 1995</i>;</li> <li>2. Agree that this rate exemption is to continue where Lamp Inc. continue to own and use the property as stated in the application at Attachment A;</li> <li>3. Continue to advocate, through the South West Zone of WALGA, for a review of Rating Exemptions; and</li> <li>4. Continue to note in the City's Annual Report the annual total cost to the City of rating exemptions.</li> </ol> <p style="text-align: right;"><b>CARRIED 7 / 0</b> EN BLOC</p> </div>	31/08/2023 David Nicholson Rate exemption applied.	31/08/2023								
16/08/2023	Amendment 54 to Local Planning Scheme 21 Lot 2883 Puzey Road, Wilyabrup	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2308/88</b> Moved Cr Paul Carter, seconded Cr Phill Cronin</p> <p>That the Council:</p> <ol style="list-style-type: none"> <li>1. In pursuance of the Planning and Development Act (Local Planning Schemes) Regulations 2015 (the Regulations) adopts Amendment 54 to the City of Busselton Local Planning Scheme 21 (the Scheme) for final approval, for the purposes of:             <ol style="list-style-type: none"> <li>a) Inserting a new 'Special Provision Area 76' as follows:                 <table border="1" data-bbox="765 1276 1745 1486" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>No.</th> <th>Particulars of Land</th> <th>Zone</th> <th>Special Provisions</th> </tr> </thead> <tbody> <tr> <td>SP76</td> <td>Strata Lots 20, 21, 31, 37, 46 and 47 on Survey-Strata Plan 54626 (Lot 2883) Puzey Road, Wilyabrup</td> <td>Viticulture and Tourism</td> <td>For Lots 20, 21, 31, 37, 46 and 47, as shown on the current Strata Plan 54626 (approved on 24 December 2021), one Chalet per lot is permitted as unrestricted length of stay.</td> </tr> </tbody> </table> </li> <li>b) Amending the Scheme Map accordingly.</li> </ol> </li> <li>2. In accordance with Regulation 35 (2) of the Planning and Development (Local Planning Schemes) Regulations 2015, determines that Amendment No. 54 of the City of Busselton Local Planning Scheme No. 21 is a 'complex amendment' in accordance with r.34 of the Regulations as it is:             <ol style="list-style-type: none"> <li>(a) an amendment that is not consistent with a Local Planning Strategy for the Scheme that has been endorsed by the Commission.</li> </ol> </li> <li>3. Pursuant to r. 53 of the Regulations, endorses the Schedule of Submissions at Attachment 5, which has been prepared in response to the public consultation process.</li> </ol> </div>	No.	Particulars of Land	Zone	Special Provisions	SP76	Strata Lots 20, 21, 31, 37, 46 and 47 on Survey-Strata Plan 54626 (Lot 2883) Puzey Road, Wilyabrup	Viticulture and Tourism	For Lots 20, 21, 31, 37, 46 and 47, as shown on the current Strata Plan 54626 (approved on 24 December 2021), one Chalet per lot is permitted as unrestricted length of stay.	07/09/2023 David Sands Letter drafted CEO approval required prior to documentation being sent to WAPC.	03/10/2023
No.	Particulars of Land	Zone	Special Provisions										
SP76	Strata Lots 20, 21, 31, 37, 46 and 47 on Survey-Strata Plan 54626 (Lot 2883) Puzey Road, Wilyabrup	Viticulture and Tourism	For Lots 20, 21, 31, 37, 46 and 47, as shown on the current Strata Plan 54626 (approved on 24 December 2021), one Chalet per lot is permitted as unrestricted length of stay.										

			<p>4. Upon preparation of the necessary documentation, refers the adopted Amendment 54 to the <a href="#">WAPC</a> for consideration and determination in accordance with the Planning and Development Act 2005.</p> <p>5. Pursuant to r. 56 of the Regulations, should directions be given that modifications to Amendment 54 are required, direct these modifications to be undertaken accordingly, on behalf of the Council, unless they are considered by officers to significantly affect the purpose and intent of the Amendment, in which case the matter shall be formally referred back to the Council for assessment and determination</p> <p style="text-align: right;"><b>CARRIED 7 / 0</b> EN BLOC</p>		
16/08/2023	Proposed Amendment 56 to Local Planning Scheme 21 Special Character Area	Completed	<p>Please action as per resolution.</p> <p><b>COUNCIL DECISION</b></p> <p><b>C2308/90</b> Moved Cr Phill Cronin, seconded Cr Anne Ryan</p> <p>That the Council:</p> <p>1. In pursuance of the <i>Planning and Development (Local Planning Schemes) Regulations 2015</i> (the Regulations) initiates Amendment 56 to the City of Busselton Local Planning Scheme No. 21 (the Scheme) for community consultation for the purposes of:</p> <p>(a) Amending clause 5.7.3 to read as follows:</p> <p>(i) <i>5.7.3 Where such objectives, provisions and/or controls are specified in Schedule 4 in relation to a designated Special Character Area, those objectives, provisions and/or controls act in conjunction with the other provisions of this Scheme relevant to that area.</i></p> <p>2. Pursuant to r.35(2) determine that Amendment 56 is a 'standard amendment' as it is:</p> <p>(a) An amendment that would have minimal impact on land in the scheme area that is not the subject of the amendment;</p> <p>(b) An amendment that does not result in any significant environmental, social, economic or governance impacts on land in the Scheme area; and</p> <p>(c) Any other amendment that is not a complex or basic amendment.</p> <p><i>*removed for brevity</i></p> <p style="text-align: right;"><b>CARRIED 6 / 0</b> BY ABSOLUTE MAJORITY</p>	01/09/2023 Justin Biggar	03/10/2023
16/08/2023	PROPOSED ABBEY SOUTH STRUCTURE PLAN AND ASSOCIATED AMENDMENT NO. 55 TO LOCAL PLANNING SCHEME NO. 21 - CONSIDERATION FOR FINAL APPROVAL	Completed	<p>Please action as per resolution.</p> <p><b>COUNCIL DECISION</b></p> <p><b>C2308/91</b> Moved Cr Kate Cox, seconded Cr Jodie Richards</p> <p>That the Council:</p> <p>1. Pursuant to Part 4 of Schedule 2 of the <i>Planning and Development (Local Planning Schemes) Regulations 2015</i> (the Regulations), adopts the Abbey South Structure Plan at Attachment 2 for final approval, in accordance with the modifications proposed in the Schedule of Modifications at Attachment 7.</p> <p>2. Pursuant to r.50 of the Regulations, adopts Amendment No. 55 to Local Planning Scheme No. 21 for final approval, in accordance with the modifications proposed in the Schedule of Modifications shown at Attachment 7, for the purposes of:</p>	06/09/2023 Noah Scott-McDonald	27/09/2023

			<p><i>*removed for brevity</i></p> <p style="text-align: right;"><b>CARRIED 6 / 1</b></p> <p style="text-align: center;"><b>FOR: Cr Grant Henley, Cr Paul Carter, Cr Jodie Richards, Cr Kate Cox, Cr Phill Cronin and Cr Ross Paine</b></p> <p style="text-align: right;"><b>AGAINST: Cr Anne Ryan</b></p>		
16/08/2023	APPLICATION FOR DEVELOPMENT APPROVAL (DA21/0822) - PROPOSED EXTRACTIVE (SAND) - LOT 4205 GALE ROAD KALOORUP	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2308/92</b> Moved Cr Paul Carter, seconded Cr Phill Cronin</p> <p>That the Council:</p> <ol style="list-style-type: none"> <li>1. That application DA21/0822 submitted for development of Industry – Extractive (Sand) at Lot 4205 Gale Road Kaloorup is considered by the Council to be generally consistent with Local Planning Scheme No. 21 and the objectives of the zone within which it is located.</li> <li>2. That Development Approval is granted for the proposal referred to in (1) above subject to the following conditions:</li> </ol> <p><b>GENERAL CONDITIONS</b></p> <ol style="list-style-type: none"> <li>1. The development hereby approved is permitted to operate for five years from the date of this Decision Notice. The site shall be fully rehabilitated in accordance with the approved development plans before the expiry date of this development approval.</li> <li>2. The owner must ensure that the development is undertaken in accordance with the approved development plans, the following management plans and any plans or details approved pursuant to other conditions of this approval:                             <ol style="list-style-type: none"> <li>2.1 Excavation – Rehabilitation Management Plan (Landform Research) dated 8 August 2023; and</li> <li>2.2 Water Management Plan (Accendo Australia) dated August 2023.</li> </ol> </li> </ol> <p>Where there is a conflict between a provision contained within a condition of this development approval and a provision contained within one of the plans listed above, the requirements of the condition shall prevail.</p> <p><i>*removed for brevity</i></p> <p style="text-align: right;"><b>CARRIED 7 / 0</b></p> <p style="text-align: right;">EN BLOC</p> </div>	07/09/2023 Kellie Woodcoe-Bryant DA Issued 30/8/2023	20/12/2023

16/08/2023	NAMING PORT GEOGRAPHE COVES AND PARKS	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2308/93</b> Moved Cr Paul Carter, seconded Cr Phill Cronin</p> <p>That the Council:</p> <ol style="list-style-type: none"> <li>1. That the Council approve the suggested names in this report resulting from consultation with representatives of the Traditional Owners of the Port Geographe area.</li> <li>2. That the Council approve for the City Officer to submit an application with Landgate for using the names in the Port Geograhe area.</li> </ol> <p style="text-align: right;"><b>CARRIED 7 / 0</b> EN BLOC</p> </div>	<p><b>07/09/2023 Daniell Abrahamse</b></p> <p>The City has received approval from Landgate of the proposed names and will be obtaining quotes to manufacture and install the signs as soon as possible.</p>	07/09/2023
16/08/2023	RFT10/23 Arboriculture Services	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2308/94</b> Moved Cr Paul Carter, seconded Cr Phill Cronin</p> <p>That the Council endorses the outcome of the evaluation panel's assessment of RFT10/23 Arboriculture Services and accepts the tender from Fine Brand Pty Ltd as trustee for Guy Badger Family Trust trading as Arbor Guy as most advantageous.</p> <p style="text-align: right;"><b>CARRIED 7 / 0</b> EN BLOC</p> </div>	<p><b>07/09/2023 Iain Ferry</b></p> <p>Tender has been awarded</p>	07/09/2023
16/08/2023	Council Policy - Appointment of Acting Chief Executive Officer	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2308/96</b> Moved Cr Paul Carter, seconded Cr Phill Cronin</p> <p>That the Council adopt the Council Policy 'Appointment of Acting Chief Executive Officer' as per Attachment 1.</p> <p style="text-align: right;"><b>CARRIED 7 / 0</b> EN BLOC</p> </div>	<p><b>31/08/2023 Jo Barrett-Lennard</b></p> <p>Policy adopted and uploaded to website.</p>	01/09/2023

20/09/2023	DA22/0817 - Bed and Breakfast (Change of Use of Existing Single House and New Development)	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2309/110</b> Moved Cr Sue Riccelli, seconded Cr Anne Ryan</p> <p><b>That the Council determines:</b></p> <p>A. That application DA22/0817 submitted for development of Bed and Breakfast (Change of Use of Existing Single House and New Development) on Lot 301 (No. 29) Carnarvon Castle Drive, Eagle Bay, is considered by the Council to not be consistent with Local Planning Scheme No. 21 and the objectives of the zone within which it is located.</p> <p>B. To refuse to grant development approval, for the following reasons:</p> <ol style="list-style-type: none"> <li>1. Uncertainty exists around whether lawful discretion exists.</li> <li>2. The proposal is considered to have excessive landscape and visual elements particularly in relation to: <ol style="list-style-type: none"> <li>a) Dwelling is proposed to reach a height of 8.45m, inclusive of a chimney height, in lieu of the development standard permitted height of 7.5m.</li> <li>b) Upper store floor area covers 89% of the ground floor footprint in lieu of 50% permitted by the Development Standard.</li> <li>c) Outbuilding wall height (shed) of 3.34m in lieu of permitted Residential Design Codes - 2.4m, LPS 21 - 2.7m and LPP 4.10 - 3.1m.</li> </ol> </li> </ol> <p style="text-align: right;"><b>CARRIED 5 / 3</b></p> <p style="text-align: center;"><b>FOR: Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Cr Phill Cronin and Cr Sue Riccelli</b></p> <p style="text-align: center;"><b>AGAINST: Cr Grant Henley, Cr Paul Carter and Cr Ross Paine</b></p> <p>Reasons: There is a lack of certainty around whether lawful discretion is able to be used for this proposal because of the issue of which setback standards apply is critical to the determination of the application. There appears to be overdevelopment of the built form on site.</p> </div>	04/10/2023 Jo Barrett-Lennard Decision notice issued 27/9/23	04/10/2023
20/09/2023	Application for Development Approval (DA23/0270) - Single House (Outbuilding) - Lot 43 (9) Campion Way Quindalup	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2309/111</b> Moved Cr Anne Ryan, seconded Cr Paul Carter</p> <p><b>That the Council:</b></p> <ol style="list-style-type: none"> <li>1. Consider that the application for a Single House (Outbuilding) is generally consistent with Local Planning Scheme No. 21 and the objectives of the zone in which it is located; and</li> <li>2. Grant development approval for the proposal subject to the following conditions:</li> </ol> <p><b>GENERAL CONDITIONS:</b></p> <ol style="list-style-type: none"> <li>1. The development hereby approved shall be substantially commenced within two years of the date of this decision notice.</li> </ol> </div>	03/10/2023 Justin Biggar Approval notice issued	03/10/2023

