

# Local Planning Policy No. 2.2

## INDUSTRIAL DEVELOPMENT

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### 1. HEAD OF POWER

This Policy has been adopted pursuant to *Planning and Development (Local Planning Schemes) Regulations 2015*, Schedule 2 (Deemed Provisions), Clause 4 and applies to development across the whole of the City.

### 2. PURPOSE

#### 2.1 Introduction

The purpose of this Policy is to provide developers and the general public with a guide to the City's objectives for the development of industrial areas and buildings in a simple, easy to read document. This Policy applies to all industrial development within Busselton City including Prescribed Premises.

This Policy also provides guidelines and standards in respect to designated showroom industrial precincts within Industrial Areas, and identified Special Industrial Precincts.

This Policy is a guide only and should be read in conjunction with the City District Zoning Scheme which sets out statutory requirements. Any person considering an industrial development is also advised to discuss the proposal with City officers prior to the commencement of detailed site planning.

The dimensions detailed in this Code are generally minimum requirements. Individual circumstances may require the use of dimensions different from those specified in order to achieve a satisfactory development and adequate on-site traffic circulation areas.

The objectives of the Code are to establish guidelines which will:

- a. Result in the construction of industrial development which is attractive in appearance and provides a safe, pleasant and efficient working environment.
- b. Conserve and enhance the amenity of the neighbourhood and adjoining developments.
- c. Provide opportunities in designated areas for showroom based industrial activities, and ensure an adequate standard of development.

#### 2.2 Application

Application for Planning Consent requires the submission of a completed Application Form for Development Approval, payment of fees and is to be accompanied by four (4) sets of site, elevation and floor plans of the proposed development.

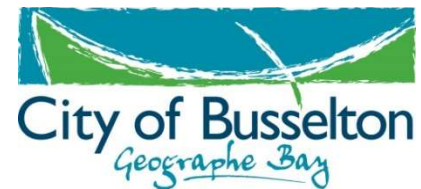
Detailed landscape plans should also be submitted once the site plan is confirmed as satisfactory to the City. Section 4.8 specifies the requirements in preparing detailed landscape plans.

The following should be submitted with all applications.

- (a) A site plan or plans at a suitable scale (1:100 or 1:200 preferred) showing:
  - i. The boundaries of the site, with dimensions and direction of grid north indicated accurately.
  - ii. The outline of proposed buildings on the site and distances from the boundaries. The purposes for which areas are proposed to be used shall be nominated. The site plan shall also show the location

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- of buildings on adjacent sites and crossovers adjoining or adjacent to the site.
- iii. Where a building will form part of a future complex of buildings, a staging plan to indicate the location, orientation and proposed use of future buildings and their relationships one to another.
  - iv. Refuse and waste disposal areas and the treatment thereof.
  - v. The location of individual car parking spaces and access driveways including manoeuvring areas with all dimensions.
  - vi. The location and dimensions of all loading and unloading areas, the means of access, and the screening and landscaping thereof.
  - vii. The location of any adjoining kerb and gutter, concrete footpath, electricity supply, Telecom conduit, etc. and street trees.
  - viii. Details of existing ground levels and proposed finished levels of the site including accurate exiting and proposed spot levels at all corners of proposed buildings and at the corners of property boundaries. The finished floor levels of all buildings shall also be shown. All levels shall be related to a nearby permanent benchmark.
  - ix. The location of all proposed paving of hard standing areas and driveways, specifying the materials to be used for the surface finishes.
  - x. Proposed methods of stormwater disposal, including all proposed drainage lines and existing drainage.
  - xi. The location and type of signage.
- (b) Floor plans and elevations of proposed buildings at a scale of 1:100 or other acceptable scale. The floor plan is to contain a concise description of the uses to which each section of each building is to be put.
  - (c) Full details of the construction material and colour of external walls and roof materials. Submission of samples of materials may also be required.

An application for a building licence will not be processed until the development approval of the City has been granted.

### 3. INTERPRETATION

The following terms are used in the policy and will need to be understood in making application for Planning Consent.

**Industry** shall have the same meaning as in the Scheme.

**"Net Lettable Area" (NLA)** has the same meaning as in the Model Scheme Text.

**Prescribed Premises** shall have the same meaning as in the Environmental Protection Act 1986.

**Bulky Goods** shall have the same meaning as in the Scheme.

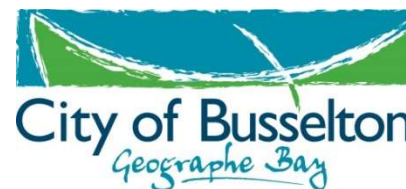
### 4. POLICY STATEMENT

#### 4.1 Land Use

To identify those uses which are 'permitted' or can be approved by the City on land zoned for 'Industrial' use, applicants should, in the first instance, refer to the City District Zoning Scheme.

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### 4.2 Development

The following development standards are applicable to industrial development in the City:

	Industry
Minimum Lot Size	1000m <sup>2</sup>
Average Lot Size	2000m <sup>2</sup>
Minimum Effective Frontage	25m
Maximum Plot Ratio	0.5:1
Minimum Boundary Setbacks	
* Front (1)	15m
* Minor St (Cnr Lots ) (2)	9m
* Rear (nil setback possible with the City approval)	10m
* Sides (nil Setback on one side)	5m
* Sides (both) - minimum	N/A

- (1) A minimum setback of 22m applies to Showroom Industrial lots fronting Cook St, Busselton.
- (2) The Minor St setback may be reduced to 6m on lots less than 2000m<sup>2</sup> subject to adequate visual truncation.

At the discretion of the City and subject to the objectives of the code being met, buildings may be constructed up to the side and rear boundaries and abutting laneways of any lot.

Additional development standards specific to Trade Displays and Motor Vehicle Wreckers are set out under “Other Special Requirements”.

### 4.3 Development standards & guidelines for factoryette or grouped industrial unit development

A “factoryette” is a unit of a factory tenement building that is the subject of a separate occupancy. The following development standards apply to factory tenement buildings and factoryettes:

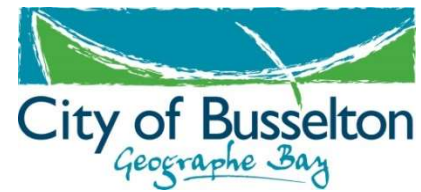
Minimum NLA	90m <sup>2</sup>
Minimum width and length	6m
Minimum area for garbage storage (in open yard)	9m <sup>2</sup>
Maximum height of enclosing fence (for open yard)	2.4m
Maximum area of signboard for factoryettes	2m <sup>2</sup>

**OPEN YARDS:** Certain factory tenement buildings may require an open service yard to be provided adjacent to each factoryette unit. As a guide, the area of the open yard should be equal to *at least* 30% of the floor area of the factoryette and have a minimum dimension of 5m.

The City will consider relaxing this requirement based on merit assessment of the space requirements for a specific proposed business. Roofed areas will be considered for storage purposes in complying with the open yard requirement.

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**BIN STORAGE:** Each open yard shall contain a garbage facility which has vehicular access from a paved internal service road.

**SERVICING:** Each factoryette will require servicing by a front, side or rear accessway from street or paved service road, which should have a minimum width of 5m for two way vehicle movement on straight road sections. For curved road sections, reference shall be made to design specifications set out in the City's Carparking Policy. For areas other than customer parking areas, the minimum design requirements of a small rigid truck should be used as a guide to accessway widths.

It may be necessary for service road widths to be widened in special circumstances to accommodate specific uses.

**SCREENING:** Each open yard will require visual screening from public roads and adjoining factoryettes by a close fence or wall. (The City may relax this requirement in case where it considers the requirement unnecessary, or require a landscape buffer as an alternative).

**CONSTRUCTION:** Factoryettes shall be separated from each other by an internal wall or walls constructed in brick, stone or concrete.

**ADVERTISING:** A single signboard, on which is marked the names and occupations of the occupiers of the factoryette, may be displayed on the parcel of land on which a factory tenement building is situated. The signboard must be erected either within the landscaped area or behind the building setback and is subject to a separate approval. For sites containing two or more factory tenements, one pylon sign only displaying all tenements (apart from building signs in accordance with Local Laws) may be displayed. The use of multiple "A Frame" signs and similar types of temporary signage is expressly prohibited.

#### 4.4 Parking & access requirements

4.4.1 Car parking shall be provided in accordance with the City's Carparking Policy as follows. However, a minimum of 5 car spaces per development is required in all situations.