			<p>2. The development hereby approved shall be undertaken in accordance with the signed and stamped Approved Development Plans(s), including any notes placed thereon in red by the City of Busselton.</p> <p><b>PRIOR TO COMMENCEMENT OF ANY WORKS CONDITIONS:</b></p> <p>3. The development hereby approved, or any works required to implement the development shall not commence until the following plans or details have been submitted to the City and approved in writing:</p> <p>1. A minimum of two (2) <i>Agonis flexuosa</i> (WA peppermint trees) with a minimum age of 2 years and height of 2m at the time of planting are to be planted within the site in accordance with the Approved Development Plan. Unless otherwise first agreed in writing, any trees which, within a period of five years from first planting are removed, die or are assessed by the City as being seriously damaged, shall be replaced within the next available planting season with others of the same species, size and number as originally approved.</p> <p><b>PRIOR TO OCCUPATION/USE OF THE DEVELOPMENT CONDITIONS:</b></p> <p>4. The development hereby approved shall not be occupied or used, until all plans, details or works required by Condition 3 have been implemented.</p> <p><b>ON-GOING CONDITIONS</b></p> <p>5. The works undertaken to satisfy Condition 3 shall be subsequently maintained for the life of the development.</p> <p style="text-align: right;"><b>CARRIED 8 / 0</b></p> <p style="text-align: right;"><b>FOR: Cr Grant Henley, Cr Paul Carter, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Cr Phill Cronin, Cr Sue Riccelli and Cr Ross Paine</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p> <p><b>EN BLOC</b></p>		
20/09/2023	Application for Development Approval (DA23/0194) - Single House (Drive Way Addition) - Lot 61 (40) Campion Way, Quindalup	Completed	<p>Please action as per resolution.</p> <p><b>COUNCIL DECISION</b></p> <p><b>C2309/112</b> Moved Cr Anne Ryan, seconded Cr Paul Carter</p> <p><b>That the Council:</b></p> <p>1. Consider that the application for a Single House (Driveway Addition) DA23/0194 is generally consistent with Local Planning Scheme No. 21 and the objectives of the zone in which it is located; and</p> <p>2. Grant development approval for the proposal subject to the following conditions:</p> <p><b>GENERAL CONDITIONS:</b></p> <p>1. The development hereby approved shall be substantially commenced within two years of the date of this decision notice.</p> <p>2. The development hereby approved shall be undertaken in accordance with the signed and stamped, Approved Development Plan(s), including any notes placed thereon in red by the City.</p> <p><b>PRIOR TO COMMENCEMENT OF ANY WORKS CONDITIONS:</b></p>	04/10/2023 David Sands Issue decision letter.	04/10/2023

			<p>3. The development hereby approved, or any works required to implement the development shall not commence until the following plans or details have been submitted to the City and approved in writing:</p> <p>1. A minimum of two (2) <i>Agonis flexuosa</i> (WA peppermint trees) with a minimum age of 2 years and height of 2m at the time of planting are to be planted within the site in accordance with the Approved Development Plan. Unless otherwise first agreed in writing, any trees which, within a period of five years from first planting are removed, die or are assessed by the City as being seriously damaged, shall be replaced within the next available planting season with others of the same species, size and number as originally approved.</p> <p>PRIOR TO OCCUPATION/USE OF THE DEVELOPMENT CONDITIONS:</p> <p>4. The development hereby approved shall not be occupied or used, until all plans, details or works required by Condition 3 have been implemented.</p> <p>ON-GOING CONDITIONS</p> <p>5. The works undertaken to satisfy Condition 3 shall be subsequently maintained for the life of the development.</p> <p style="text-align: right;"><b>CARRIED 8 / 0</b></p> <p style="text-align: right;"><b>FOR: Cr Grant Henley, Cr Paul Carter, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Cr Phill Cronin, Cr Sue Riccelli and Cr Ross Paine</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p> <p><b>EN BLOC</b></p>		
20/09/2023	Application for Development Approval (DA23/0133) - Grouped Dwelling (Alterations and Additions) - Lot 1 (17A) Gibney Street Dunsborough	Completed	<p>Please action as per resolution.</p> <p><b>COUNCIL DECISION</b></p> <p><b>C2309/113</b> Moved Cr Anne Ryan, seconded Cr Paul Carter</p> <p>That the Council:</p> <p>1. Consider that the application for a Grouped Dwelling (Alterations/Additions) is generally consistent with Local Planning Scheme No. 21 and the objectives of the zone in which it is located; and</p> <p>2. Grant development approval for the proposal subject to the following conditions:</p> <p>GENERAL CONDITIONS:</p> <p>1. The development hereby approved shall be substantially commenced within two years of the date of this decision notice.</p> <p>2. The development hereby approved shall be undertaken in accordance with the signed and stamped Approved Development Plans(s), including any notes placed thereon in red by the City of Busselton.</p> <p>ONGOING CONDITIONS:</p> <p>3. The works undertaken to satisfy Condition 2 shall be subsequently maintained for the life of the development, and the following Condition(s) complied with:</p> <p>3.1 Stormwater to be retained for use and/or infiltration within the lot at a rate of 1m<sup>3</sup> per 40m<sup>2</sup> of impervious area.</p>	04/10/2023 Jo Barrett-Lennard Approved and issued	04/10/2023

			<p style="text-align: right;"><b>CARRIED 8 / 0</b></p> <p><b>FOR: Cr Grant Henley, Cr Paul Carter, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Cr Phill Cronin, Cr Sue Riccelli and Cr Ross Paine</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p> <p><b>EN BLOC</b></p>		
20/09/2023	Youth Plan	Completed	<p><b>COUNCIL DECISION</b></p> <p><b>C2309/114</b> Moved Cr Anne Ryan, seconded Cr Paul Carter</p> <p>That the Council adopt the Youth Plan 'What's up in your world' (Attachment 1) as a guiding document for the delivery of youth services for the City of Busselton.</p> <p style="text-align: right;"><b>CARRIED 8 / 0</b></p> <p><b>FOR: Cr Grant Henley, Cr Paul Carter, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Cr Phill Cronin, Cr Sue Riccelli and Cr Ross Paine</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p> <p><b>EN BLOC</b></p> <p>Please action as per resolution.</p>	<p><b>29/09/2023 Angela Griffin</b> Emailed Ana Marsic a copy of the youth plan to be listed on public website</p> <p><b>29/09/2023 Angela Griffin</b> Uploaded document into ECM</p> <p>Id doc url <a href="https://cob.t1cloud.com/T1Default/CiAnywhere/Web/COB/ECMCore/Rendition/Get?docsetId=5226259&amp;renditionType=pdf&amp;suite=ECM">https://cob.t1cloud.com/T1Default/CiAnywhere/Web/COB/ECMCore/Rendition/Get?docsetId=5226259&amp;renditionType=pdf&amp;suite=ECM</a></p> <p><b>29/09/2023 Angela Griffin</b> Emailed precision print for a quote to print high quality booklets</p> <p><b>29/09/2023 Angela Griffin</b> Emailed a copy of the the plan to all organisations on the Cape-to-Cape youth network meeting. 90 email addresses</p> <p><b>02/10/2023 Angela Griffin</b> Quote received and printing will commence asap. Youth plan is on website under youth- youth programs</p> <p><b>17/10/2023 Angela Griffin</b> Received paper copy of the youth plan</p>	17/10/2023

20/09/2023	2023/2024 Community Assistance Program Round 1 Outcomes	Completed	<p>Please action as per resolution.</p> <p><b>COUNCIL DECISION</b></p> <p><b>C2309/115</b> Moved Cr Anne Ryan, seconded Cr Paul Carter</p> <p>That the Council endorses the following funding allocation, to be funded from the 2023/2024 Community Assistance Program budget:</p> <table border="1" data-bbox="756 478 1774 730"> <thead> <tr> <th>Applicant</th> <th>Requested</th> <th>Recommendation</th> </tr> </thead> <tbody> <tr> <td>Cape Mountain Bikers</td> <td>\$30,000</td> <td>{resolution}</td> </tr> <tr> <td>Advocacy WA</td> <td>\$10,000</td> <td>{resolution}</td> </tr> <tr> <td>Busselton City Football Club</td> <td>\$6,000</td> <td>\$6,000</td> </tr> <tr> <td>West Busselton Primary School</td> <td>\$9,600</td> <td>{resolution}</td> </tr> <tr> <td>RSPCA WA</td> <td>\$5,400</td> <td>\$3,500</td> </tr> <tr> <td><b>TOTAL</b></td> <td><b>\$61,000</b></td> <td><b>\$9,500</b></td> </tr> </tbody> </table> <p style="text-align: right;"><b>CARRIED 8 / 0</b></p> <p style="text-align: right;"><b>FOR: Cr Grant Henley, Cr Paul Carter, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Cr Phill Cronin, Cr Sue Riccelli and Cr Ross Paine</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p> <p><b>EN BLOC</b></p>	Applicant	Requested	Recommendation	Cape Mountain Bikers	\$30,000	{resolution}	Advocacy WA	\$10,000	{resolution}	Busselton City Football Club	\$6,000	\$6,000	West Busselton Primary School	\$9,600	{resolution}	RSPCA WA	\$5,400	\$3,500	<b>TOTAL</b>	<b>\$61,000</b>	<b>\$9,500</b>	<p><b>02/10/2023 Jessica Watts</b></p> <p>Grant agreements drafted. Council Resolution number entered into agreements from most recent Council minutes.</p>	30/10/2023
Applicant	Requested	Recommendation																								
Cape Mountain Bikers	\$30,000	{resolution}																								
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<b>TOTAL</b>	<b>\$61,000</b>	<b>\$9,500</b>																								
20/09/2023	Adoption of Corporate Business Plan 2023-2027	Completed	<p>Please action as per resolution.</p> <p>Prior to the meeting, officers foreshadowed an amendment to the officer recommendation. The amended recommendation was moved first and carried.</p> <p><b>COUNCIL DECISION</b></p> <p><b>C2309/116</b> Moved Cr Ross Paine, seconded Cr Anne Ryan</p> <p>That the Council adopts the Corporate Business Plan 2023-2027 in the form attached to this report, subject to:</p> <p>1. the following action being added to Key Theme 2 Lifestyle:</p> <table border="1" data-bbox="756 1392 1908 1690"> <thead> <tr> <th rowspan="2">Action</th> <th rowspan="2">Strategic Priority Link</th> <th rowspan="2">Supporting Plans and Strategies</th> <th rowspan="2">Action Owners</th> <th rowspan="2">Action Type / Budget</th> <th colspan="3">Delivery</th> </tr> <tr> <th>23-24</th> <th>24-25</th> <th>25-26</th> </tr> </thead> <tbody> <tr> <td><b>Holiday Homes Regulations</b> Further to outcomes of the State Government's review, consider further changes to the City's Holiday Homes Regulatory Framework.</td> <td>2.8</td> <td></td> <td>Strategic Planning</td> <td>Program Operational</td> <td>X</td> <td></td> <td></td> </tr> </tbody> </table> <p>1. the retiring action 'Holiday Homes Regulations' being removed.</p> <p style="text-align: right;"><b>CARRIED 8 / 0</b></p>	Action	Strategic Priority Link	Supporting Plans and Strategies	Action Owners	Action Type / Budget	Delivery			23-24	24-25	25-26	<b>Holiday Homes Regulations</b> Further to outcomes of the State Government's review, consider further changes to the City's Holiday Homes Regulatory Framework.	2.8		Strategic Planning	Program Operational	X			<p><b>04/10/2023 Sarah Pierson</b></p> <p>Corporate Business Plan finalised and published post adoption. Available in hard copy format and on the City's website.</p>	04/10/2023		
Action	Strategic Priority Link	Supporting Plans and Strategies	Action Owners						Action Type / Budget	Delivery																
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			<p><b>FOR: Cr Grant Henley, Cr Paul Carter, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Cr Phill Cronin, Cr Sue Riccelli and Cr Ross Paine</b></p> <p><b>AGAINST: Nil</b></p> <p>BY ABSOLUTE MAJORITY</p> <p>Reasons: Council indicated a desire to retail an action associated with the regulation of holiday homes, with stage 3 of the initiated review pending State Government action.</p>		
20/09/2023	Notice of Motion - Recognition of Nina Kennedy	Completed	<p>Please action as per resolution.</p> <p><b>COUNCIL DECISION</b></p> <p><b>C2309/118</b> Moved Cr Grant Henley, seconded Cr Anne Ryan</p> <p>That the Council requests the CEO to prepare a report outlining potential recognition of Nina Kennedy.</p> <p><b>CARRIED 8 / 0</b></p> <p><b>FOR: Cr Grant Henley, Cr Paul Carter, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Cr Phill Cronin, Cr Sue Riccelli and Cr Ross Paine</b></p> <p><b>AGAINST: Nil</b></p>	04/10/2023 Sarah Pierson	05/10/2023
20/09/2023	Notice of Motion - Introduction of Recreation Facilities Reserve	In Progress	<p>Please action as per resolution.</p> <p><b>COUNCIL DECISION</b></p> <p><b>C2309/119</b> Moved Cr Grant Henley, seconded Cr Anne Ryan</p> <p>That Council requests the CEO to prepare a report that considers the introduction of a dedicated Recreation Facilities Reserve as part of budget preparations and review of the Long-Term Financial Plan.</p> <p><b>CARRIED 8 / 0</b></p> <p><b>FOR: Cr Grant Henley, Cr Paul Carter, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Cr Phill Cronin, Cr Sue Riccelli and Cr Ross Paine</b></p> <p><b>AGAINST: Nil</b></p>	04/10/2023 Sarah Pierson	
18/10/2023	Audit and Risk Committee - 13/09/2023 - CEO Review of Systems and Procedures (REG 17) - Update on Recommendations	Completed	<p>Please action as per resolution.</p> <p><b>COUNCIL DECISION</b></p> <p><b>C2310/123</b> Moved Cr Jodie Richards, seconded Cr Paul Carter</p> <p>That the Council acknowledge the status update in relation to the recommendations of the Regulation 17 Review (as at the beginning of September 2023) and note that the next update will be provided to the Audit and Risk Committee post the conclusion of the December 2023 quarter.</p> <p><b>CARRIED 9 / 0</b></p> <p><b>FOR: Cr Grant Henley, Cr Paul Carter, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Cr Phill Cronin, Cr Sue Riccelli, Cr Mikayla Love and Cr Ross Paine</b></p>	01/12/2023 Tim Allingham	01/12/2023