- Factory, warehouse, showroom and prescribed premises: 1 bay per 50m<sup>2</sup> of NLA.
- Office: 1 bay per 30m<sup>2</sup> NLA (only applied if office space is greater than 25% of NLA, otherwise 1 space/50m<sup>2</sup> NLA applies).
- Plus: 1 loading bay per Factory Unit as appropriate.

Design details and standards for the provision of the required parking bays and internal roadways are contained in the City's Carparking Policy. All car parking and access areas are to be sealed, paved, marked, drained and constructed to the satisfaction of the City.

The minimum size of loading bays shall be 3m x 7m. This may need to be increased to reflect the location of the bays and the intended use of the building. The minimum manoeuvring area design requirements are to be for small rigid truck vehicles in accordance with the City's Carparking Policy.

### 4.5 Location

To minimise the physical and visual aspects of vehicles on the site, it is preferable that the bulk of parking be located so that it is screened from public view. It is preferable for only visitor parking to be situated forward of the building line and be easily accessible.

Delivery, loading and storage areas must be located behind the setback line and screened from public view. Staff and service vehicle parking is preferred to be located within the side and rear setback areas.

### 4.6 Use of Parking Areas

Parking areas and vehicle accessways are to be adequately paved and drained, in particular within the front setback area. These areas are not to be used for the storage of goods, rubbish, equipment or for industrial or automotive purposes such as wrecking and repair.

### 4.7 Crossovers

Crossover construction to the City specification is required. Construction requirements include sealing or paving to design levels, including provision for swale drainage. Details are to be shown on development plans. The City requires a bond to be paid to ensure construction to specifications.

### 4.8 LANDSCAPING

#### 4.8.1 Objectives & Application Requirements

Landscaped areas are to be provided to enhance the appearance of the development from adjoining lands and public areas within the site. A landscape plan or working drawing will be required to be submitted with the Development Application and should show the following detail:

- general site layout;
- species or plant type (e.g. native shrub to 1-3m);
- earthworks and contours;
- paved and grassed areas;
- non-plant landscape elements; and
- areas to be landscaped, including verges.

The landscape plan will be assessed as an element of the Development Application. It is important that landscaping provided is low maintenance in nature so that maximum long-term amenity benefits can be obtained at minimal ongoing cost. High maintenance areas of landscape, such as lawns, should be kept to a minimum.

The landscape plan is to outline the ongoing maintenance commitment.

#### 4.8.2 Landscape Provision

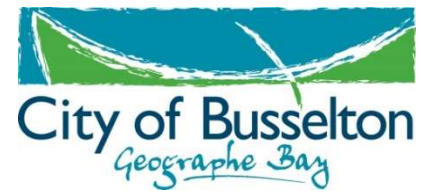
The requirements for each development site are as follows:

- A minimum 3m landscape strip adjacent to street boundaries is to be established (only 2m is required for the Strelly Street Light Industrial Precinct and Aviation Precinct of the Busselton Airport Industrial Airpark).

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- Where practicable, existing trees are to be retained.
- A minimum 1m landscape strip adjacent to the side boundary forward of the building line is to be established, excepting those areas used for service access and vehicle parking.
- Landscaping shall be established in all portions of the front setback not otherwise used for access or parking and may include a grassed trade display area.
- Landscaping is to be used to screen service and storage areas where it is not practicable for these to be screened in the building design.
- Selection of landscaping plants should acknowledge the need to maintain a visual connection between the street and building frontages and utilise the height difference between low shrubs and the understorey of trees to either screen views or create vistas for passing motorists.
- Landscaping is to be provided within parking areas to provide shade and soften the impact of extensive paved areas.
- Landowners will be requested to co-operate in landscaping and maintaining adjoining road verges.
- Landscaping should be used to soften the impact of the bulk of the building and any other structure on site.

To enhance the overall amenity benefit achieved from landscaping consistency between developments is encouraged in terms of plant species utilised. Recommended preferred species are native on the basis of their suitability to the land types in the 'Industrial' areas. A Species List is available from the City.

Exotic species will be considered for approval subject to the developer demonstrating how the species will harmonise with the existing landscape character of the streetscape; complement the building character; not create maintenance problems, including leaf litter and service line damage.

A Certificate of Classification will not be issued for the building and bonds will not be returned until landscaping is completed in accordance with the approved plan, or an alternative arrangement satisfactory to the City has been made. In any event, all landscape treatment shall be completed within 3 months of practical completion of the development.

Landscaping plans shall take account of, and be integrated with, requirements for servicing of the development.

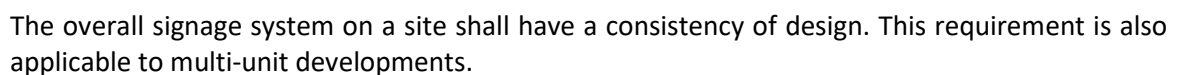
The intent of good landscaping is illustrated on the attached diagram.



City of Busselton  
Geographe Bay

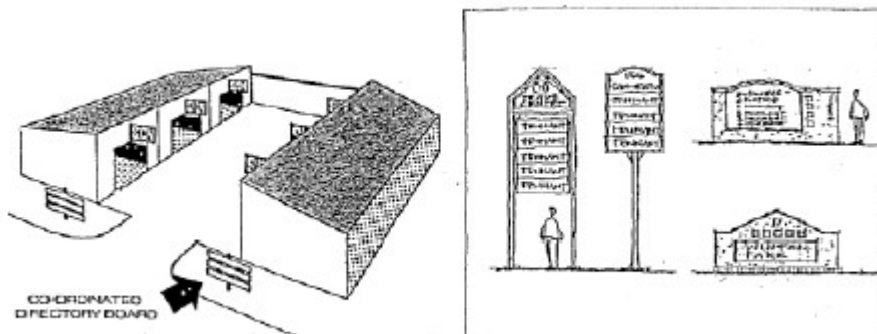


The objective of signage is to permit the display of the name of the occupier and the activity conducted in the building. Signs should generally be for information purposes, not product display.



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### Directionary Signs

In addition to building signs, one only free-standing identification sign, i.e a pylon sign, may be provided, and be located at the front entry to the site. This may be replaced with a co-ordinated directory board for multi-unit developments.

### Building Signs

Signs on buildings, including flush wall signs, fascia sign and awning signs are generally acceptable. Additional pylon signs and the scale of all signs proposed will be subject to merit assessment.

Advertising devices, including coloured bunting, flashing/revolving or other moving signs hoardings, roof signs, sandwich boards and billposting are not generally considered desirable.

## OTHER SPECIAL REQUIREMENTS

### 4.10 Building Appearance

In keeping with the City's aim of achieving an attractive and quality environment for industry every encouragement will be given to applicants who produce innovative and attractive building designs that are sensitive to the existing and intended surroundings and adjoining development.

The facades of buildings visible from public roads are to maintain a high standard of visual amenity and should be constructed of brick, stone, painted or rendered concrete. Other materials may be considered by the City where it can be demonstrated that required visual standards can be achieved.

The City also has discretionary powers to ensure consistency in building form and character with respect to the following:

- (a) To ensure heights of buildings are limited to maintain the streetscape amenity.
- (b) To ensure that colour and finish selection complements the building materials selected and is consistent with other colours and finishes selected for the Industrial Area.
- (c) To ensure that building materials, building colours and window glazing is low reflective and that the glare does not adversely affect adjacent properties and public areas, including public roads and rights-of-way.
- (d) Walls abutting or adjacent to laneways are to be of a finish and colour which is to the City's satisfaction.

### **4.11 Minimum Fencing Requirement**

A requirement for fencing in front of the building line should generally be avoided. Where a lot has two street frontages, fencing will be permitted within the setback from the secondary street.

The minimum fencing requirement shall be 1.8m high black PVC coated galvanised linkmesh and associated with provision of landscaping.

### **4.12 External Storage**

Buildings should be designed to ensure that no refuse, raw materials, machinery or plant equipment, etc. is stored within the street frontage setback (with some latitude for lots with two street frontages as for 'Fencing' above).

Storage areas shall be screened so as not to be visible from the street or adjacent development and screening walls shall be constructed to be consistent with the buildings on the lot.

All bin and garbage storage areas shall be enclosed by a screen wall 1.8m high.

### **4.13 Loading & Service Areas**

Each development shall provide sufficient on-site loading facilities to accommodate site activities. All loading movements, including turnaround, shall be made off the public roads.