			<b>AGAINST: Nil</b>		
18/10/2023	Policy and Legislation Committee - 13/09/2023 - Bush Fire Brigades Local Law	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2310/124</b> Moved Cr Jodie Richards, seconded Cr Paul Carter</p> <p>That the Council:</p> <ol style="list-style-type: none"> <li>1. Resolves to make the <i>City of Busselton Bushfire Brigades Local Law 2023</i> in accordance with section 3.12(4) of the <i>Local Government Act 1995</i>.</li> <li>2. Authorises the Chief Executive Officer to carry out the processes required to make the <i>City of Busselton Bushfire Brigades Local Law 2023</i> in accordance with section 3.12(5) and section 3.12(6) of the <i>Local Government Act 1995</i>.</li> </ol> <p style="text-align: right;"><b>CARRIED 9 / 0</b></p> <p><b>FOR: Cr Grant Henley, Cr Paul Carter, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Cr Phill Cronin, Cr Sue Riccelli, Cr Mikayla Love and Cr Ross Paine</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p> </div>	01/11/2023 Briony McGinty Gazetted on 31 October 2023.	
18/10/2023	Policy and Legislation Committee - 13/09/2023 - Council Policy Review - Compliance and Enforcement Policy	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2310/125</b> Moved Cr Jodie Richards, seconded Cr Paul Carter</p> <p>That the Council:</p> <ol style="list-style-type: none"> <li>1. Rescind the following Council policies: <ol style="list-style-type: none"> <li>a. Compliance Policy (Attachment 1)</li> <li>b. Ranger and Emergency Services – Approach to Regulatory Functions Policy (Attachment 2)</li> </ol> </li> <li>2. Adopt the Compliance and Enforcement Policy (Attachment 3).</li> </ol> <p style="text-align: right;"><b>CARRIED 9 / 0</b></p> <p><b>FOR: Cr Grant Henley, Cr Paul Carter, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Cr Phill Cronin, Cr Sue Riccelli, Cr Mikayla Love and Cr Ross Paine</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p> </div>	01/11/2023 Jo Barrett-Lennard Policies rescinded and new policy on website	01/11/2023

18/10/2023	Policy and Legislation Committee - 13/09/2023 - Council Policy Review - Portable Advertising Signs in Public Places	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2310/126</b> Moved Cr Jodie Richards, seconded Cr Paul Carter</p> <p>That the Council readopt the Portable Advertising Signs in Public Places Policy (Attachment 1).</p> <p style="text-align: right;"><b>CARRIED 9 / 0</b></p> <p><b>FOR: Cr Grant Henley, Cr Paul Carter, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Cr Phill Cronin, Cr Sue Riccelli, Cr Mikayla Love and Cr Ross Paine</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p> </div>	01/11/2023 Jo Barrett-Lennard Policy re-adopted	01/11/2023
18/10/2023	Policy and Legislation Committee - 13/09/2023 - Proposed Local Planning Policy: Identification of Complex Development Applications - Consideration for Final Adoption after Advertising	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2310/127</b> Moved Cr Jodie Richards, seconded Cr Paul Carter</p> <p>That the Council, pursuant to clauses 3 and 4 of Schedule 2 of the <i>Planning and Development (Local Planning Schemes) Regulations 2015</i>, proceeds with Local Planning Policy: Complex Development Applications (Attachment 1) without modification.</p> <p style="text-align: right;"><b>CARRIED 9 / 0</b></p> <p><b>FOR: Cr Grant Henley, Cr Paul Carter, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Cr Phill Cronin, Cr Sue Riccelli, Cr Mikayla Love and Cr Ross Paine</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p> </div>		
18/10/2023	Policy and Legislation Committee - 13/09/2023 - Various Council Policy Reviews - Environment, Investment, Loan Facilities, Payment and Presentations on Termination, Misconduct, Fraud and Corruption	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2310/128</b> Moved Cr Jodie Richards, seconded Cr Paul Carter</p> <p>That the Council adopts the following policies to replace the current council policies, with the policies having been reviewed as part of the City's ongoing policy review cycle:</p> <ol style="list-style-type: none"> <li>1. Environment (Attachment 1)</li> <li>2. Investment (Attachment 2)</li> <li>3. Loan Facilities (Attachment 3)</li> <li>4. Payment and Presentation on Termination (Attachment 4)</li> <li>5. Misconduct, Fraud and Corruption (Attachment 5)</li> </ol> <p style="text-align: right;"><b>CARRIED 9 / 0</b></p> <p><b>FOR: Cr Grant Henley, Cr Paul Carter, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Cr Phill Cronin, Cr Sue Riccelli, Cr Mikayla Love and Cr Ross Paine</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p> </div>	01/11/2023 Jo Barrett-Lennard Policies updated as required	01/11/2023
18/10/2023	Policy and Legislation Committee -	Completed	Please action as per resolution.	01/12/2023 Tegan Robertson	01/12/2023

	13/09/2023 - Statutory Review of Delegations of Authority		<p><b>COUNCIL DECISION</b></p> <p><b>C2310/130</b> Moved Cr Grant Henley, seconded Cr Phill Cronin</p> <p>That the Council adopts the City of Busselton Delegations of Authority as per the Committee Recommendation, with the exception of delegation <i>DA 7 – 01 Development Control</i> which be amended as set out below:</p> <p>Delete the current condition 2:</p> <p><b>2. Applications in Special Character Areas requiring the exercise of discretion</b></p> <p><i>All applications for development approval affecting land within the Special Character Areas, as identified in schedule 4 of the Scheme, and which involve the exercise of discretion, must be brought to the Council for determination, unless the application has been approved under delegated authority.</i></p> <p>Replace the current condition 2 with the following new condition:</p> <p><b>2. Applications in Special Character Areas</b></p> <p><i>*removed for brevity</i></p>	Updated delegations issued and Delegations of Authority register re-published on City website															
18/10/2023	Finance Committee - 04/10/2023 - Budget Amendment - Reserve Closures	Completed	<p>Please action as per resolution.</p> <p><b>COUNCIL DECISION</b></p> <p><b>C2310/131</b> Moved Cr Anne Ryan, seconded Cr Sue Riccelli</p> <p>That the Council:</p> <p><b>1. Closes the following reserves:</b></p> <table border="1" data-bbox="747 1339 1911 1864"> <thead> <tr> <th>Reserve Name</th> <th>Purpose of the Reserve</th> </tr> </thead> <tbody> <tr> <td>Commonage Precinct Infrastructure Road Reserve</td> <td>To be utilised for the purpose of road infrastructure and road safety upgrades within the Commonage Contribution Area in accordance with the Commonage Contributions Area policy provisions.</td> </tr> <tr> <td>Community Facilities - City District</td> <td>To hold development contributions received by the City for the provision of new or upgraded community infrastructure within the District.</td> </tr> <tr> <td>Community Facilities – Broadwater</td> <td>To hold development contributions received by the City for the provision of new or upgraded community infrastructure within the Precinct as per the adopted contributions plan.</td> </tr> <tr> <td>Community Facilities – Busselton</td> <td>To hold development contributions received by the City for the provision of new or upgraded community infrastructure within the Precinct as per the adopted contributions plan.</td> </tr> <tr> <td>Community Facilities – Dunsborough</td> <td>To hold development contributions received by the City for the provision of new or upgraded community infrastructure within the Precinct as per the adopted contributions plan.</td> </tr> <tr> <td>Community Facilities - Dunsborough Lakes Estate</td> <td>To hold development contributions received by the City for the provision of new or upgraded community infrastructure within the Precinct as per the adopted contributions plan.</td> </tr> </tbody> </table>	Reserve Name	Purpose of the Reserve	Commonage Precinct Infrastructure Road Reserve	To be utilised for the purpose of road infrastructure and road safety upgrades within the Commonage Contribution Area in accordance with the Commonage Contributions Area policy provisions.	Community Facilities - City District	To hold development contributions received by the City for the provision of new or upgraded community infrastructure within the District.	Community Facilities – Broadwater	To hold development contributions received by the City for the provision of new or upgraded community infrastructure within the Precinct as per the adopted contributions plan.	Community Facilities – Busselton	To hold development contributions received by the City for the provision of new or upgraded community infrastructure within the Precinct as per the adopted contributions plan.	Community Facilities – Dunsborough	To hold development contributions received by the City for the provision of new or upgraded community infrastructure within the Precinct as per the adopted contributions plan.	Community Facilities - Dunsborough Lakes Estate	To hold development contributions received by the City for the provision of new or upgraded community infrastructure within the Precinct as per the adopted contributions plan.	01/11/2023 Paul Sheridan Completed	01/11/2023
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18/10/2023	Proposed Amendment 57 To Local Planning Scheme No. 21 - Lots 1 (No 45A) and 2 (No 45B) Clydebank Avenue West Busselton	Completed	<p>Please action as per resolution.</p> <p><b>COUNCIL DECISION</b></p> <p><b>C2310/134</b> Moved Cr Jodie Richards, seconded Cr Paul Carter</p> <p><b>That the Council:</b></p> <ol style="list-style-type: none"> <li>1. In pursuance of the <i>Planning and Development (Local Planning Schemes) Regulations 2015</i>, (the Regulations) adopts Amendment 57 to the City of Busselton Local Planning Scheme 21 for final approval, in accordance with the modifications proposed in the ‘Schedule of Modifications’ shown at Attachment 5, for the purposes of:             <ol style="list-style-type: none"> <li>a. Amending the zoning of Lot 1 (No 45A) Clydebank Avenue, West Busselton from ‘Local Centre’ to ‘Reserve for Public Purposes’ (Water Authority); and</li> <li>b. Amending the zoning of Lot 2 (No 45B) Clydebank Avenue, West Busselton from ‘Local Centre’ to ‘Residential’ (R30).</li> </ol> </li> <li>2. Advise the Western Australian Planning Commission (WAPC) that Amendment 57 is considered a standard amendment pursuant to the Regulations for the following reasons:             <ol style="list-style-type: none"> <li>a. An amendment that is consistent with a Local Planning Strategy for the Scheme that has been endorsed by the Commission.</li> </ol> </li> <li>3. Pursuant to r. 53 of the Regulations, endorses the Schedule of Submissions at Attachment 4 which has been prepared in response to the public consultation process</li> <li>4. Upon preparation of the necessary documentation, refers Amendment 57 to the WAPC for consideration and determination in accordance with the <i>Planning and Development Act 2005</i>.</li> </ol>	<p><b>01/11/2023 Justin Biggar</b></p> <p>Amendment documentation forwarded to the WAPC on 31/10/2023</p>	01/11/2023

			<p style="text-align: right;"><b>CARRIED 9 / 0</b></p> <p style="text-align: center;"><b>FOR: Cr Grant Henley, Cr Paul Carter, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Cr Phill Cronin, Cr Sue Riccelli, Cr Mikayla Love and Cr Ross Paine</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p>		
18/10/2023	Development Application - DA20/0437.01 - Industry - Extractive - 157 Haag Road, Yelverton	Completed	<p>Please action as per resolution.</p> <p><b>COUNCIL DECISION</b></p> <p><b>C2310/137</b> Moved Cr Sue Riccelli, seconded Cr Paul Carter</p> <p><b>That the Council determines:</b></p> <p><b>A. That application DA20/0437.01 submitted for ‘Amendment to Development Approval - Industry – Extractive’ (sand) at Lot 75 (157), Haag Road, Yelverton is considered by the Council to be generally consistent with Local Planning Scheme No. 21 and the objectives of the zone within which it is located.</b></p> <p><b>B. That the conditions of Development Approval for the proposal referred to in (A) above be amended as follows –</b></p> <p>General Conditions</p> <p><i>*removed for brevity</i></p>	04/12/2023 Jo Barrett-Lennard Action completed	01/11/2023
18/10/2023	Application for Development Approval - DA23/0359 – Holiday Home (Single House) 8 People – Lot 101 (18) Eagle Crescent, Eagle Bay	Completed	<p>Please action as per resolution.</p> <p><b>COUNCIL DECISION</b></p> <p><b>C2310/138</b> Moved Cr Jodie Richards, seconded Cr Paul Carter</p> <p><b>That the Council:</b></p> <p><b>1. Consider that the application for a Change of Use Holiday Home (Single House) 8 People is generally consistent with Local Planning Scheme No. 21 and the objectives of the zone in which it is located; and</b></p> <p><b>2. Grant development approval for the proposal subject to the following conditions:</b></p> <p><b>GENERAL CONDITIONS:</b></p> <p><b>1. The development hereby approved shall be substantially commenced within two years of the date of this decision notice, if not substantially commenced within this period, the approval will lapse and be of no further effect.</b></p> <p><b>2. The use hereby approved shall be in accordance with the Approved Plan dated 18 October 2023 and endorsed Bushfire Management Plan and Emergency Evacuation Plan (Version 1 dated 19 May 2023) including any conditions placed thereon by the City of Busselton.</b></p> <p><b>3. The use hereby approved shall at all times comply with the definition of Holiday Home (Single House) as provided in Schedule 1 of the City of Busselton Local Planning Scheme No. 21, as follows:</b></p> <p><i>“Holiday Home (Single House)” means a single house (excluding ancillary accommodation), which may also be used for short stay accommodation for hire or reward for no more than 12 people (but does not include a bed and breakfast, chalet development, guesthouse, rural tourist accommodation or tourist accommodation).</i></p>		

Notwithstanding the above interpretation, the approved use is restricted to a maximum of eight persons.

4. Advertising signage associated with the approved use shall be no greater than 0.2m<sup>2</sup>, located within the subject site.

5. A minimum number of three car parking bays shall be provided on site.

**PRIOR TO COMMENCEMENT OF ANY WORKS CONDITIONS:**

6. The development hereby approved, or any works required to implement the development, shall not commence until the following plans or details have been submitted to the City and approved in writing:

6.1 A notification in the form of a section 70A notification, pursuant to the *Transfer of Lands Act 1893* (as amended) is to be placed on the Certificate of Title of the lot advising that:

*“This land is within a bushfire prone area as designated by an Order made by the Fire and Emergency Services Commissioner. The approval of the Holiday Home (Single House) is conditional upon the details contained within the Bushfire Management Plan (BMP) prepared by Bushfire Prone Planning, Version 1, dated 19 May 2023 and the accompanying Bushfire Emergency Evacuation Plan (BEEP).”*

A copy of the Certificate of Title with section 70A notification registered against it, or a Landgate lodgement receipt, is to be submitted to the City.

6.2 Details of on-site effluent disposal works.

6.3 Details of the means and method of providing a potable water supply of 135,000 litres or equivalent alternative water supply as otherwise approved in writing by the City.

**PRIOR TO OCCUPATION/USE OF THE DEVELOPMENT CONDITIONS:**

7. The development hereby approved shall not be occupied, or used, until all plans, details or works required by Conditions 2, 3, 4, 5 and 6 have been implemented.

**ONGOING CONDITIONS:**

8. The works undertaken to satisfy Conditions 2, 3, 4, 5 and 6 shall be subsequently maintained for the life of the development and the following conditions complied with:

1. The owner must implement the approved Bushfire Management Plan and Bushfire Emergency Evacuation Plan prepared by Bushfire Prone Planning, Version 1, dated 19 May 2023 for the duration of the development.

**CARRIED 9 / 0**

**FOR: Cr Grant Henley, Cr Paul Carter, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Cr Phill Cronin, Cr Sue Riccelli, Cr Mikayla Love and Cr Ross Paine**

**AGAINST: Nil**

18/10/2023	Bushfire Notice Review	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2310/139</b> Moved Cr Jodie Richards, seconded Cr Paul Carter</p> <p>That the Council, with respect of the review of the City's Bushfire Notice:</p> <ol style="list-style-type: none"> <li>1. Notes that this report satisfies point 3 of the previous resolution (C2209/228); and</li> <li>2. Supports the planned consultation approach and timeline as described within the officer comment section of this report.</li> </ol> <p style="text-align: right;"><b>CARRIED 9 / 0</b></p> <p style="text-align: right;"><b>FOR: Cr Grant Henley, Cr Paul Carter, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Cr Phill Cronin, Cr Sue Riccelli, Cr Mikayla Love and Cr Ross Paine</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p> </div>	<p><b>06/12/2023 Rachel Runco</b></p> <p>Consultation planning underway, with Community engagement planned for 9 weeks from mid January 2024 - end of March 2024.</p>	08/01/2024
18/10/2023	Surf Lifesaving Services 2023/24 Season	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2310/140</b> Moved Cr Kate Cox, seconded Cr Phill Cronin</p> <p>That the Council enter into the optional third season Surf Lifesaving WA (SLSWA) contract to provide professional beach lifeguarding services for 2023/24 Season 3: (15 December 2023 to 30 January 2024).</p> <p style="text-align: right;"><b>CARRIED 9 / 0</b></p> <p style="text-align: right;"><b>FOR: Cr Grant Henley, Cr Paul Carter, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Cr Phill Cronin, Cr Sue Riccelli, Cr Mikayla Love and Cr Ross Paine</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p> </div>	<p><b>02/11/2023 Adrian Fisher</b></p> <p>Letter of Award to SLSWA for season 15 December 2023 to 30 January 2024:</p> <ul style="list-style-type: none"> <li>- Draft Letter of Award sent to legal and approved</li> <li>- Letter of Award and General conditions sent to Paul Needham for signing</li> <li>- Signed Letter of Award and General conditions sent to Cam Robbins SLSWA for signing</li> </ul>	02/11/2023
18/10/2023	Application for Development Approval (DA23/0434) - Single House (Outbuilding) - Lot 6 (4) Wilson Avenue Quindalup	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2310/141</b> Moved Cr Jodie Richards, seconded Cr Paul Carter</p> <ol style="list-style-type: none"> <li>1. Consider that the application for a Single House (Outbuilding) is generally consistent with Local Planning Scheme No. 21 and the objectives of the zone in which it is located; and</li> <li>2. Grant development approval for the proposal subject to the following conditions:</li> </ol> <p><b>GENERAL CONDITIONS:</b></p> <ol style="list-style-type: none"> <li>1. The development hereby approved shall be substantially commenced within two years of the date of this decision notice.</li> </ol> </div>	<p><b>01/11/2023 Jo Barrett-Lennard</b></p> <p>Issued decision notice 26 October 2023</p>	01/11/2023

			<p>2. The development hereby approved shall be undertaken in accordance with the signed and stamped Approved Development Plans(s), including any notes placed thereon in red by the City of Busselton.</p> <p><b>ONGOING CONDITIONS:</b></p> <p>3. The works undertaken to satisfy Condition 2 shall be subsequently maintained for the life of the development, and the following Condition(s) complied with:</p> <p>3.1 Stormwater to be retained for use and/or infiltration within the lot at a rate of 1m<sup>3</sup> per 65m<sup>2</sup> of impervious area.</p> <p style="text-align: right;"><b>CARRIED 9 / 0</b></p> <p><b>FOR: Cr Grant Henley, Cr Paul Carter, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Cr Phill Cronin, Cr Sue Riccelli, Cr Mikayla Love and Cr Ross Paine</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p>		
18/10/2023	RFT12/23 Jalbarragup Rd Shoulder Widening and Reseal	Completed	<p>Please action as per resolution.</p> <p><b>COUNCIL DECISION</b></p> <p><b>C2310/142</b> Moved Cr Jodie Richards, seconded Cr Paul Carter</p> <p>That the Council endorses the outcome of the evaluation panel's assessment of RFT12/23 Jalbarragup Rd Shoulder Widen and Reseal and accepts the tender from Leeuwin Civil Pty Ltd with as most advantageous.</p> <p style="text-align: right;"><b>CARRIED 9 / 0</b></p> <p><b>FOR: Cr Grant Henley, Cr Paul Carter, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Cr Phill Cronin, Cr Sue Riccelli, Cr Mikayla Love and Cr Ross Paine</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p>	01/11/2023 Kevin Bowser Contract awarded	01/11/2023
18/10/2023	Relinquish management over portion of Holgate Reserve 41550	Completed	<p>Please action as per resolution.</p> <p><b>COUNCIL DECISION</b></p> <p><b>C2310/143</b> Moved Cr Jodie Richards, seconded Cr Paul Carter</p> <p>That the Council endorses excision of an area up to 629m from Lot 4896 on Crown Reserve 41550 as shown hatched on Attachment 1 to enable the adjoining landowner to acquire the land from the State, to facilitate continuity of use.</p> <p style="text-align: right;"><b>CARRIED 9 / 0</b></p> <p><b>FOR: Cr Grant Henley, Cr Paul Carter, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Cr Phill Cronin, Cr Sue Riccelli, Cr Mikayla Love and Cr Ross Paine</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p>	15/12/2023 Ann Strang The proposed excision of land from the Reserve was advertised for public comment. Submissions closed 13 December 2023. No submissions were received. The City wrote to the Department of Planning, Lands and Heritage confirming consent to the excision.	15/12/2023

18/10/2023	2023 Review of Committees, Working Groups and Terms of Reference	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2310/147</b> Moved Cr Sue Riccelli, seconded Cr Kate Cox</p> <p>That the Council,</p> <p>1. Pursuant to s. 5.8 of the <i>Local Government Act 1995</i>, retains the following Committees:</p> <ul style="list-style-type: none"> <li>a. Audit and Risk Committee</li> <li>b. Finance Committee</li> <li>c. Policy and Legislation Committee</li> <li>d. Airport Advisory Committee</li> <li>e. CEO Performance Review Committee</li> <li>f. Behaviour Complaints Committee</li> <li>g. Meelup Regional Park Committee</li> </ul> <p><i>*removed for brevity</i></p> <p style="text-align: right;"><b>CARRIED 8 / 1</b></p> <p><b>FOR: Cr Grant Henley, Cr Paul Carter, Cr Anne Ryan, Cr Jodie Richards, Cr Phill Cronin, Cr Sue Riccelli , Cr Mikayla Love and Cr Ross Paine</b></p> <p style="text-align: right;"><b>AGAINST: Cr Kate Cox</b></p> </div>	<p><b>01/11/2023 Jo Barrett-Lennard</b></p> <p>Completed in accordance with the 21 October 2023 Election</p>	22/12/2023				
18/10/2023	Recognition of Nina Kennedy	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2310/148</b> Moved Cr Jodie Richards, seconded Cr Paul Carter</p> <p>That the Council supports the provision of a letter from the Mayor on behalf of the City of Busselton, recognising and congratulating Nina Kennedy on her achievements at the 2023 World Athletics Championships, and exploring the opportunity for Ms. Kennedy to attend the upcoming City of Busselton Sports Awards.</p> <p style="text-align: right;"><b>CARRIED 9 / 0</b></p> <p><b>FOR: Cr Grant Henley, Cr Paul Carter, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Cr Phill Cronin, Cr Sue Riccelli, Cr Mikayla Love and Cr Ross Paine</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p> </div>	<p><b>01/11/2023 Tegan Robertson</b></p> <p>Letter of congratulations issued. Ms. Kennedy provided a video message to share at the City of Busselton Sports Awards.</p>	01/11/2023				
18/10/2023	Budget Amendment Bussell Highway Resurfacing SLK 0 to 3.8	Completed	<p>Please action as per resolution</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2310/151</b> Moved Cr Kate Cox, seconded Cr Paul Carter</p> <p>That the Council endorse the following requested budget amendments:</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="width: 10%;"></th> <th style="width: 90%;">Amendment Description</th> </tr> </thead> <tbody> <tr> <td></td> <td>Project Description</td> </tr> </tbody> </table> </div>		Amendment Description		Project Description	<p><b>02/11/2023 Paul Sheridan</b></p> <p>Processed by Finance</p> <p><b>02/11/2023 Matthew Twyman</b></p> <p>Physical works have begun on Bussell Highway and intersection works will follow.</p>	02/11/2023
	Amendment Description								
	Project Description								

Item #	Budgeted Municipal Net Current Position	Operational Expenditure Budget	Capital Expenditure Budget	Grant	Reserves or Restricted Assets
	Increase / (Decrease)	Increase / (Decrease)	Increase / (Decrease)	(Increase) / Decrease	Increase / (Decrease)
<b>New capital project</b>					
1	Project 15108 - Bussell Highway Resurfacing SLK 0 to 3.8 (C)				
	-	-	181,000	-	(181,000)
<b>Remove capital project</b>					
2	Project 14857 West Street - Rehabilitation SLK 1.02 to 1.15 (C)				
	-	-	(181,000)	-	181,000
<b>TOTAL S</b>	-	-	-	-	-

**CARRIED 9 / 0**

**FOR: Cr Grant Henley, Cr Paul Carter, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Cr Phill Cronin, Cr Sue Riccelli, Cr Mikayla Love and Cr Ross Paine**

**AGAINST: Nil**

15/11/2023

Budget Amendments - Infrastructure and Environment

Completed

Please action as per resolution.

**COUNCIL DECISION**

**C2311/169** Moved Cr Jodie Richards, seconded Cr Andrew Macnish

Amendment Description					
Project Description					
Item #	Budgeted Municipal Net Current Position	Operational Expenditure Budget	Capital Expenditure Budget	Grant	Reserves or Restricted Assets
	Increase / (Decrease)	Increase / (Decrease)	Increase / (Decrease)	(Increase) / Decrease	Increase / (Decrease)
<b>Increase Capital Budget</b>					
1	Project 15103 - Prince Street Laneway (C)				
	-	-	28,003	-	(28,003)
<b>Reduce Capital Budget on Existing Project</b>					
2	Project 14859 Kershaw Street – Asphalt Overlay SLK 0 to 0.25 (C)				
	-	-	(18,825)	-	18,825
<b>Reduce Capital Budget on Existing Project</b>					
3	Project 14858 Artisan Street – Asphalt Overlay SLK 0.02 to 0.16 (C)				
	-	-	(9,178)	-	9,178
<b>Remove Capital Project</b>					
4	Project 14875 – Eagle Bay Boat Ramp Parking off Fern Road (C)				
	-	-	(30,000)	-	30,000
5	<b>Add Budget to Existing Capital Project</b>				

			<table border="1"> <tr> <td></td> <td colspan="5"><b>Project 14876 Eagle Crescent on Road Parking – Footpath Alignment (C)</b></td> </tr> <tr> <td></td> <td>-</td> <td>-</td> <td>23,000</td> <td>-</td> <td>(23,000)</td> </tr> <tr> <td rowspan="2">6a</td> <td colspan="5"><b>Add Budget to Existing Capital Project</b></td> </tr> <tr> <td colspan="5"><b>Project 11986 – Causeway Road Shared Path (C)</b></td> </tr> <tr> <td></td> <td>-</td> <td>-</td> <td>30,000</td> <td>-</td> <td>(30,000)</td> </tr> <tr> <td rowspan="2">6b</td> <td colspan="5"><b>Add Grant Revenue for Existing Capital Project</b></td> </tr> <tr> <td colspan="5"><b>Project 11986 – Causeway Road Shared Path (C)</b></td> </tr> <tr> <td></td> <td>-</td> <td>-</td> <td>75,000</td> <td>(75,000)</td> <td>-</td> </tr> <tr> <td rowspan="2">7</td> <td colspan="5"><b>Increase Capital Budget</b></td> </tr> <tr> <td colspan="5"><b>Project 11998 – Kaloorup Road Reseal SLK 0.82 to 8.76 (C)</b></td> </tr> <tr> <td></td> <td>-</td> <td>-</td> <td>60,000</td> <td>-</td> <td>(60,000)</td> </tr> <tr> <td rowspan="2">8</td> <td colspan="5"><b>Reduce Capital Budget</b></td> </tr> <tr> <td colspan="5"><b>Project 14850 – Cook Street Asphalt Overlay SLK 0.39 to 1.25 (C)</b></td> </tr> <tr> <td></td> <td>-</td> <td>-</td> <td>(60,000)</td> <td>-</td> <td>60,000</td> </tr> <tr> <td><b>Totals</b></td> <td>-</td> <td>-</td> <td>98,000</td> <td>(75,000)</td> <td>(23,000)</td> </tr> </table> <p style="text-align: right;"><b>CARRIED 7 / 0</b></p> <p style="text-align: right;"><b>FOR: Cr Andrew Macnish, Cr Jarrod Kennedy, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Mayor Phill Cronin and Cr Sue Riccelli</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p> <p style="text-align: right;"><b>BY ABSOLUTE MAJORITY</b></p>		<b>Project 14876 Eagle Crescent on Road Parking – Footpath Alignment (C)</b>						-	-	23,000	-	(23,000)	6a	<b>Add Budget to Existing Capital Project</b>					<b>Project 11986 – Causeway Road Shared Path (C)</b>						-	-	30,000	-	(30,000)	6b	<b>Add Grant Revenue for Existing Capital Project</b>					<b>Project 11986 – Causeway Road Shared Path (C)</b>						-	-	75,000	(75,000)	-	7	<b>Increase Capital Budget</b>					<b>Project 11998 – Kaloorup Road Reseal SLK 0.82 to 8.76 (C)</b>						-	-	60,000	-	(60,000)	8	<b>Reduce Capital Budget</b>					<b>Project 14850 – Cook Street Asphalt Overlay SLK 0.39 to 1.25 (C)</b>						-	-	(60,000)	-	60,000	<b>Totals</b>	-	-	98,000	(75,000)	(23,000)		
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15/11/2023	Application for Development Approval (DA22/0806) - Single House - Lots 41 & 42 Caves Road, Marybrook	Not yet started	<p>Please action as per resolution.</p> <p><b>COUNCIL DECISION</b></p> <p><b>C2311/170</b> Moved Cr Kate Cox, seconded Cr Anne Ryan</p> <p>That the Council determines:</p> <p>1. That application DA22/0806 submitted for development of a Single House on Lots 41 and 42 Caves Road, Marybrook is considered by the Council to not be consistent with Local Planning Scheme No. 21 and the objectives of the zone within which it is located: and</p> <p>2. To refuse to grant development approval for the following reasons:</p> <p>a. It can be reasonably demonstrated that this proposal does not meet the following Design Principles 3.1 – Building Setbacks of the R-Codes. This application fails to satisfy the deemed to comply criteria of the R-Codes by requesting a significant reduction in the East boundary (1.5m instead of the required 7.5m) and West boundary (1.6m instead of the required 7.5m) setbacks. This will result in the bulk of the buildings being located far too close to the neighbouring boundaries and further, the loss of significant areas of native vegetation on these lots which are located wholly within an area of high landscape value.</p> <p>b. This site is zone residential R2.5 which subsequently allows for the construction of a single dwelling (1 dwelling per 4000m<sup>2</sup>). This development application could not be considered to be a single dwelling and is therefore inconsistent with the minimum area required per dwelling.</p> <p>c. This application does not comply with cl.5.4.2 of LPS 21 as the size and location of the building footprint will require the removal of a substantial amount of native WA Peppermint Tree’s (Agonis Flexuosa)</p>	13/12/2023 Lezia Ramsden	Refusal communicated to applicant																																																																																						