Where practical, loading docks shall be located to the side or rear of the lot, effectively screening them from the street and public areas.

### **4.14 Exterior Lighting**

Lighting of the front elevation of the building, service and parking areas to security levels is encouraged.

### **4.15 Road Improvement**

Where development is associated with sub-standard roads, the City will require road frontage upgrading and/or contribution to road upgrading. Upgrading works could include widening, kerbing and drainage.

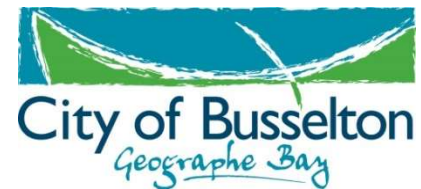
### **4.16 Motor Vehicle Wrecking and/or Storage**

The use of land in any 'Industry' Zone for the purpose of wrecking and/or storage of motor vehicles will be required to be carried out in accordance with the following standards:

- (a) The area of land actually used for the purpose of wrecking and/or storage of motor vehicles shall not exceed one (1) hectare;
- (b) Where motor vehicles have been reduced in size to a volume of two (2) cubic metres or less, no more than 1500 such vehicles per hectare shall be stored on the land;
- (c) Where motor vehicles have not been so reduced in size, no more than 200 vehicles per hectare shall be stored on the land;

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- (d) Motor vehicles, whether reduced in size or not, shall be stored in neat rows on the land;
  - (e) Motor vehicles shall not be stored on the land in any manner greater than three (3) metre in height;
  - (f) The boundaries of any wrecking/storage site will be required to be subject of comprehensive screening treatment by use of appropriate landscaping and fencing.
- Overall, the City will not consent to the use and development of land in an 'Industrial' Zone for the purposes of the wrecking and/or storage of motor vehicles unless it is first satisfied that the proposed use and development will not adversely affect the amenity of the locality.

### 4.17 Subdivision Requirements

Many new subdivisions for industrial development incorporate shared access of driveways and access ways to parking on individual lots for the convenience and safety of customers and general road users. This design element is also encouraged by this Policy. In order to secure the early completion of these facilities within new developments the City will require the securing of the continued use of joint access ways by a condition of subdivision that the paved access ways be constructed by the subdivider prior to clearance of lots and carry an easement in gross for use in perpetuity.

Joint access ways between the parking areas on individual lots are to be shared to enable continuous linking of on-site parking areas. In the event that an integrated access and parking area is not constructed at the time of the initial subdivision, each landowner may be required to construct practical access/egress to the parking area from the street as an interim arrangement until such time as integrated access and parking can be completed. This will result in the interim crossover being removed, at the expense of the developer(s), once a shared joint access way is provided.

## 5. SPECIAL INDUSTRIAL PRECINCTS

Special Industrial Precincts are designated areas where the City has formulated special land use, development and building controls to achieve a particular industrial function and character. The precincts and associated controls have been determined on the basis of one or more of the following criteria:

- a. The precinct is located in a high amenity area.
- b. The precinct's locational attributes consist of exposure to traffic on major access roads.
- c. To cater for specific business and industry needs and requirements.
- d. To encompass general community attitudes and needs.

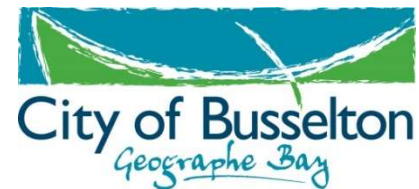
### 5.1 SPECIAL INDUSTRIAL PRECINCT NO. 1

#### Busselton Showroom Industrial Precinct

The establishment of this precinct is based on the close proximity of industrial lots to the future Busselton Bypass Road. The lots are seen as suitably located for showroom facilities and are considered likely to be subject to significant pressure for Showroom type use from the private sector due to its exposure to passing traffic. The precinct controls acknowledge this and seek to promote development of a suitable character and style of building form.

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The subject lots are shown on the attached Plan (Appendix 3). Guide Development Plans for the Precinct are attached (Appendix 3A, B, C, D).

The intent of the controls and guidelines for this precinct are described hereunder:

- a. Provide for the establishment of "bulky goods" operations predominantly based on the sale and/or display of items of such a size, shape or height as to require a large area for handling storage or display, or direct vehicular access to the site for collection of purchased goods, and
- b. Ensure suitable characteristics exist for the successful operation of the precinct in terms of access, visibility, passing trade and general amenity.

These guidelines seek to ensure that the amenity of these precincts will be maintained or improved to a level commensurate with the high visibility and prominence of the sites, and the commercial nature of the operations.

### 5.1.1 Application Procedures

The application procedures are in accordance with general industrial development requirements.

### 5.1.2 Permitted Usage

The nature of commercial activities that can be established in the precinct are set out in the Scheme.

Within the Busselton Showroom Industrial Precinct, the G.F.A of any portion of the building used for manufacturing, processing or warehousing should not generally exceed 30% of the total G.F.A unless the activity is integral to the marketing of the product that is being made on-site. No manufacturing, processing or warehousing shall be undertaken external to a building on any site in the Showroom Industrial Precinct.

### 5.1.3 Additional Design Standards & Guidelines

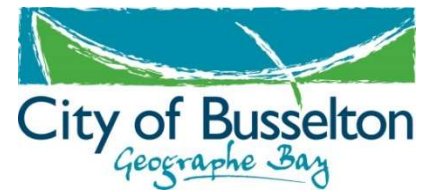
The following sub-clauses contain guidelines and established standards additional to the requirement for general industrial development and should be read in conjunction with those requirements:

#### SETBACKS, BUILDING ORIENTATION & DESIGN

- A minimum setback of 6m is required from the common boundary of a site with an area of Public Reserve land, but not including roads.
- A 22m front setback is required on those lots fronting Cook St to achieve the required separation of buildings from the Busselton Bypass Road i.e. Lot 166-117 inclusive.
- Buildings should be sited and orientated to present a high amenity facade to areas of high traffic intensity such as the Busselton Bypass, in addition to the street frontage.
- The front elevation should be designed to address the street and to provide a corporate image and an inviting and legible entrance. To achieve this, the facade could include significant areas of glass, with overhangs or sheltering devices. Entrance points to buildings are to be designed as focus points and should provide protection for pedestrians.
- The use of verandahs, canopies or colonnades on the front elevation of buildings is encouraged.

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- Where more than one building is proposed for a site, the design and form should present an integrated approach.
- Outbuildings and sundry structures, such as bin enclosures, electrical substation/letterboxes and the like shall be designed in a character and finished to complement the major site building.
- All exposed mechanical plant, ductwork and conduiting and the like shall be positioned and coloured to minimise visual impact.
- In the development of Showroom Industrial Precincts, the provision of rear access to lots for servicing and goods pick-up is desirable and will be encouraged.
- Lot design should provide for the development of buildings of a minimum 400m<sup>2</sup> G.F.A per tenancy.

### 5.1.4 Parking & Access

- (a) All car parking is to be located and the number of car bays provided in accordance with the Guide Development Plan and to be of a bitumen sealed, drained and marked out standard. The parking areas are to link with adjoining lots and therefore construction levels between lots must be the same.
- (b) Dimensions of car parking spaces to be not less than 2.5m by 5.5m with a 7.0 turning area.
- (c) Access to lots will be restricted to those points denoted on the Guide Development Plan. All lot owners will be required to use a joint accessway provided with the adjoining owner and continuous linking of on-site parking areas. Sizes of access points, parking and vehicle manoeuvring areas shall be in accordance with those examples denoted on the Guide Development Plan and to the satisfaction of the City's Technical Services Department.
- (d) Designated parking areas are to be freely available to the public at all times.
- (e) Loading bay dimensions and manoeuvring area requirements shall be in accordance with general industrial development requirements.
- (f) Use of parking areas are to be in accordance with general industrial development requirements.
- (g) Crossovers shall be in accordance with general industrial development requirements.

### 5.1.5 Landscaping

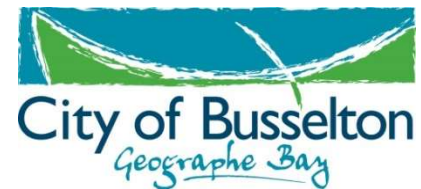
- All areas of a site that are not built upon or paved and are visible from areas having unfettered public access shall be landscaped.
- Lot owners are encouraged to take responsibility for the landscaping and maintenance of the street verge fronting their property on an integrated basis with adjoining properties.
- Where significant areas of customer parking are provided forward of the building setback line, these shall be integrated within landscaping to reduce the visual impact and provide a physical separation from the building.
- The identification of a building's function and main entry point should be enhanced through judicious species selection and not obscured by excessively dense foliage.