			<p>which will adversely impact the WRP habitat. In addition the requirement of additional siteworks and fill of up to 1.4m to achieve the finished floor of 3.4 AHD will also have a significantly detrimental effect on the flora located within these lots.</p> <p style="text-align: right;"><b>CARRIED 7 / 0</b></p> <p><b>FOR: Cr Andrew Macnish, Cr Jarrod Kennedy, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Mayor Phill Cronin and Cr Sue Riccelli</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p>		
15/11/2023	DA21/0659 - Vacant Land (Bulk Fill of Future Subdivision) Special Control Area	Completed	<p>Please action as per resolution. The officer recommendation was moved and carried.</p> <p><b>COUNCIL DECISION</b></p> <p><b>C2311/171</b> Moved Cr Anne Ryan, seconded Cr Kate Cox</p> <p>That the Council determines:</p> <p>A. That application DA21/0659 submitted for Use Not Listed (Bulk Fill) Special Control Area at Lot 9003 Broadwater Boulevard, Broadwater is generally consistent with Local Planning Scheme No. 21 and the objectives of the zone within which it is located; and</p> <p>B. Grant development approval for the proposal subject to the following conditions:</p> <p><b>GENERAL CONDITIONS:</b></p> <p><i>*removed for brevity</i></p> <p style="text-align: right;"><b>CARRIED 7 / 0</b></p> <p><b>FOR: Cr Andrew Macnish, Cr Jarrod Kennedy, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Mayor Phill Cronin and Cr Sue Riccelli</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p> <p><b>En Bloc</b></p>	15/01/2024 Andrew Watts	Action completed

15/11/2023	2023/2024 Community Assistance Program Round 2 Outcomes	Completed	<p>Please action as per resolution.</p> <p><b>COUNCIL DECISION</b></p> <p><b>C2311/172</b> Moved Cr Sue Riccelli, seconded Cr Kate Cox</p> <p>That the Council endorses the following funding allocation, to be funded from the 2023/2024 Community Assistance Program budget:</p> <table border="1" data-bbox="759 491 1774 1058"> <thead> <tr> <th>Applicant</th> <th>Allocation</th> </tr> </thead> <tbody> <tr><td>1 Busselton Hospice Care</td><td>\$9,680.88</td></tr> <tr><td>2 Busselton Squash Club</td><td>\$10,000.00</td></tr> <tr><td>3 Dunsborough Community Men’s Shed</td><td>\$1,977.00</td></tr> <tr><td>4 Dunsborough Cricket Club</td><td>\$5,912.00</td></tr> <tr><td>5 Dunsborough Greenfield Community Group</td><td>\$5,591.97</td></tr> <tr><td>6 Dunsborough Towners Football Club</td><td>\$8,342.28</td></tr> <tr><td>7 Geographe Bay Yacht Club</td><td>\$10,000.00</td></tr> <tr><td>8 Get Skilled Access</td><td>\$10,000.00</td></tr> <tr><td>9 Smiths Beach Surf Lifesaving Club</td><td>\$10,000.00</td></tr> <tr><td>10 Southwest Junior Windsurf School</td><td>\$5,000.00</td></tr> <tr><td>11 The People Place Busselton</td><td>\$4,728.00</td></tr> <tr><td>12 Therapy Focus</td><td>\$7,000.00</td></tr> <tr><td>13 Vasse Community Garden</td><td>\$9,486.00</td></tr> <tr><td><b>TOTAL</b></td><td><b>\$97,718.13</b></td></tr> </tbody> </table> <p style="text-align: right;"><b>CARRIED 7 / 0</b></p> <p><b>FOR: Cr Andrew Macnish, Cr Jarrod Kennedy, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Mayor Phill Cronin and Cr Sue Riccelli</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p>	Applicant	Allocation	1 Busselton Hospice Care	\$9,680.88	2 Busselton Squash Club	\$10,000.00	3 Dunsborough Community Men’s Shed	\$1,977.00	4 Dunsborough Cricket Club	\$5,912.00	5 Dunsborough Greenfield Community Group	\$5,591.97	6 Dunsborough Towners Football Club	\$8,342.28	7 Geographe Bay Yacht Club	\$10,000.00	8 Get Skilled Access	\$10,000.00	9 Smiths Beach Surf Lifesaving Club	\$10,000.00	10 Southwest Junior Windsurf School	\$5,000.00	11 The People Place Busselton	\$4,728.00	12 Therapy Focus	\$7,000.00	13 Vasse Community Garden	\$9,486.00	<b>TOTAL</b>	<b>\$97,718.13</b>		30/11/2023
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15/11/2023	Business Development, Events and Marketing Program Outcomes	Completed	<p>Please action as per resolution.</p> <p><b>COUNCIL DECISION</b></p> <p><b>C2311/173</b> Moved Cr Jodie Richards, seconded Cr Jarrod Kennedy</p> <p>That the Council:</p> <p>1. Endorses the single year cash allocations towards the following economic focused events as per Column A to be funded from the 2023/2024 Events Budget, and notes the estimated in-kind support in Column B:</p> <table border="1" data-bbox="759 1747 1774 1885"> <thead> <tr> <th>Event Name</th> <th>Column A (cash allocation)</th> <th>Column B (in-kind support)</th> </tr> </thead> <tbody> <tr> <td>Nautical Drive Festival</td> <td>\$2,000</td> <td>Approx. \$1,000 (ground hire, event fees)</td> </tr> </tbody> </table>	Event Name	Column A (cash allocation)	Column B (in-kind support)	Nautical Drive Festival	\$2,000	Approx. \$1,000 (ground hire, event fees)	<p><b>01/12/2023 Peta Pulford</b></p> <p>All events have been notified of the outcome from Council. Funding agreements are currently being prepared and will be completed by end of December 2023.</p> <p><b>03/01/2024 Peta Pulford</b></p> <p>All successful funding recipients have received agreements for execution.</p>	03/01/2024																								
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15/11/2023	2024 Council and Committee meeting dates	Completed	<p>Please action as per resolution.</p> <table border="1"> <tr> <td><b>COUNCIL DECISION</b></td> </tr> <tr> <td><b>C2311/174</b> Moved Cr Andrew Macnish, seconded Cr Anne Ryan</td> </tr> </table>	<b>COUNCIL DECISION</b>	<b>C2311/174</b> Moved Cr Andrew Macnish, seconded Cr Anne Ryan	<p><b>28/11/2023 Jo Barrett-Lennard</b> Internal calendar for staff use drafted - currently in review process.</p> <p><b>11/12/2023 Jo Barrett-Lennard</b> Advertising complete</p>	11/12/2023																																					
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That the Council adopts the following schedule for Ordinary Meetings of Council and Committees of Council in the 2024 calendar year:

	Ordinary Meeting of Council	Finance Committee*	Airport Advisory Committee*	Audit and Risk Committee*	Policy & Legislation Committee*	CEO Performance Review Committee*	Meelup Regional Park Committee*
Jan	31-Jan-24				24-Jan-24		
Feb	21-Feb-24	07-Feb-24	07-Feb-24				
Mar	20-Mar-24	06-Mar-24		13-Mar-24	20-Mar-24		06-Mar-24
Apr	17-Apr-24	03-Apr-24	03-Apr-24			10-Apr-24	
May	15-May-24	01-May-24		08-May-24	15-May-24		
June	19-Jun-24	05-Jun-24	05-Jun-24				
July	31-Jul-24	24-Jul-24		24-Jul-24	31-Jul-24	31-Jul-24	
Aug	21-Aug-24	07-Aug-24	07-Aug-24				
Sept	18-Sep-24	04-Sep-24			18-Sep-24		
Oct	16-Oct-24	02-Oct-24	02-Oct-24			09-Oct-24	
Nov	20-Nov-24	06-Nov-24		13-Nov-24	20-Nov-24		06-Nov-24
Dec	11-Dec-24	04-Dec-24	04-Dec-24				

\* The meeting dates represent the minimum number to be held as prescribed in the Terms of Reference for each committee. Clause 3.4 of the City of Busselton *Standing Orders Local Law 2018* allows for additional meetings to be convened throughout the year as required.

**CARRIED 7 / 0**

**FOR: Cr Andrew Macnish, Cr Jarrod Kennedy, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Mayor Phill Cronin and Cr Sue Riccelli**

**AGAINST: Nil**

13/12/2023  
Policy and Legislation Committee - 8 November 2023 - Local Planning Policy Review: Adoption following consultation - LPP 1.5 Coastal Setbacks  
Completed

**COUNCIL DECISION**

**C2312/195** Moved Cr Anne Ryan, seconded Cr Kate Cox

That the Council pursuant to Clause 4, Part 2 of Schedule 2, 4 (3) (iii) – Deemed Provisions for Local Planning Schemes of the Planning and Development (Local Planning Schemes) Regulations 2015 resolve not to proceed with draft LPP 1.5 Coastal Setbacks, as set out at Attachment A and retain existing policy LPP1.5 (Policy number changed from LPP 18 on 11/5/2020) adopted by Council on 10/3/2010 C1003/069.

**CARRIED 6 / 0**

**FOR: Mayor Phill Cronin, Cr Anne Ryan, Cr Jodie Richards, Cr Val Kaigg, Cr Andrew Macnish and Cr Kate Cox**

**AGAINST: Nil**

15/01/2024 Jo Wilson  
Letters have been sent to all submitters that commented on the revised Local Planning Policy and the current Policy remains in effect for assessment of applications.

15/01/2024

13/12/2023	Policy and Legislation Committee - 8 November 2023 - Dogs Local Law	Completed	<p><b>COUNCIL DECISION</b></p> <p><b>C2312/196</b> Moved Cr Anne Ryan, seconded Cr Jodie Richards</p> <p><b>That the Council:</b></p> <ol style="list-style-type: none"> <li>1. Resolves to make the City of Busselton Dogs Local Law 2023 in accordance with section 3.12(4) of the <i>Local Government Act 1995</i>.</li> <li>2. Authorises the Chief Executive Officer to carry out the processes required to make the City of Busselton Dogs Local Law 2023 in accordance with section 3.12(5) and section 3.12(6) of the <i>Local Government Act 1995</i>.</li> </ol> <p style="text-align: right;"><b>CARRIED 6 / 0</b></p> <p style="text-align: center;"><b>FOR: Mayor Phill Cronin, Cr Anne Ryan, Cr Jodie Richards, Cr Val Kaigg, Cr Andrew Macnish and Cr Kate Cox</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p> <p style="text-align: right;">BY ABSOLUTE MAJORITY</p>	<p><b>15/01/2024 Briony McGinty</b></p> <p>Gazetted Local Law. Copy sent to Minister 15/1/24.</p>	15/01/2024
13/12/2023	Policy and Legislation Committee - 8 November 2023 - Council Policy Review - Bush Fire Brigade Management Policies	Completed	<p><b>COUNCIL DECISION</b></p> <p><b>C2312/197</b> Moved Cr Anne Ryan, seconded Cr Kate Cox</p> <p><b>That the Council:</b></p> <ol style="list-style-type: none"> <li>1. Rescind the following Council policies: <ol style="list-style-type: none"> <li>a. Bushfire Brigades – Bushfire Accounting (Attachment 1)</li> <li>b. Bushfire Brigades – Bushfire Brigade Grievance Process Disciplinary Action (Attachment 2)</li> <li>c. Bushfire Brigades – Code of Conduct, Bush Fire Brigade Objectives and Values (Attachment 3)</li> <li>d. Bushfire Brigades – Meetings of Bush Fire Brigades (Attachment 4)</li> <li>e. Bushfire Brigades – Membership of Bush Fire Brigades (Attachment 5)</li> <li>f. Bushfire Brigades – Qualifications of Bush Fire Brigade Officers (Attachment 6)</li> <li>g. Bushfire Brigades – Roles of Bush Fire Brigade Officers (Attachment 7)</li> </ol> </li> <li>2. Adopt the Bush Fire Brigade Management Policy (Attachment 8).</li> </ol> <p style="text-align: right;"><b>CARRIED 6 / 0</b></p> <p style="text-align: center;"><b>FOR: Mayor Phill Cronin, Cr Anne Ryan, Cr Jodie Richards, Cr Val Kaigg, Cr Andrew Macnish and Cr Kate Cox</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p> <p style="text-align: right;">BY ABSOLUTE MAJORITY</p>	<p><b>22/12/2023 Jo Barrett-Lennard</b></p> <p>Policies rescinded, new policy uploaded to website 21 December 2023</p>	22/12/2023
13/12/2023	Policy and Legislation Committee - 8 November 2023 - Council Policy Review - Media and Public Statements	Completed	<p><b>COUNCIL DECISION</b></p> <p><b>C2312/198</b> Moved Cr Anne Ryan, seconded Cr Andrew Macnish</p> <p><b>That the Council adopts the amended Council Policy ‘Media and Public Statements’ (Attachment 1) to replace the current policy, with the policy having been reviewed as part of the City’s ongoing policy review cycle.</b></p> <p style="text-align: right;"><b>CARRIED 6 / 0</b></p>	<p><b>22/12/2023 Jo Barrett-Lennard</b></p> <p>Policy Updated 21 December 2023</p>	22/12/2023