### 5.1.6 Signage

- The objective of signage in the Showroom Industrial Zone is to permit adequate identification and business advertising and ensure that all businesses have the opportunity of reasonable exposure.
- Signs attached to buildings shall be designed as an integral part of the building, i.e

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recessed into the facade, contained within a frame, or hanging from an awning, verandah or colonnade.

- Sign writing directly onto the building, other than glazing on the shopfront window panel, is considered desirable.
- Details on the graphics of all signage are to be supplied when application is made for a sign licence and will be considered on its merits.
- Trade displays should not dominate the public appearance of the site or compromise the landscaping and will be considered on an individual basis in the context of each application. In any event, trade displays should be set back at least 3m from any site boundary.

### 5.2 SPECIAL INDUSTRIAL PRECINCT NO. 2

#### Dunsborough Service & Trades Industrial Park

This precinct was formed on the basis that there is a need for special co-ordination and control on the industrial development of the subdivision due to the location of the site within the sensitive urban/rural fringe area of Dunsborough/Quindalup.

A Policy Plan is to be attached (Appendix 4).

#### 5.2.1 Intent of Precinct

The intent of the controls and guidelines for this precinct are described and outlined below:

- (a) To establish an innovative Industrial Park which caters for a wide range of service and industrial activities and other urban-based industrial activities which are required to cater for the existing and future industrial and service needs of the township of Dunsborough and immediate surrounding areas.
- (b) To support only "dry" industrial activities and other activities which will collectively not impinge on available water resources for residential use nor lead to pollution of any water resource.
- (c) To provide guidance to the subdivider and developer to ensure that sensitive environmental design principles are adopted and environmental safeguards and measures are implemented, which includes the establishment of adequate buffers, to ensure protection of the following:
  - (i) The scenic/tourist value of the general area and Commonage Road in particular
  - (ii) The residential/rural amenity values of the abutting and adjacent properties and their use of Genoli Road; and
  - (iii) The environmental/conservation values of the traversing watercourses, groundwater sources and of the remnant vegetation.

#### 5.2.2 Application Procedures

The application procedures are in accordance with requirements specified for general industrial development. The only variations are in respect to permissible and desirable land uses.

#### 5.2.3 Permissible & desirable land uses/activities

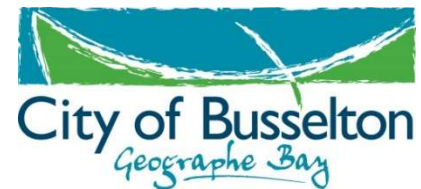
Those uses that are not permitted in the Dunsborough Industrial Park are specified in the Local Planning Scheme.

The following activities are considered appropriate within the Dunsborough Industrial Park:

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- Auctioneer's premises
- Bakery
- Boat maintenance and sales
- Building construction industries - building material supply - services and sales
- Courier services
- Earthmoving vehicles depot
- Firewood storage
- Funeral parlour
- Gymnasium
- Housing appliance and furniture service and sales
- Light engineering
- Lunch bar
- Market and market garden sales
- Motor vehicle maintenance and sales
- Plant nursery - retail and wholesale
- Recreation Facility
- Research Establishment
- Rural supplies
- Showrooms
- Storage units
- Timber./hardware wholesale
- Transport depot
- Veterinary clinic and hospital
- Warehouse

The following Prescribed Premises and Offensive Trades are considered appropriate but subject to obtaining approval from the Environmental Protection Authority and the Health Department of W.A.

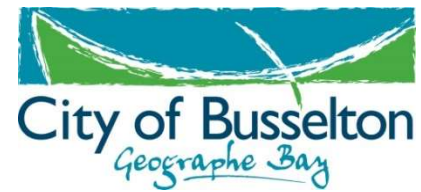
- Abrasive Blast Cleaning
- Concrete Batching Plant
- Crematoria
- Dry Cleaning Premises
- Fibreglass Manufacturing
- L.P Gas Storage
- Leather & Artificial Leather Processing Production
- Sawmills
- Seafood Processors (not demanding on water supplies)
- Service Stations
- Soap & Other Detergents Production

Applications for industrial activities that are not mentioned in the above lists will be considered on a merit basis in respect to the intent of the precinct. No activity which involves food processing or allows for public assembly (which includes recreation facilities) should be constructed within 500m of the lot which contains the W.A Water Authority Sewerage Treatment Plant.

Applications for Prescribed Premises and Offensive Trades are encouraged to apply for lots central to the subdivision to establish as greater buffer as possible to adjacent dwellings. Any activity which will create pollution or distractions of nuisance levels, through odours, mechanical repair,

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noise, etc. are also encouraged to apply for sites which are well away from existing dwellings. If applications are made for activities as described in this paragraph on lots which are adjacent to the existing dwellings, then the proponent may be required to prepare a Social Impact Statement in addition to an Environmental Impact Study to the Environmental Protection Authority's and the City's satisfaction. Applicants are to obtain details of the Department of Environmental Protection Policy on buffer requirements.

### 5.2.4 Development Standards & Guidelines

The development standards are to be in accordance with the general industrial requirements.

### 5.2.5 Parking & Access

The parking and access standards are to be in accordance with the general industrial requirements.

### 5.2.6 Landscaping

Landscaping standards are to be in accordance with the general industrial requirements.

### 5.2.7 Signage

Signage standards are to be in accordance with the general industrial requirements.

### 5.2.8 Other Special Requirements

Building appearance (including facade), fencing, external storage, loading and service areas shall be in accordance with general industrial requirements.

## 5.3 SPECIAL INDUSTRIAL PRECINCT NO. 3

### Strelly Street Light Industrial Precinct

This precinct was formed as a composite industrial park with special development controls. It sets an example of how comprehensive and co-ordinated planning measures can maximise on the site's locational attributes and maintain amenity standards for industrial areas.

The Guide Development Plans are attached (Appendices 5 & 6).

### 5.3.1 Intent of Precinct

The intent of imposing guidelines and controls for this precinct are outlined below:

- (a) To develop a well-designed Industrial Park with uniform development standards that is complementary to its riverside location and attractive to passing traffic along the distributor road, Strelly Street.
- (b) To develop a co-ordinated access arrangement off Strelly Street to foster safe traffic flows by reducing potential conflicts on Strelly Street and creating easy on-site access between industrial lots.
- (c) To provide composite industrial lots which are attractive for construction of high standard caretakers' dwellings due to the riverside location.

### 5.3.2 Application Procedures

Application procedures are in accordance with general industrial development requirements.

### 5.3.3 Development Standards & Guidelines

All development in the Special Light Industrial Development Area is to conform with the requirements set out on the Development Guide Plan. In summary, the Development Standards which have been applied to the Development Guide Plan are the following:

- (a) The following building setback requirements apply to industrial developments:
  - (i) A minimum front boundary setback of 14m
  - (ii) A minimum rear setback of 10m (a nil setback may be considered by the City in certain circumstances). The rear setback shall be taken from the rear cadastral boundary of the lot in the case of lots specifically zoned for industry use only, or the screen fence line for those lots provided with a residential building envelope
  - (iii) A minimum side setback of 5m from one side
  - (iv) The other side to be set back 3m unless a brick parapet wall is used, whereby it can be built on the boundary as per the Building Code of Australia requirements
  - (v) Where a lot has two street frontages, the 14m minimum front setback shall apply to the narrowest frontage of the lot and a minimum setback of 7m shall be applied to the other street frontage.
- (b) Building setbacks for residential buildings have been determined by the location of a building envelope, i.e. 7.5m minimum from the rear boundary, 7.5m from the screen fence and 1.2m from both side boundaries.

No dwelling house shall be constructed or occupied other than in the area denoted as a residential building envelope on the adopted Subdivision & Development Guide Plan.

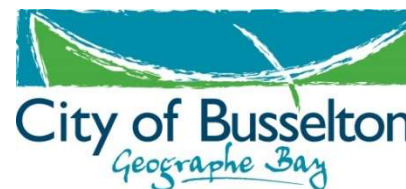
- (c) All other requirements are in accordance with general industrial development requirements.