			<p style="text-align: center;"><b>FOR: Mayor Phill Cronin, Cr Anne Ryan, Cr Jodie Richards, Cr Val Kaigg, Cr Andrew Macnish and Cr Kate Cox</b></p> <p style="text-align: center;"><b>AGAINST: Nil</b></p> <p style="text-align: center;">EN BLOC</p>		
13/12/2023	Policy and Legislation Committee - 8 November 2023 - Council Policy Review - Elected Member Training and Professional Development	Completed	<p><b>COUNCIL DECISION</b></p> <p style="text-align: center;"><b>C2312/199</b> Moved Cr Anne Ryan, seconded Cr Jodie Richards</p> <p><b>That the Council adopts the amended Council Policy ‘Elected Member Training and Professional Development’ (Attachment 1) to replace the current policy, with the policy having been reviewed as part of the City’s ongoing policy review cycle.</b></p> <p style="text-align: center;"><b>CARRIED 6 / 0</b></p> <p style="text-align: center;"><b>FOR: Mayor Phill Cronin, Cr Anne Ryan, Cr Jodie Richards, Cr Val Kaigg, Cr Andrew Macnish and Cr Kate Cox</b></p> <p style="text-align: center;"><b>AGAINST: Nil</b></p> <p style="text-align: center;">BY ABSOLUTE MAJORITY</p>	22/12/2023 Jo Barrett-Lennard Policy Updated 21 December 2023	22/12/2023
13/12/2023	Policy and Legislation Committee - 8 November 2023 - Council Policy Review - Waste Management Facility and Plant Reserve	Completed	<p><b>COUNCIL DECISION</b></p> <p style="text-align: center;"><b>C2312/200</b> Moved Cr Anne Ryan, seconded Cr Andrew Macnish</p> <p><b>That the Council adopts the amended Council Policy ‘Waste Management Facility and Plant Reserve’ (Attachment 1) to replace the current policy, with the policy having been reviewed as part of the City’s ongoing policy review cycle.</b></p> <p style="text-align: center;"><b>CARRIED 6 / 0</b></p> <p style="text-align: center;"><b>FOR: Mayor Phill Cronin, Cr Anne Ryan, Cr Jodie Richards, Cr Val Kaigg, Cr Andrew Macnish and Cr Kate Cox</b></p> <p style="text-align: center;"><b>AGAINST: Nil</b></p> <p style="text-align: center;">EN BLOC</p>	22/12/2023 Jo Barrett-Lennard Policy updated 21 December 2023	22/12/2023
13/12/2023	Policy and Legislation Committee - 8 November 2023 - Various Council Policy Reviews - Early Clearance of Subdivisions, Reinstatement Works in Road Reserves, Crossovers, Defects Liability Bonds for	Completed	<p><b>COUNCIL DECISION</b></p> <p style="text-align: center;"><b>C2312/201</b> Moved Cr Anne Ryan, seconded Cr Andrew Macnish</p> <p><b>That the Council adopts the following policies to replace the current council policies, with the policies having been reviewed as part of the City’s ongoing policy review cycle:</b></p> <ol style="list-style-type: none"> <li>1. Early Clearance of Subdivisions (Attachment 1)</li> <li>2. Reinstatement Works in Road Reserves (Attachment 3)</li> <li>3. Crossovers (Attachment 3)</li> </ol>	22/12/2023 Jo Barrett-Lennard All Policies updated 21 December 2023	22/12/2023

	Subdivisions and Management and Removal of Asbestos		<p>4. Defects Liability Bonds for Subdivisions (Attachment 4)</p> <p>5. Management and Removal of Asbestos (Attachment 5)</p> <p style="text-align: right;"><b>CARRIED 6 / 0</b></p> <p style="text-align: center;"><b>FOR: Mayor Phill Cronin, Cr Anne Ryan, Cr Jodie Richards, Cr Val Kaigg, Cr Andrew Macnish and Cr Kate Cox</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p> <p style="text-align: right;">EN BLOC</p>		
13/12/2023	Audit and Risk Committee - 29 November 2023 - 2022/23 Annual Financial Statements, Audit Report and Management Letter	Completed	<p><b>COUNCIL DECISION</b></p> <p><b>C2312/202</b> Moved Cr Andrew Macnish, seconded Cr Kate Cox</p> <p>1 That the Council highlights the difficulty in discerning the actual real, up to date measure of the gap between the current replacement value of the City's total assets and their written down value and the City's reserves to replace them and the ability of the City to annually cashback its depreciation. Accordingly, the ARC requests the Auditor to make a special report of this as soon as possible.</p> <p>2 That the Council acknowledges receipt of the 2022/23 Annual Financial Statements including Auditors Opinion and Audit Management Letter as per the attached documentation, per section 7.12A(3)(aa) of the Local Government Act 1995.</p> <p>3 That the Council notes that it has met with the requirements of section 7.12A(2) of the Local Government Act 1995 with the Audit and Risk Committee, on behalf of the Council, having met with a representative of the Office of the Auditor General on 29 November 2023.</p> <p style="text-align: right;"><b>CARRIED 6 / 0</b></p> <p style="text-align: center;"><b>FOR: Mayor Phill Cronin, Cr Anne Ryan, Cr Jodie Richards, Cr Val Kaigg, Cr Andrew Macnish and Cr Kate Cox</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p> <p style="text-align: right;">BY ABSOLUTE MAJORITY</p>		
13/12/2023	Capital Programs of Works	Completed	<p><b>COUNCIL DECISION</b></p> <p><b>C2312/205</b> Moved Cr Anne Ryan, seconded Cr Andrew Macnish</p> <p>That the Council:</p> <ol style="list-style-type: none"> <li>1. Endorse the programs of works to guide long term financial planning and development of future capital budgets; <b>and</b></li> <li>2. Acknowledge that this item and recommendation 1 meets the requirements of CEO Performance Criterion #6.</li> </ol> <p style="text-align: right;"><b>CARRIED 6 / 0</b></p> <p style="text-align: center;"><b>FOR: Mayor Phill Cronin, Cr Anne Ryan, Cr Jodie Richards, Cr Val Kaigg, Cr Andrew Macnish and Cr Kate Cox</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p> <p style="text-align: right;">EN BLOC</p>		

13/12/2023	Busselton Jetty Inc Short Term Lease of Ballaarat Room as Interim Marine Discovery Centre	In Progress	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2312/206</b> Moved Cr Anne Ryan, seconded Cr Val Kaigg</p> <p><b>That the Council :</b></p> <p><b>1. Resolves to enter into a Lease and Licence with Busselton Jetty Inc (BJI) in respect of a portion of the 'Ballaarat Room' which forms part of 'Railway House' situated within Reserve 38558 on the following terms and conditions:</b></p> <ul style="list-style-type: none"> <li>(a) Term 2 years with an option exercisable by the City of a further 2 years;</li> <li>(b) Rent will be a minimum of \$28,038 or 25% of 15% of the annual gross revenue generated from the use and occupation of the leased premises, whichever is the greater;</li> <li>(c) The permitted use of the premises will be the creation and operation of a Marine Discovery Centre (MDC);</li> <li>(d) Fit out and ongoing maintenance of the MDC will be the responsibility of BJI and a bond will be taken to guarantee the cost of returning the area to its current condition at the end of the term;</li> <li>(e) Maintenance and insurance of the structure of the Ballaarat Room (except if caused by acts or omissions of BJI) will be the responsibility of the City;</li> <li>(f) Cleaning costs of the MDC and internal toilets will be the responsibility of BJI;</li> <li>(g) Such further terms and conditions as outlined in this report and as may be necessary to include in the best interests of the parties or to meet the requirements of the Department of Planning Lands and Heritage.</li> </ul> <p style="text-align: right;"><b>CARRIED 6 / 0</b></p> <p style="text-align: center;"><b>FOR: Mayor Phill Cronin, Cr Anne Ryan, Cr Jodie Richards, Cr Val Kaigg, Cr Andrew Macnish and Cr Kate Cox</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p> </div> <p>Part 2 was moved and carried.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2312/207</b> Moved Cr Jodie Richards, seconded Cr Andrew Macnish</p> <p><b>That the Council :</b></p> <p><b>2. Delegates power and authority to the Chief Executive Officer to negotiate and enter into a variation of the Lease granted to the Margaret River Busselton Tourism Association (MRBTA) to remove the portion of Ballaarat House currently licenced and managed by MRBTA on behalf of the City and allocate responsibility for outgoings between the users.</b></p> <p><b>ABSOLUTE MAJORITY REQUIRED</b></p> <p style="text-align: right;"><b>CARRIED 6 / 0</b></p> <p style="text-align: center;"><b>FOR: Mayor Phill Cronin, Cr Anne Ryan, Cr Jodie Richards, Cr Val Kaigg, Cr Andrew Macnish and Cr Kate Cox</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p> <p style="text-align: right;"><b>BY ABSOLUTE MAJORITY</b></p> </div>	<p><b>11/01/2024 Sharon Woodford-Jones</b></p> <p>working through responsibilities in terms of relocation and removal of items within the Ballaarat Room, signage and practicalities of who does what</p>	
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13/12/2023	Annual Report 2022-2023	In Progress	<p><b>COUNCIL DECISION</b></p> <p><b>C2312/208</b> Moved Cr Anne Ryan, seconded Cr Jodie Richards</p> <p><b>That the Council:</b></p> <ol style="list-style-type: none"> <li><b>1. Accepts the 2022-2023 Annual Report (Attachment 1);</b></li> <li><b>2. Determines that the Annual Electors' meeting is to be held in the 'Undalup Room' at 2 Southern Drive, Busselton on Monday, 29 January 2024 at 5:30 pm.</b></li> </ol> <p style="text-align: right;"><b>CARRIED 6 / 0</b></p> <p style="text-align: center;"><b>FOR: Mayor Phill Cronin, Cr Anne Ryan, Cr Jodie Richards, Cr Val Kaigg, Cr Andrew Macnish and Cr Kate Cox</b></p> <p style="text-align: center;"><b>AGAINST: Nil</b></p> <p style="text-align: right;">BY ABSOLUTE MAJORITY</p>	<p><b>16/01/2024 Tegan Robertson</b></p> <p>Local public notice of the availability of the 2022/23 annual report and notice of the Annual General Meeting of Electors has been provided pursuant to s5.55A and s5.29 of the Local Government Act 1995.</p>	
13/12/2023	CEO Performance Criteria Reporting: Local Procurement	Completed	<p>Please action as per resolution.</p> <p><b>COUNCIL DECISION</b></p> <p><b>C2312/209</b> Moved Cr Anne Ryan, seconded Cr Andrew Macnish</p> <p><b>That the Council:</b></p> <ol style="list-style-type: none"> <li><b>1. Note the current levels of local procurement and the existing and potential measures outlined in this report to assist in increasing the City's level of local procurement;</b></li> <li><b>2. Acknowledge that this item and recommendation 1 meets the requirements of CEO Performance Criterion #3.</b></li> </ol> <p style="text-align: right;"><b>CARRIED 6 / 0</b></p> <p style="text-align: center;"><b>FOR: Mayor Phill Cronin, Cr Anne Ryan, Cr Jodie Richards, Cr Val Kaigg, Cr Andrew Macnish and Cr Kate Cox</b></p> <p style="text-align: center;"><b>AGAINST: Nil</b></p> <p style="text-align: right;">EN BLOC</p>	<p><b>07/01/2024 Ben Whitehill</b></p> <p>Acknowledge report has been noted. Officers will continue to work on measures to improve the levels of local procurement.</p>	07/01/2024
13/12/2023	Audit and Risk Committee - 29 November 2023 - Saltwater Project	Completed	<p>Please action as per resolution.</p> <p><b>COUNCIL DECISION</b></p> <p><b>C2312/214</b> Moved Cr Andrew Macnish, seconded Cr Anne Ryan</p> <p><b>That the Council endorse the alternative recommendation contained in confidential attachment 2.</b></p> <p style="text-align: right;"><b>CARRIED 4 / 2</b></p> <p style="text-align: center;"><b>FOR: Cr Anne Ryan, Cr Jodie Richards, Cr Val Kaigg and Cr Andrew Macnish</b></p> <p style="text-align: center;"><b>AGAINST: Mayor Phill Cronin and Cr Kate Cox</b></p>	<p><b>16/01/2024 Maxine Palmer</b></p> <p>Actioned as per Council resolution</p>	