### 5.3.4 Parking & Access

- (a) All car parking is to be located and the number of car bays provided in accordance with the Guide Development Plan and to be of a bitumen sealed, drained and marked out standard. The parking areas are to link with adjoining lots and therefore construction levels between lots must be the same.
- (b) Dimensions of car parking spaces to be not less than 2.5m by 5.5m with a 6.5m turning area.
- (c) Access to lots will be restricted to those points denoted on the Subdivision & Development Guide Plan. All lot owners will be required to use a joint accessway provided with the adjoining owner and continuous linking of on-site parking areas. Sizes of access points, parking and vehicle manoeuvring areas shall be in accordance with those examples denoted on the Subdivision & Development Guide Plan and to the satisfaction of the City Technical Services Division.
- (d) Designated parking areas are to be freely available to the public at all times.
- (e) Loading bay dimensions and manoeuvring area requirements shall be in accordance with general industrial development requirements.
- (f) Use of parking areas are to be in accordance with general industrial development requirements.
- (g) Crossovers shall be in accordance with general industrial development requirements.

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### 5.3.5 Landscaping

Landscaping along street frontages is to be provided in accordance with the Guide Plan. The minimum width of the landscaping area within the lot shall be 2m or as defined on the Guide Plan.

All other requirements for landscaping are in accordance with general industrial development requirements.

### 5.3.6 Signage

Signage requirements are in accordance with general industrial development requirements.

### 5.3.7 Other Special Requirements

Building appearance (including facade) requirements, lighting and storage areas are in accordance with general industrial area requirements. The fencing requirements are to comply with the following:

- (a) All goods and materials stored on the site should be stored in such a way as to not be visible from the roadway and adjoining residential uses. Appropriate screen fencing is required.
- (b) A screen wall of not less than 1.8m in height shall be constructed on those residential/industrial lots at the time of constructing a dwelling in the position shown on the Subdivision & Development Guide Plan.
- (c) No boundary fence being constructed forward of the building front setback.

## 5.4 SPECIAL INDUSTRIAL PRECINCT NO. 4

### Busselton Airport Industrial Airpark Precinct

#### 5.4.1 Location

This precinct consists of property immediately adjacent to the runways of the Busselton Airport. The airport is located approximately 6km from Busselton Town Centre as shown on the location plan.

The Guide Development Plan is attached (Appendix 7).

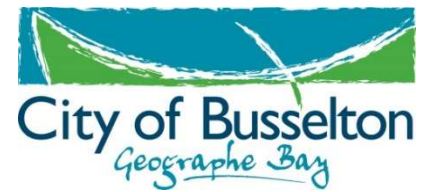
#### 5.4.2 Intent of Precinct

The intent of imposing technical guidelines for the industrial park is outlined below:

- (a) To develop a high standard airpark which is aesthetically pleasing to visitors and employees and creates an environment which will attract capital investment.
- (b) To regulate development to conform with the Development Guide Plan and with uniform development standards which will result in an efficient and safe built environment.
- (c) To ensure that the development is totally compatible with and creates no hazard or nuisance affect to aircraft activity.
- (d) To specify standards for the level type and quality of infrastructure and technical services.
- (e) To regulate ancillary and incidental development eg. signage which will promote high standard streetscape amenity.
- (f) To address landscaping requirements to complement the building development.

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### 5.4.3 Application Procedures

Application procedures are to be in accordance with the general industrial development requirements i.e. making formal application for the City Planning Consent payment of fees and submission of appropriate plans.

### 5.4.4 Permitted Uses

The nature of the land uses/activities and structures that can be established within the airpark shall be those which are related to aviation transportation including those which benefit or support the aviation industry. The permitted uses are restricted to the following:

#### Aviation Precinct

- Aeroplane and helicopter hangers for aircraft storage/parking
- Maintenance and repairs of aircraft.
- Office/administration use connected with air transportation business and service e.g. chartered flights, reconnaissance, air photography, Royal Flying Doctors administration etc.
- Educational/training uses i.e. pilot training.

#### Aviation Support Industry Precinct

- Service industry which is involved in the repair, temporary storage and maintenance of aircraft and including retail trade of aircraft spare parts, lubricants and other aircraft sales activity.
- Service industry which services ground support vehicles which are used in airport activity i.e. emergency vehicles, freighting goods and luggage etc.
- A service industry which services any building or structure in the airport e.g. construction, electrical, navigational etc.

#### Aviation Business Precinct

- Wholesale trade industry which relies on air transportation for freighting or receiving goods, products and produce i.e. warehouses and markets.
- Manufacturing/Processing Industry which processes packages and alters products for freighting by air.
- High technology manufacturing of high value, low bulk items scientific services, laboratory testing services, meteorological research.
- Retail trade businesses which provide for food processing and catering for airpark works and passengers/visitors in transit; aircraft display sales and accessory sales, sale of products/items used in an aviation business or service.
- Educational services in aviation training, including pilot training.
- Government offices and services including a State Emergency Services depot.
- Air transport operator's offices.

### 5.4.5 Development Standards & Guidelines

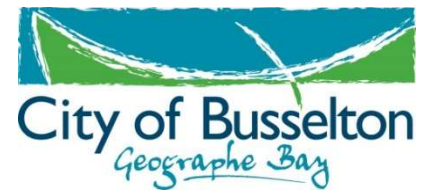
#### Aviation Precinct

All buildings including hangers are to be setback 12 metres minimum from the Road Reserve boundary, 3 metres minimum from the leasehold side boundary from one boundary and 5 metres from the other side boundary. Setbacks to the side boundary may be required to increase

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depending upon the floor space size and bulk of the hanger and the setback requirements for infrastructure and access for the leasehold allotment.

### **Building Design**

All buildings must be aesthetically pleasing and conform with uniform design standards. All buildings are to be built in accordance with the Building Code of Australia requirements and determined as satisfactory by the Federal Airport Corporation. All buildings including hangers built of metal sheeting are not having a reflective value of greater 30% and are to use natural earthy green or brown colours. The facade of the building is to consist of gable roof structure with contrasting colour schemes for gutters, door and window frames. Office components are to have a brick veneer appearance or an alternative appearance which is aesthetically pleasing as determined by the City. The kitchen, office, ablution and public assembly components of the hangers are to be built in accordance with the City's Health regulations.

All buildings are to consist of a concrete floor and the finished floor level is to be no less than 100mm above the crown of the adjoining road. The doorway of the hanger adjacent to the road reserve is to be no less than 2.4 metres in width. A maximum height restriction of 10 metres to the ridgeline of the roof will apply.

### **Services**

#### *Electrical Power*

All applicants shall contribute to the Electrical Power Infrastructure Program which will achieve the establishment of a Western Power transformer site and a reticulated network of underground powerlines.

Applicants shall:

- (a) Provide details of the maximum supply demand figures.
- (b) Install an electrical metre.
- (c) Pay full cost for supplying power.

Temporary or alternative power can be supplied by the developer to the site subject to the City of Busselton approval prior to construction of the hanger. Only licensed electrical contractors are permitted to make terminations or connections.

#### *Water*

As reticulated scheme water will not be available until at least the year 2001 all applicants are required to demonstrate that adequate on-site water supply is available for drinking, cleaning and fire fighting purposes. All applicants will contribute towards the upgrading of a water headworks scheme and towards the installation of an emergency water supply point for fire fighting purposes.

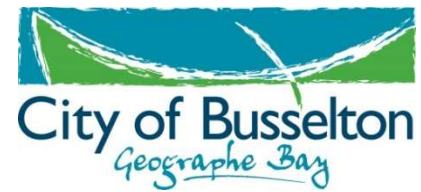
Applicants shall:

- (a) Provide details for water requirements and water tank provisions.
- (b) Install a water meter (by a licensed plumber).
- (c) Pay full cost for supplying water to the site.
- (d) Make allowance for connection to a reticulated water scheme.

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- (e) Provide the City with details of proposals for groundwater abstraction to be determined by the Department of Water. The water tank is to be installed within one of the side setback area.

### **Drainage**

All applicants shall contribute towards the Airport drainage program which will achieve a reticulated piped stormwater drainage system and a stormwater nutrient stripping basin and other flood mitigation works. In the interim all runoff is to be contained within the leasehold allotment. All roof runoff is to be contained on site by tanks or soak wells.

### **Roads**

All applicants shall contribute towards the Airport Road Reserve program which will achieve the construction of a 7.4 metre wide sealed carriageway within the road reserve. The roadway is required to be constructed to the leasehold site prior to the use of the hanger.