25/12/2030	Policy and Legislation Committee - 10/5/2023 - CONSOLIDATED PARKING SCHEME AMENDMENT - PROWSE WAY, DUNSBOROUGH	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2306/109</b> Moved Councillor P Cronin, seconded Councillor J Richards</p> <p><b>That the Council:</b></p> <ol style="list-style-type: none"> <li>1. Resolves, pursuant to clause 2.1(1)(a) of the City of Busselton <i>Parking Local Law 2020</i>, that the bend abutting 17 Prowse Way, Dunsborough be determined a no-stopping road or verge area as shown in the attached Line Marking and Sign Installation Plan (Attachment A);</li> <li>2. Notes: <ol style="list-style-type: none"> <li>(a) that the resolution will take affect after the giving of public notice by the CEO; and</li> <li>(b) the Consolidated Parking Scheme will be amended to reflect the resolution.</li> </ol> </li> </ol> <p style="text-align: right;"><b>CARRIED 8/0</b></p> <p style="text-align: center;"><b>EN BLOC</b></p> </div>	<p><b>03/08/2023 Ian McDowell</b></p> <ol style="list-style-type: none"> <li>1. The Consolidated Parking Scheme has been amended to reflect this new restriction.</li> <li>2. Public Notice of the amendment to the scheme was published on the City's website on Monday, 3 July 2023; and in the local newspaper on Friday, 7 July 2023.</li> <li>3. A CRM has been raised and assigned to EWS to carry out the line marking, and install the regulatory signage. CRM WE23/1610 refers.</li> </ol> <p><b>07/08/2023 Ian McDowell</b></p> <p>No Stopping Signs have been installed. Line marking to be carried out in the next 1-2 weeks.</p> <p><b>07/09/2023 Ian McDowell</b></p> <p>Line marking completed on Thursday, 7 September 2023</p>	07/09/2023
25/12/2030	Audit and Risk Committee - 7/6/2023 - CEO REVIEW OF SYSTEMS AND PROCEDURES (REG 17)	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2306/111</b> Moved Councillor P Cronin, seconded Councillor J Richards</p> <p><b>That the Council</b></p> <ol style="list-style-type: none"> <li>1. Accept the CEO's Regulation 17 review of the appropriateness and effectiveness of the City of Busselton systems and procedures in relation to risk management, internal control and legislative compliance; and</li> <li>2. request that the Chief Executive Officer provide quarterly updates to the Audit and Risk Committee on the progress of the recommendations for improvement identified as part of the review.</li> </ol> <p style="text-align: right;"><b>CARRIED 8/0</b></p> <p style="text-align: center;"><b>EN BLOC</b></p> </div>	<p><b>01/09/2023 Tim Allingham</b></p> <p>Confirmed with Governance that this item should be transferred to Director FCS.</p> <p><b>04/10/2023 Sarah Pierson</b></p> <p>Update on recommendations provided to ARC in September 2023. Next update scheduled post the conclusion of the second quarter (early 2024).</p>	01/12/2023
25/12/2030	Policy and Legislation Committee - 10/5/2023 - BUSHFIRES BRIGADES LOCAL LAW	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 5px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2306/121</b> Moved Councillor K Cox, seconded Councillor R Paine</p> <p><b>That the Council:</b></p> <ol style="list-style-type: none"> <li>1. Commences the law-making process for the City of Busselton Bush Fire Brigades Local Law; the purpose and effect of the local law being as follows:</li> </ol> <p><b>Purpose:</b> To make provisions about the organisation, establishment, maintenance and equipment of bush fire brigades.</p> <p><b>Effect:</b> Bush fire brigades are created in accordance with the <i>Bush Fires Act 1954</i>.</p> </div>	<p><b>03/08/2023 Briony McGinty</b></p> <p>Local public notice done. Letters to Minister for LG and ES done. Waiting for close of submission period before referring back to Council.</p>	06/09/2023

			<p>2. Authorises the CEO to carry out the law-making procedure under section 3.12(3) of the <i>Local Government Act 1995</i>, by:</p> <p>(a) Giving local public notice of the Proposed Local Law; and</p> <p>(b) Giving a copy of the Proposed Local Law and public notice to the Minister for Local Government and the Minister for Emergency Services.</p> <p>3. Notes that the CEO, after the close of the public consultation period, will submit a report to the Council on any submissions received on the Proposed Local Law to enable the Council to consider the submissions made and to determine whether to make the local law in accordance with section 3.12(4) of the Act.</p> <p style="text-align: right;"><b>CARRIED 8 / 0</b></p>		
25/12/2030	Policy and Legislation Committee - 10/5/2023 - PROPOSED LOCAL PLANNING POLICY: IDENTIFICATION OF COMPLEX DEVELOPMENT APPLICATIONS - CONSIDERATION OF ADOPTION FOR ADVERTISING	Completed	<p><b>COUNCIL DECISION</b></p> <p><b>C2306/108</b> Moved Councillor P Cronin, seconded Councillor J Richards</p> <p>That the Council, pursuant to clauses 3 and 4 of Schedule 2 of the <i>Planning and Development (Local Planning Schemes) Regulations 2015</i>, prepare and advertise Draft Local Planning Policy: Complex Development Applications (the Draft Policy) (Attachment A) as a draft local planning policy.</p> <p style="text-align: right;"><b>CARRIED 8/0</b></p> <p>Please action as per resolution.</p>	05/09/2023 Paul Needham	05/09/2023
25/12/2030	RFT 06/23 Mechanical Services	Completed	<p>Please action as per resolution.</p> <p><b>COUNCIL DECISION</b></p> <p><b>C2306/116</b> Moved Councillor P Cronin, seconded Councillor J Richards</p> <p>That the Council</p> <p>1. endorses the outcome of the evaluation panel’s assessment of RFT06/23 Mechanical Services and</p> <p>2. accepts the tender from CBRE (GWSLA) Pty Ltd in respect of SP1 Mechanical Services to the City of Busselton Administration Building and SP2 Mechanical Services to all City of Busselton owned facilities and buildings (excluding the Administration building) as the most advantageous tender.</p> <p style="text-align: right;"><b>CARRIED 8/0</b></p>	07/09/2023 Jo Barrett-Lennard	07/09/2023

25/12/2030	EXPRESSION OF INTEREST - RESERVE 46187 LOT 448 MARINE TERRACE; OLD TENNIS CLUB BUILDING	In Progress	<p><b>COUNCIL DECISION</b></p> <p><b>C2112/119 Moved Councillor A Ryan, seconded Councillor P Cronin</b></p> <p><b>That the Council:</b></p> <ol style="list-style-type: none"> <li>1. Acknowledges the receipt of two responses to a publicly advertised expression of interest process seeking a commercial operator to lease Reserve 46187 – Lot 448 Marine Terrace to operate a business venture consistent with the permissible building uses.</li> <li>2. Nominates Geographe Bay Brewing Co. as the ‘preferred proponent’ for a period of 6 months, as potential future lessee of Reserve 46187 – Lot 448 Marine Terrace, Busselton.</li> <li>3. Authorises the CEO to continue negotiations with the Department of Planning, Lands and Heritage to change the purpose of Lot 448 on Reserve 46187 and agree the terms of head lease with the State Government.</li> <li>4. Subject to the outcome of recommendation 3 above, authorises the CEO to enter into negotiations for the terms and conditions of a sub-lease with Geographe Bay Brewing Co, including ground rent, lease term, asset management, potential landscaping proposals and any legal considerations to operate a commercial venture and return to Council with recommendations in relation to the heads of terms.</li> </ol> <p style="text-align: right;"><b>CARRIED 9 / 0</b></p>	<p><b>27/12/2023 Sharon Woodford-Jones</b></p> <p>The proponent recently updated City officers in relation to the likely service requirements to the premises. They are nearly finished with the rebranding exercise which can be presented to Council in the New Year. CEO has confirmed that their preferred proponent status can be extended by letter</p>	
25/12/2030	RESERVE 46187 LOT 448 MARINE TERRACE: OLD TENNIS CLUB BUILDING PREFERRED PROPONENT EXTENSION	In Progress	<p><b>COUNCIL DECISION</b></p> <p><b>C2302/024 Moved Councillor P Carter, seconded Councillor K Cox</b></p> <p><b>That the Council extends the ‘preferred proponent’ status for Geographe Bay Brewing Co. as potential lessee of Reserve 46187 – Lot 448 Marine Terrace, Busselton until 31 December 2023</b></p> <p style="text-align: right;"><b>CARRIED 9 / 0</b></p> <p><b>FOR: Cr Grant Henley, Cr Paul Carter, Cr Anne Ryan, Cr Jodie Richards, Cr Kate Cox, Cr Phill Cronin, Cr Sue Riccelli, Cr Mikayla Love and Cr Ross Paine</b></p> <p style="text-align: right;"><b>AGAINST: Nil</b></p>	<p><b>03/08/2023 Tiffany Sweatman</b></p> <p>Working with DLPH with regards to terms of the headlease and proposed commercial use. Further information has been requested to justify our request for a nominal rent headlease on the basis of placing the income from the sublease into the Jetty Maintenance Reserve. Teams meeting has been sent up, which had to be rescheduled due to absence.</p> <p><b>26/10/2023 Sharon Woodford-Jones</b></p> <p>DPLH have confirmed a nominal rent headlease can be offered. The proponent is due to meet with City officers next week to discuss the terms of the sublease. An up to date valuation will be required to determine rent and meet the statutory advertising requirements. Once the parties have agreed the heads of terms a report will be presented to Council requesting authority be given to the CEO to enter into a development agreement and sublease.</p>	

25/12/2030	Community Assistance Program 2022/2023 Round 4 Outcomes	Completed	<p>Please action as per resolution.</p> <div style="border: 1px solid black; padding: 10px;"> <p><b>COUNCIL DECISION</b></p> <p><b>C2306/118</b> Moved Councillor P Cronin, seconded Councillor J Richards</p> <p>That the Council endorse the allocation of funding for the Community Assistance Program 2022/2023 Round 4 as outlined in the table below:</p> <table border="1" data-bbox="759 491 1745 651"> <thead> <tr> <th>Applicant</th> <th>Requested</th> <th>Recommendation</th> </tr> </thead> <tbody> <tr> <td>Naturaliste University of the Third Age</td> <td>\$697.98</td> <td>\$697.98</td> </tr> <tr> <td>Dunsborough Community Garden</td> <td>\$4,363.64</td> <td>\$4,363.64</td> </tr> <tr> <td>Dunsborough Greenfield Community Group</td> <td>\$5,100.00</td> <td>{resolution}-</td> </tr> <tr> <td><b>TOTAL</b></td> <td><b>\$10,161.62</b></td> <td><b>\$5,061.62</b></td> </tr> </tbody> </table> <p style="text-align: right;"><b>CARRIED 8/0</b></p> <p style="text-align: center;"><b>EN BLOC</b></p> <p style="text-align: right;"><b>CARRIED 8 / 0</b></p> </div>	Applicant	Requested	Recommendation	Naturaliste University of the Third Age	\$697.98	\$697.98	Dunsborough Community Garden	\$4,363.64	\$4,363.64	Dunsborough Greenfield Community Group	\$5,100.00	{resolution}-	<b>TOTAL</b>	<b>\$10,161.62</b>	<b>\$5,061.62</b>		03/08/2023
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**Hon David Templeman MLA  
Minister for Culture and the Arts; Sport and Recreation; International  
Education; Heritage**

Our Ref: 66-24071

Mr Tony Nottle  
Chief Executive Officer  
City of Busselton  
[city@busselton.wa.gov.au](mailto:city@busselton.wa.gov.au)

Dear Mr Nottle

**LOCAL GOVERNMENT HERITAGE CONSULTANCY GRANT**

I would like to congratulate the City of Busselton in securing a grant under the Department of Planning, Lands and Heritage's Local Government Heritage Consultancy Grants Program.

I understand that the grant is \$20,000 and will assist in delivering a review of the City's local heritage survey.

The role of local government in identifying and protecting local heritage is critical to retaining community wellbeing and a sense of place. It is a role that can easily be seen as a lesser priority, and it is encouraging that your Shire is taking steps that will assist in its heritage management processes. I am pleased that the State Government, through the Department of Planning, Lands and Heritage, has been able to provide assistance to you.

I hope that the project progresses well and delivers the intended outcomes for you and your community.

Yours sincerely

A handwritten signature in blue ink, appearing to read 'David Templeman', with a long horizontal flourish extending to the right.

**HON DAVID TEMPLEMAN MLA  
MINISTER FOR HERITAGE**

09 JAN 2024

10th Floor, Dumas House, 2 Havelock Street, West Perth Western Australia 6005  
Telephone: +61 8 6552 5400 Email: [Minister.Templeman@dpc.wa.gov.au](mailto:Minister.Templeman@dpc.wa.gov.au)  
[www.premier.wa.gov.au](http://www.premier.wa.gov.au) ABN: 61 313 082 730

- 1 -



Department of  
**Local Government, Sport  
and Cultural Industries**

Our ref: M23004682  
Enquiries: Mustafa Yildiz  
Phone: (08) 65521788  
Email: [mustafa.yildiz@dlgsc.wa.gov.au](mailto:mustafa.yildiz@dlgsc.wa.gov.au)

Mr Anthony Nottle  
Chief Executive Officer  
City of Busselton

Email: [eacouncil@busselton.wa.gov.au](mailto:eacouncil@busselton.wa.gov.au)

Dear Mr Nottle

**COPIES OF NOTIFICATIONS TO BE PROVIDED TO MINISTER (REG 87E LOCAL GOVERNMENT (ELECTIONS) REGULATIONS 1997): CITY OF BUSSELTON EXTRAORDINARY VACANCY**

Thank you for your correspondence dated 4 December 2023 to the Hon David Michael MLA, former Minister for Local Government, regarding advice on the resignation of Cr Cherise Woodhams and providing copies of notifications, in accordance with regulation 87E of the Local Government (Elections) Regulations 1997, which relate to the City of Busselton filling the vacancy without an extraordinary election in accordance with Schedule 4.1A of the *Local Government Act 1995*.

I am responding to you on behalf of the Hon Hannah Beazley MLA, the new Minister for Local Government.

I note your advice that Cr Valerie Kaigg has filled the extraordinary vacancy for a term ending on 18 October 2025.

Thank you for providing these notifications to the Minister. This information has been recorded by the Department of Local Government, Sport and Cultural Industries for our records.

Yours sincerely

Ethan Redshaw  
**A/EXECUTIVE DIRECTOR LOCAL GOVERNMENT**

15 December 2023



# City of Busselton Donation Impact Report



## Background.

People with cancer often experience serious chronic health conditions in addition to their cancer diagnosis. Fatigue, functional decline, pain, psychological distress, and a higher risk of developing comorbid illnesses such as cardiovascular disease, diabetes, and osteoporosis are all common. Cancer can also have lasting physical and psychological impacts. Significant health concerns resulting from both the cancer and its treatment can include distress, fear of cancer recurrence, fatigue, obesity, and sedentary lifestyles. One way in which Cancer Council WA helps patients to address these issues is through our Life Now Program.