### **Effluent Disposal & Treatment**

All applicants shall contribute towards the Airport sewerage scheme which will ultimately achieve the installation of a reticulated vacuum sewer, a pumping station and a treatment plant.

In the interim, all proposals are to be accompanied by applications for septic tanks and leach drains or an effluent disposal system which is to the City's Health Departments requirements. All septic tanks systems are to be contained on the leasehold lot and be setback from boundaries and structures in accordance with Health Department requirements.

### **Telecommunications**

Applicants are required to approach a telecommunication corporation directly for connection. Telecommunications connection is to be provided by underground cable.

### **Taxiways contained within the Lease Boundary**

The cost of providing a taxiway is the developer's responsibility. The width required as per the aircraft type to use the facility. Pavement shall be constructed to a minimum standard of 150mm depth of crushed rock and with 25mm of asphalt.

### **Apron & Taxiways External to Lease**

It is the developer's responsibility to connect to the nearest available apron or taxiway. Developers should also make provision for apron space if required. Unless taken up as a licensed area, the constructed aprons or taxiway become common user. A minimum standard of construction shall be of 150mm depth of crushed rock and 25mm asphalt.

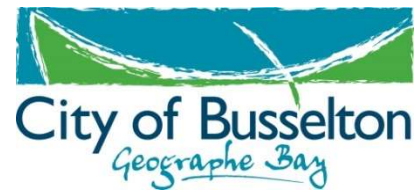
### **Pollution Control**

All waste, oils and fuels must be contained within an approval storage area so as to prevent spillage on to the ground.

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All chemicals including fuels and oils must be stored in a manner conforming to relevant Australian Standards and the City requirements.

All parts and washdown facilities must be constructed to contain all contaminants so that they do not enter the ground water system. All contaminants, both flued and solids shall be stored in an approved tank and removed from site by an approved contractor. All costs to be borne by the responsible organisation.

Aircraft washdown is not permitted on aprons unless an approved facility exists to prevent ground water contamination.

No underground tanks are permitted.

### **Carparking & Access**

Crossovers are to be constructed by the developer to the City's satisfaction. Carparking is to be provided within the front setback area in accordance with the Industrial Development Code requirements. The parking requirement is 1 space per 100m<sup>2</sup> of NLA. Large developments may be required to provide car parking bays for the disabled.

### **Landscaping**

Landscaping is required to be carried out in accordance with the Industrial Development Code requirements i.e. 2m landscape strip along road reserve, maintenance of road verge area, submission of detailed landscape plan.

### **Fencing**

The requirement for an open security fencing shall be 1.8m high black PVC coated galvanised linkmesh and associated with provision of landscaping. The fencing is not to encroach within the front setback area (ie. adjacent to the road reserve). Fencing is optional.

### **External Storage**

No refuse, raw material, machinery or equipment is to be stored outside the hanger.

### **External Lighting**

External lighting for security purposes is acceptable provided it does not interfere or create confusion with lighting associated with night use of the airport itself.

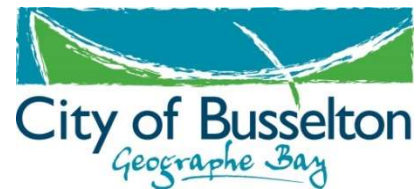
### **Electromagnetic Installations**

Electromagnetic installations are not to interfere or cause disturbance or disruption to the airport's navigational aids, as well as for aircraft navigational aids.

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### Signage

Signage is to be in keeping with the general industrial requirements.

#### AVIATION SUPPORT INDUSTRY PRECINCT

The following elements of development within the Aviation Support Industry Precinct are to comply with the general Industrial Development Code requirements.

- setbacks
- building design and appearance
- car parking and access
- landscaping
- services
- fencing
- external storage
- signage

The remaining elements of:

- pollution control
- external lighting
- electromagnetic installations

are to comply with the standards for the Aviation Precinct.

#### AVIATION BUSINESS PRECINCT

The following elements of development within the Aviation Business Precinct are to comply with the general Industry Development Code requirements.

- setbacks
- building design and appearance
- car parking and access

Generally in accordance with the Industrial Development Code although the parking requirements will differ upon the land use classification and is to be in accordance with the City's car parking code policy.

- landscaping
- services
- fencing
- signage

Fencing along the street boundary is to be restricted to a height of 1m and is to be of a feature design standard. Fencing along the side and rear boundary is to consist of low reflective Colorbond sheeting but not exceeding 1 metre within the front setback area. Fencing is optional and landscaping in its place is acceptable.

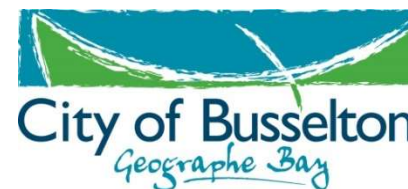
The remaining elements of:

- external storage
- pollution control
- electromagnetic installations

are to comply with the standards of the Aviation Precinct.

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### 5.5 SPECIAL INDUSTRIAL PRECINCT NO. 5

#### Vasse Light Industrial Precinct

This precinct is located and intended as a focus for employment generating activity as a necessary component of the sustainable development of the Vasse Newtown development, which will ultimately accommodate approximately 5000 people. The proximity of the industrial park to the Busselton Bypass, the future Vasse Dunsborough Arterial Road and Bussell Highway allows for it to also play a role in servicing the needs of surrounding localities and to act as a central hub for transport and related operations. The light industrial precinct will therefore fulfil several roles, ranging from development for showroom facilities to agricultural related and conventional light industry. The precinct controls acknowledge this and seek to promote a diverse range of lot sizes and development of a suitable character and style of building form to meet the various roles that the industrial park will play.

#### 5.5.1 The intent of the controls and guidelines for this precinct are described hereunder:

- (a) Provide an appropriate and aesthetic frontage to Franklin Wetland, which lies between the Vasse Yallingup Siding Road and Bussell Highway, and the view scape from Bussell Highway and future important roads.
- (b) encourages the development of high quality showroom/office frontage within the vista from Bussell Highway and other important roads;
- (c) Integrate access and car parking infrastructure within key frontages so as to achieve convenience of use, efficient utilisation of space and access, minimise crossovers and present a seamless 'public' space;
- (d) Preserve the tree-lined avenue environment of Vasse Yallingup Siding Road;
- (e) Provide an appropriate frontage for the establishment of "bulky goods" operations, particularly at the northern and southern extremities of the Industrial Park;
- (f) Supply a diverse range of lot sizes and orientations to meet the various needs of the horticultural/agricultural service industry, storage, processing facilities, batching plants, sawmills, craft/cabinet workshops, transport activities, building products and showroom/bulky goods outlets and other facilities likely to occupy the precinct, and
- (g) Ensure suitable characteristics exist for the successful operation of the precinct in terms of access, parking, servicing, commercial presentation, and general amenity.

#### 5.5.2 Application Procedures

The application procedures are in accordance with general industrial development requirements.

#### 5.5.3 Land Uses

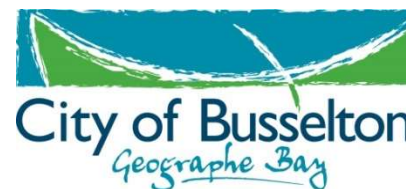
The nature of commercial and industrial activities and structures are set out in the Scheme. The location of light industry, industrial and showroom/bulk store uses shall be in accordance with the land use designations depicted on the DAP.

#### 5.5.4 Additional Design Standards & Guidelines

The following sub-clauses contain guidelines and established standards additional to the requirement for general industrial development and should be read in conjunction with those requirements.