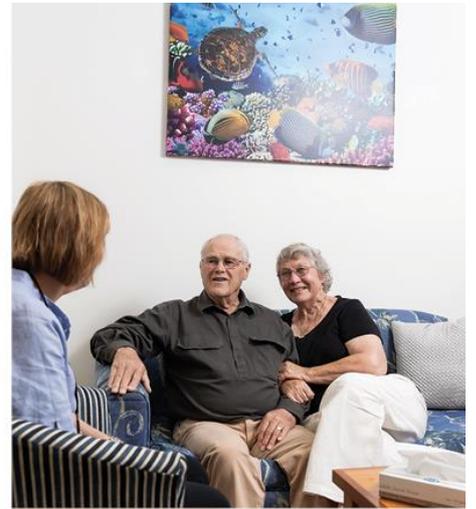
Tai Chi is a popular staple of the Life Now Program, but as an entirely community-funded initiative, we rely on generous donations like yours in order to offer these courses. In particular, we wanted to bring a face-to-face Tai Chi course to Busselton, to offer local cancer patients and carers a convenient and supportive service close to home.

Thanks to support from the City of Busselton, we have been able to offer a free six-week Tai Chi course to people with cancer and their primary carers in late 2023.

## Your generosity at work.

Your donation has meant that we could afford to pay for an experienced Tai Chi facilitator to travel to Busselton and run the six-week Tai Chi course, as well as cover the costs of hiring the Blue Room at the City of Busselton High Street Hall for all six weeks of sessions.





As shown in the above photos taken during one of these sessions, these classes were very well received. Sessions ran from Thursday 26<sup>th</sup> October until Thursday 30 November 2023, from 10.30am to 11.30am.

We had 19 cancer patients and carers registered to attend, with 12 participants ultimately attending all sessions.

Our facilitator also provided each student with a USB of the tai chi exercises so they could continue the practice and follow along at home. He provided the following comment:

*“A number of the group commented that the class required them to focus and concentrate on how they moved and were able to appreciate how slow movements helped them to relax and had a strengthening effect on their body. One participant says she is happy practicing and pleased that she has USB of me so that she can follow”.*

Participants were pleased to have a Tai Chi course available in their locality. We would love to make this a more regular occurrence in the future if funding permits.

We would like to once again warmly thank the City of Busselton for this incredibly generous donation, which has allowed us to provide direct support to Busselton residents.



**Thank you for your  
generous support.**



**Contact us**

**Siobhan Hodge**

Level 1, 420 Bagot Road  
Subiaco WA 6008

**T:** 08 9212 4383

**E:** [Siobhan.Hodge@cancerwa.asn.au](mailto:Siobhan.Hodge@cancerwa.asn.au)

## Terms of Reference

### 1. INTRODUCTION

- 1.1. The Council of the City of Busselton has established the Vasse River and Toby Inlet Committee (the Committee) pursuant to sections 5.8 and 5.9(2)(d) of the *Local Government Act 1995* (the Act)
- 1.2. The Committee will act for and on behalf of Council in accordance with the provisions of the Act, local laws, and policies of the City of Busselton and this document.
- 1.3. The Committee is to consider matters that require a formal decision of the Committee and/or the Council, in order to meet the stated objectives as per Part 2 of this terms of reference.

### 2. OBJECTIVE

Objectives of the Committee are as follows:

- 2.1 Identifying practical and implementable actions to sustainably improve the health and amenity of the Lower Vasse River and Toby Inlet;
- 2.2 Identifying means of funding those actions;
- 2.3 Assisting the City with implementation of the Lower Vasse River Waterway Management Plan, the Toby Inlet Waterway Management Plan and any other action required to assist with 2.1 above;
- 2.4 Facilitating information exchange on waterway management, restoration, and enhancement;
- 2.5 Promoting and encouraging collaboration between stakeholders;
- 2.6 Having input into the approach and priorities for, and reviewing the outcomes of, trials, research, and monitoring;
- 2.7 To foster liaison and cooperation between the community and stakeholders on matters associated with the Vasse River;
- 2.8 To consider the City's position as Interim Asset Manager.

### 3. MEMBERSHIP

Membership of the Committee are as follows:

- 3.1 Up to four (but not less than two) City of Busselton Elected Members and one deputy Elected Member;
- 3.2 Two members of the community will be appointed by the Council as independent external members of the Committee;
- 3.3. Membership of the Committee will, unless determined otherwise, cease on the day of the next ordinary Council election.
- 3.4. Council will appoint new members to the Committee, in accordance with paragraphs 3.1 and (if required) 3.2, at the next meeting of Council following the Council election.

- 3.5 Independent external members of the Committee may nominate to be reappointed to this role following an ordinary Council election, with appointments and reappointments to the Committee to be made by the Council following each ordinary Council election.
- 3.6 Nothing in these Terms excludes the Community Members from continuing in their appointed role following an ordinary Council election.
- 3.7. Members of the community who have been appointed as independent members of the Committee will be required to agree to adhere to the City of Busselton Code of Conduct for Council Members, Committee Members and Candidates.
- 3.8 The Presiding Member and Deputy Presiding Member shall be an elected member of the City of Busselton

#### **4. SELECTION OF COMMUNITY MEMBERS**

- 4.1 Selection of Community Members' will be appointed to the Committee via an Expression of Interest process. All interested community members will be invited to provide a short summary of why they are interested in joining the Committee and what they could contribute.

The following criteria will be used to assess applications:

- a. Qualifications;
- b. Level of engagement in local water quality issues, especially through the development of the Lower Vasse River Waterway Management Plan;
- c. Connection to others in the community interested in local water quality issues, and ability to support two-way communication with others in the community; and
- d. Ability to think openly and creatively about local water quality issues and solutions.
- e. The final decision on memberships will be made by the Council.

#### **5. STAFF AND OUTSIDE REPRESENTATIVES ADVICE**

- 5.1 City of Busselton staff from relevant departments will advise the Committee.
- 5.2 Representatives of partner agencies and other stakeholders may be invited to attend Committee meetings as required. This may include:
  - a. Department of Water and Environmental Regulation;
  - b. GeoCatch;
  - c. Department of Biodiversity, Conservation and Attractions; and
  - d Water Corporation.

#### **6. CHAIR**

- 6.1. Committee will appoint an elected member representative to the role of Chair. The role of the Chair is to preside over meetings and manage and facilitate participation and discussion.

#### **7. MEETINGS**

- 7.1 The Committee will meet at a minimum two times per year.
- 7.2 Notice of meetings will be given to members at least seven days prior to each meeting.

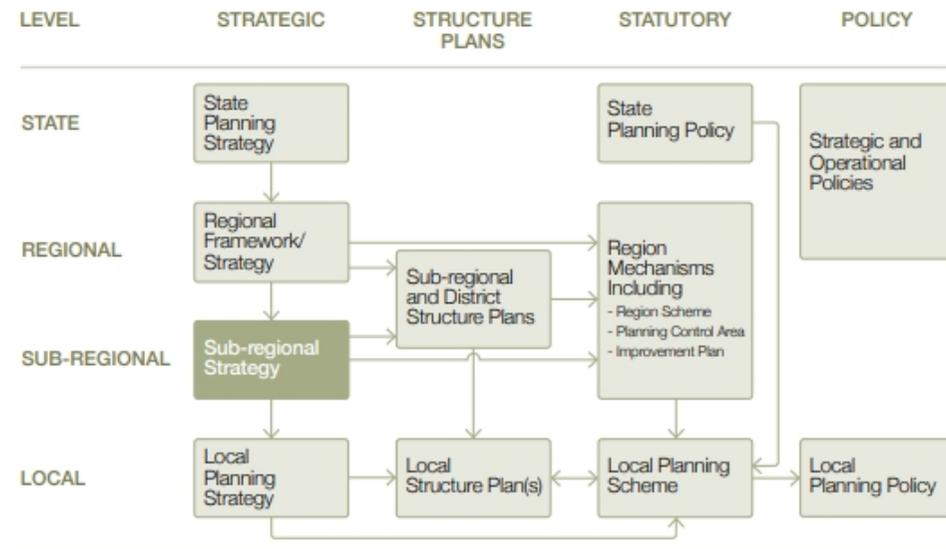
- 7.3 The Presiding Member will ensure that minutes of all meetings are kept in accordance with the Act and the City of Busselton Standing Orders Local Law.
- 7.4 All members of the Committee will have one vote. If the vote of the members present is equally divided, the Presiding Member will cast a second vote.
- 7.5 Where a Committee member wishes the Committee to consider an alternative to the officer recommendation in relation to any particular item, they should, as far as practicable, indicate in writing (preferably email) to the Chief Executive Officer or Chief Executive Officer's nominee their intention to do so, setting out their proposed alternative and the reasons for that alternative, by 3pm on the day before the scheduled commencement of the meeting. The Chief Executive Officer or Chief Executive Officer's nominee will then provide officer comment on the proposed alternative in writing to Committee members prior to the scheduled commencement of the meeting.
- 7.6 Where the Committee makes a decision that involves the making of a recommendation for consideration by the Council, the Chief Executive officer will ensure that the recommendation is presented to the Council as part of the first practicable ordinary Council meeting agenda following the Committee meeting.

#### **8. QUORUM**

- 8.1. Quorum for a meeting will be at least 50% of the number of offices of membership, whether vacant or not.
- 8.2. A decision of the Committee does not have effect unless it has been made by a simple majority.

#### **9. DELEGATED POWERS**

- 9.1. Pursuant to section 5.17 of the Act, the Committee is delegated the powers to note an item that does not require a recommendation to Council or further action of Council.
- 9.2. Committee recommendations, other than those made in accordance with the delegated powers in paragraph 7.1, are not binding on Council and must be endorsed by Council to take effect.



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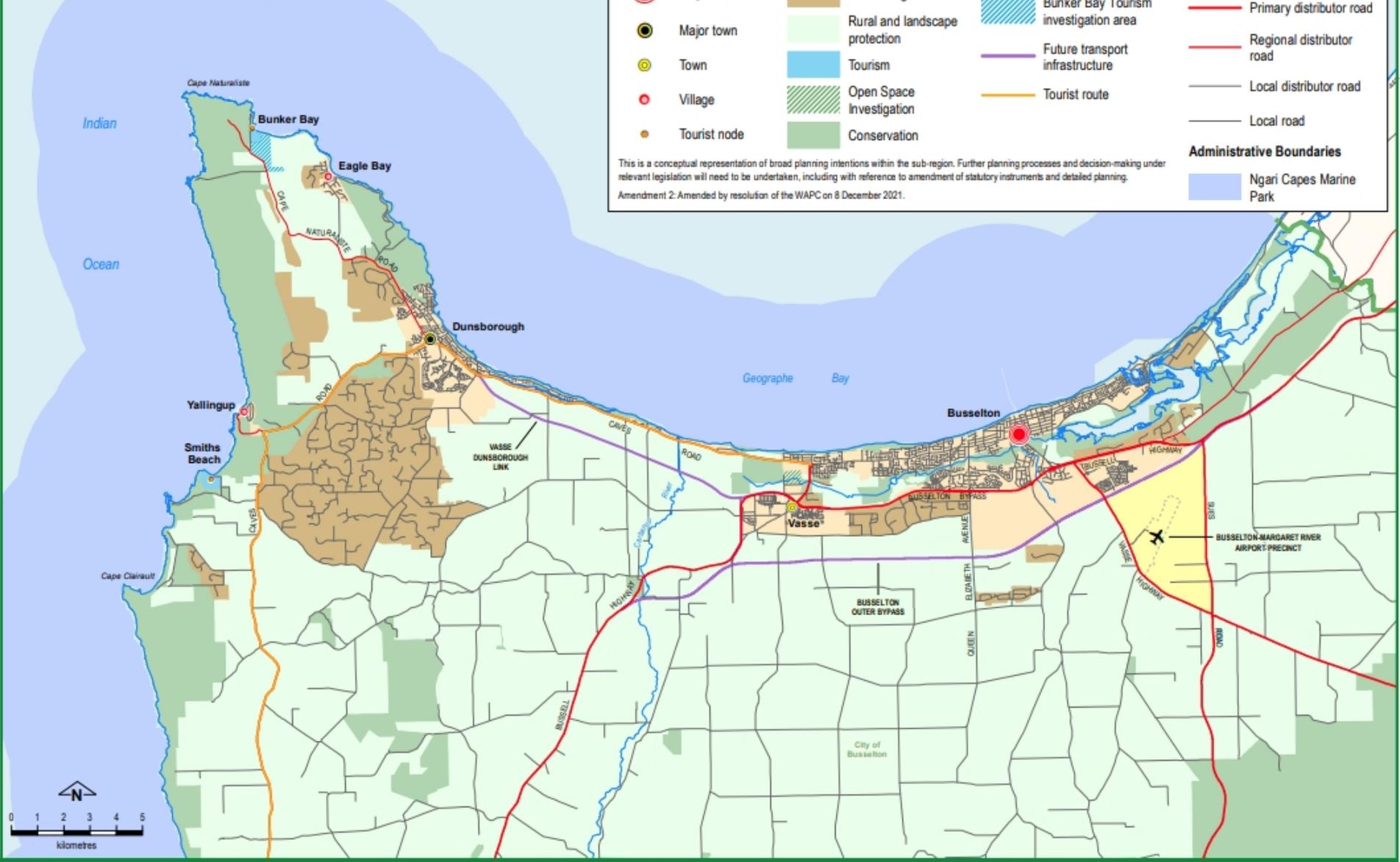
Base information supplied by  
 Western Australian Land Information Authority  
 SLIP 1180-2020-1

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Marine park supplied by Department of  
 Biodiversity, Conservation and Attractions

Legend	Land Use	Strategic Directions	Transport
<b>Settlement Hierarchy</b>	Urban	Busselton-Margaret River Airport Precinct	Transport
Regional centre	Rural living	Bunker Bay Tourism investigation area	Primary distributor road
Major town	Rural and landscape protection	Future transport infrastructure	Regional distributor road
Town	Tourism	Tourist route	Local distributor road
Village	Open Space Investigation		Local road
Tourist node	Conservation		<b>Administrative Boundaries</b>
			Ngari Capes Marine Park

This is a conceptual representation of broad planning intentions within the sub-region. Further planning processes and decision-making under relevant legislation will need to be undertaken, including with references to amendment of statutory instruments and detailed planning.  
 Amendment 2: Amended by resolution of the WAPC on 8 December 2021.



Strategy Plan - Geographe Bay (detail)