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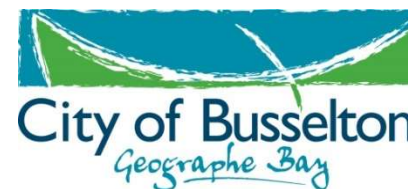


### 5.5.5 Building setbacks, articulation, orientation & design

- a) A minimum building setback of 6m is required from the common boundary of a site to an area of Public Reserve land, but not including roads.
- b) Minimum boundary setbacks in accordance with clause 3.2 of this Policy will apply to all lots not covered by the specific requirements of the Detailed Area Plan. Development of lots for which standards are specified in the DAP shall be in accordance with those specifications.
- c) Buildings having a facade which is visible from Vasse Yallingup Siding Road, Bussell Highway, the western arterial link road and/or Vasse to Dunsborough Arterial Road and/or showroom uses should be designed, sited and orientated to present a high quality commercial facade which incorporates colour, overhangs, verandah's, sheltering devices and/or other architectural features to create visual interest. These facades shall incorporate a minimum of 80% of the frontage of the development comprising major openings (a major opening is defined as a commercial window or visually permeable door(s) with a minimum vertical dimension of 2.1 metres). Where such buildings have frontage to a secondary street, the frontages for these buildings should be similarly treated to address the secondary street.
- d) The front elevation should be designed to address the street and to provide a corporate image and an inviting and legible entrance. To achieve this, the facade could include significant areas of glass, with overhangs or sheltering devices. Entrance points to buildings are to be designed as focus points and should provide shade/protection for pedestrians.
- e) Within the Showroom/Bulk Store use precinct as depicted on the DAP Concrete tilt slab walls that are visible from the street shall be either painted, rendered or covered with colourbond or timber cladding to the satisfaction and specifications of the City.
- f) Where a building is proposed on a corner lot, the secondary façade shall also include significant areas of glass window openings, with overhangs, sheltering devices and other architectural features to create visual interest.
- g) An enclosed storage/outdoor area shall be provided for individual units/tenancies with a minimum dimension of 24 sqm/unit, for any group of two or more individual tenancies on any parent lot, to the specification and satisfaction of the City. The fencing should preferably be constructed of colourbond or painted metal deck material (maximum 2.4 m high) but in any event be capable of screening the outdoor area from public view. At least 60 % of the required area shall be maintained without roof cover.
- h) The use of verandah's, canopies or colonnades on the front elevation of buildings is encouraged.
- i) Where more than one building is proposed for a site, the design and form should present an integrated approach.
- j) Sundry structures such as bin enclosures, electrical substation and an integrated signage board for units/tenancies shall be designed in a style and finished to complement the major site building. Appropriate landscaping of these structures will be required where they are proposed to be located forward of the building line.
- k) All exposed mechanical plant, ductwork, conduits and the like shall be positioned and coloured to minimise visual impact.
- l) Fencing of the property forward of the Building line will require the assessment and approval of the City. Fencing forward of the building line is not permitted within the Showroom/Bulk Store use precinct.

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- m) A colour palette indicative of the preferred combinations of colours appropriate to us within the Vasse Light industrial area may be attached as a schedule to this Policy for guidance purposes only.

### 5.5.6 Parking & Access

- a) All car parking is to be located and the number of car bays provided in accordance with Clause 5 of this Policy or as per the DAP where applicable. The parking areas designated on the DAP are to link with adjoining lots and therefore construction levels between lots must be the same.
- b) Parking and reversing design standards shall be in accordance with the City's Car Parking Policy except for developments within the integrated parking and access precinct which shall be provided in accordance with the detail 1 on the DAP.
- c) Access to lots will be restricted to those points denoted on the DAP. The dimensions and design of access points, parking and vehicle manoeuvring areas shall be in accordance with the DAP and be constructed to the satisfaction and specification of the City.
- d) Joint access ways between the parking areas on individual lots are to be shared to enable continuous linking of on-site parking areas. In the event that an integrated access and parking area is not constructed in accordance with a DAP at the time of the initial subdivision, each landowner may be required to construct practical access/egress to the parking area from the street as an interim arrangement until such time as integrated access and parking can be completed. This will result the interim crossover being removed, at the expense of the developer(s), once a shared joint access way is provided.

The City may require the securing of the continued use of joint access ways by a legal instrument and/or condition of subdivision.

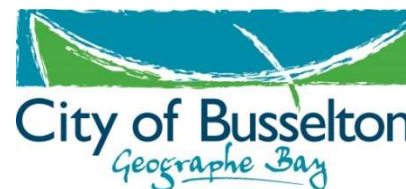
- e) Designated customer parking areas are to be freely available to the public at all times. The use of parking bays for storage will not be permitted.
- f) Loading bay dimensions and manoeuvring area requirements shall be generally in accordance with general industrial development requirements specified at clause 5.1. However in instances where the type of use proposed dictates otherwise, the loading bay shall be designed for the type of use proposed and allow space/turning area for loading/unloading by automotive forklift where necessary.
- h) Use of parking areas are to be in accordance with general industrial development requirements. (refer to clause 5.3 of this Policy)
- i) Crossovers shall be constructed to the specifications and satisfaction of the City in accordance with general industrial development requirements (refer to clause 5.4 of this Policy)

### 5.5.7 Landscaping

- a) All areas of a site that are not built upon or paved and are visible from roadways and public areas shall be suitably landscaped with predominantly local native species.
- b) Lot owners are encouraged to take responsibility for the landscaping and maintenance of the street verge fronting their property on an integrated basis with adjoining properties or in accordance with an Approved Landscaping Plan.
- c) Where significant areas of customer parking are provided forward of the building setback line, these shall be integrated within landscaping to reduce the visual impact and provide a physical separation from the building.

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- d) Water wise landscaping and the planting of landscaped areas with native species and low water use/low maintenance species such as native groundcovers is encouraged.

### 5.5.8 Signage

- a) The objective of signage is to permit adequate identification and business advertising and ensure that all businesses have the opportunity of reasonable exposure.
- b) Signs attached to buildings shall be designed as an integral part of the building, i.e. recessed into the facade, contained within a frame, or hanging from an awning, verandah or colonnade.
- c) Sign writing directly onto the shopfront window is not generally permissible. Where sign writing is proposed directly on to the to the façade it is to occupy no more than 20% of the façade area and be consistent with the corporate image and professional standard expected within this area (refer to Local signage Law with the Building Department)
- d) A detail on the graphics of all signage is to be supplied when application is made for a sign licence and will be considered on its merits.
- e) Trade displays associated with a land use shall not dominate the public appearance of the site or compromise the landscaping/car parking design and will be considered on an individual basis in the context of each application. In any event, trade displays should be set back at least 3m from any site boundary. Trade display for the purposes of this provision shall mean premises used for the display of trade goods and equipment for the purpose of advertisement.

## 5.6 SPECIAL PRECINCT NO. 6

### Dunsborough Enterprise Park

#### 5.6.1 Introduction

The following design guidelines have been prepared to ensure that service industry and other compatible uses within the Dunsborough Enterprise Park are developed in a manner that has minimal impact on the amenity of the surrounding residential area whilst maintaining a high quality appearance.

- a) The design guidelines set out the minimum standards and conditions under which land use and development within the Dunsborough Enterprise Park will normally be applied.
- b) Land use and development in accordance with the guidelines is deemed to comply and subject to planning consent being granted by the City. Variations to the guidelines are permitted subject to demonstration that the proposed development is in keeping with the intent of the guidelines.

#### 5.6.2 Application of Guidelines

The design guidelines apply to the Service Industry Area identified in Detailed Local Area Plan (DLAP) 6 which has been adopted pursuant to Local Planning Scheme No. 21 ('Scheme'). The design guidelines form part of DLAP 6 adopted by the City and endorsed by the Western Australian Planning Commission pursuant to the relevant special provisions of the Scheme.

The guidelines also form part of the City's Industrial Development Code – Special Precinct No. 6 and are enforced as Local Planning Policy under the Scheme. Where the special precinct provisions

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vary from the general policy requirements of the Industrial Development Code, the special precinct provisions in respect of the Dunsborough Enterprise Park shall prevail.

### 5.6.3 Permitted Uses

DLAP 6, with reference to the Town Planning Scheme, identifies the uses permitted in the Dunsborough Enterprise Park and those uses which may be approved by the City. All development or change of use within the Dunsborough Enterprise Park will require approval in accordance with these provisions.

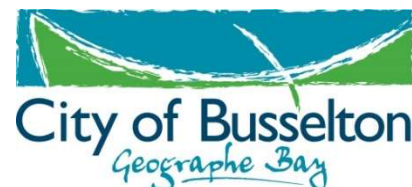
### 5.6.4 Design Guidelines

The following guidelines apply to all proposed development and land use within the Service Industry area as identified in DLAP 6, according to the design areas identified below:



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### Development Standards

Lots	Service Display - Industry	Service General - Industry	Service Core - Industry
Front Setback	27 metres	4m	8m <sup>3</sup>
Rear Setback	2 metres	3m	8m
Side Setback	Nil	Nil	Nil
Site Coverage	65%	65%	70%
Height	10m from NGL	10m from NGL	10m from NGL
Parking	1 car bay per 40m <sup>2</sup> of NLA plus disabled parking as required. <sup>1</sup>	1 car bay per 40m <sup>2</sup> of NLA plus disabled parking as required. <sup>1</sup>	1 car bay per 40m <sup>2</sup> of NLA plus disabled parking as required. <sup>1</sup>
Loading bays	1 per development <sup>2</sup>	1 per development <sup>2</sup>	1 per development <sup>2</sup>

<sup>1</sup> A financial contribution may also be required under the provisions of DLAP 6.

<sup>2</sup> 1 loading bay per lot shall be provided unless it can be demonstrated to the City that such provision is not required for the proposed use.

<sup>3</sup> For lots fronting Inverness Avenue, car parking is to be provided in front of the building – refer provisions below.

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### Provisions

#### General

The following general provisions apply to all design areas within the Service Industry Area:

Subject	Provisions
Car Parking	Design details and standards for the provision of the required parking bays, turning areas and associated access ways are to be in accordance with the requirements of 8 – General Development and Process Standards Policy (8A: Car Parking Provisions).
	Parking areas and vehicle access ways are to be paved and drained to City specifications and these areas are not to be used for storage of goods, rubbish and equipment or for industrial or automotive purposes such as wrecking and repair.
Fencing	Solid fencing or chain mesh will not be permitted to be constructed at any point forward of the building line without approval from the City.
	All side and rear fencing shall be located on the lot boundary at a maximum height of 1.8m with black PVC coated galvanized linkmesh (including black PVC coated poles).
	Where a boundary forms secondary street frontage then landscaping along the inside fence line will be required.
Signage	The provision of signage is to be consistent with the requirements of the City's Signs, Hoardings & Billposting Local Laws and/or Sign Policy.
	All signage, including flush wall signs, fascia sign, awning signs and pylon signs are generally acceptable and will be subject to individual merit based assessment.
	Use of flashing/revolving or other moving signs, roof signs, sandwich boards, roster signs and billposting are generally not acceptable.
Landscaping	To assist in achieving a common theme within the Dunsborough Enterprise Park and for the mutual benefit of landowners and tenants the establishment and maintenance of suitable landscaping (including the verge) is required as follows; a) Grass/lawn (automatically reticulated and regularly mowed and weeded). b) Native garden beds, auto reticulated and regularly weeded and tended.
Street Trees	No street trees can be removed without the prior approval of the City.
	Crossovers must not interfere with existing street trees. Where a street tree is required to be removed for development, written approval must be obtained and the costs shall be borne by the owner. Street trees which have been removed shall be replaced with an equal number of trees of the same species, located to the satisfaction of the City.
External	Open yard areas visible from a street shall not be used for

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Storage/ Use of Outdoor Space	storage of goods or for vehicle repair and industrial activities. Such areas must be screened from public view either with landscaping or by a closed wall-fence to be constructed of materials approved by the City.
Building Materials and Facades	<p>Building materials, colours and window glazing shall have low reflective qualities to ensure that glare does not impact on adjacent properties or public areas, including roads and laneways. All development shall comply with 3 – Special Character Areas and Visual Management Policy (3F: Reflective Building Materials).</p> <p>All parapet walls are to be constructed of materials similar to the rest of the building.</p> <p>To improve street facades, The City will discourage the use of face brick or concrete tilt panels in large expansive areas without relief by way of major openings or other measures to diminish building bulk.</p> <p>The City may require that all expansive areas of raw concrete to be painted in a colour which complements the building.</p> <p>Facades to buildings should be designed with streetscape sensitive features such as large windows, eaves, awnings, verandahs and gables in order to enhance the overall streetscape and development quality within the locality.</p>

Design Area One: Service Industry – Display:

Subject	Provisions
Setbacks	<p>The front setback area may be used for trade display purposes where car parking is not required and upon granting of planning consent by the City.</p> <p>Where trade display is permitted within the front setback area the minimum setback may be reduced to 22 metres.</p> <p>Setback areas shall not be used for the parking of vehicles which are being wrecked or repaired, the storage of materials, products, by-products or wastes or the storage of fuel, except in underground tanks.</p> <p>Nil side setbacks may be accepted on both sides of a lot subject to sufficient loading and unloading bays being provided at the rear. Where this cannot be achieved a side setback of 5 metres on one side of the building shall apply.</p>
Car Parking	<p>All delivery and loading areas shall be confined to the side or rear of the lot.</p> <p>Customer parking is to be provided within the Commonage Road setback area adjacent to the laneway.</p> <p>Where more extensive parking is required for larger vehicles and other machinery it shall be located at the side or rear of buildings. This generally applies to staff and service vehicle parking as well.</p>
Access	<p>Access to lots for service industry vehicles shall only be permitted from the rear service road.</p> <p>Public access to lots will be provided via the reciprocal rights of access easement at the front of each lot.</p>
Fencing	Fences or walls placed forward of the building line shall be of masonry and open grill construction (e.g. masonry piers and steel

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	picket infill) and a maximum of 1m in height.
Signage	Location of signage within the front setback area will require the approval of the City.
Landscaping	A landscaping strip at an average of 1 metre should be provided between all car parking areas and common boundaries to adjoining lots and between any rear parking areas and the verge.
	The landscape strip adjacent to Commonage Road is to be maintained by the adjoining lot owner/occupier to satisfaction of the City.
	The 20 metre Landscape Strip to Commonage Road may be reduced by 5.5 metres to accommodate additional car parking (to be provided by landowners) where the City is satisfied that reciprocal access can be maintained to adjoining lots in an appropriate manner. A landscaping buffer of 14.5m is to be retained. Infill planting may be required to ensure this reduced buffer area can retain its effectiveness.
Building Materials and Facades	All facades to buildings that face the laneway shall be constructed in brick, concrete and masonry. Other materials may be considered where it can be demonstrated that required visual standards can be achieved. Metal cladding (including colorbond®) will generally not be accepted.
	Where the main building height exceeds 4m in height, the front shall be single storey with at least one element that rises above the eaves line to break the facade. This may be a roof gable, a tower or raised parapet and may carry suitably scaled signage.

### Design Area Two: Service Industry General:

Subject	Provisions
Setbacks	The rear setback of Lots 1155, 1156, 1159, 1161 & 897 – 901 may be further reduced to a nil setback where deemed appropriate and subject to approval by the City.
	Setbacks to Service Industry buildings on corner lots may be reduced to 1.5 metres.
	Rear and side setbacks may be interchanged where deemed appropriate.
	Setback areas shall not be used for the parking of vehicles which are being wrecked or repaired, the storage of materials, products, by-products or wastes or the storage of fuel.
	Nil side setbacks may be permitted on both sides subject to sufficient parking, delivery, unloading bays and storage areas being provided on-site. Where this cannot be achieved a side setback requirement of 5 metres on one side of the building shall apply.
Access	Where a lot is serviced by a rear access away continuation of traffic movement may be permitted.
Fencing	Fences or walls placed forward of the building line shall be of masonry and open grill construction (e.g. masonry piers and steel picket infill) and a maximum of 1.8m in height.
Landscaping	A landscaping strip at an average of 1 metre is required for the front boundary, 1 metre on the side boundaries within frontage areas and 1 metre between rear parking areas (including the interface with the verge).

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Building Materials and Facades	All facades to buildings that have road frontage shall be constructed in brick, concrete or masonry. Other materials may be considered where it can be demonstrated that required visual standards can be achieved. Metal cladding (including colorbond®) will generally not be accepted.
	All buildings shall be designed with frontage to the primary setback area.

### Design Area Three: Service Industry Core:

Subject	Provisions
Setbacks	The primary and secondary street setback for Service Industry buildings on corner lots may be reduced to 1.5 metres with City approval.
	Setback areas shall not be used for the parking of vehicles which are being wrecked or repaired, the storage of materials, products, by-products or wastes or the storage of fuel.
	To increase the setback of development to Inverness Avenue for Lots 915 to 923, parking is to be provided fronting Inverness Avenue.
Car Parking	To increase the setback of development to Inverness Avenue for Lots 915 to 923, parking is to be provided fronting Inverness Avenue.
Access	Continuation of traffic movement to the rear access way may be permitted.
	To encourage fewer crossovers to Inverness Avenue and facilitate additional on street parking, reciprocal access and parking across neighbouring lots is encouraged for Lots 915 to 923.
Fencing	Fences or walls placed forward of the building line shall be of masonry and open grill construction (e.g. masonry piers and steel picket infill) and a maximum of 1.8m in height.
Landscaping	A minimum landscaping strip of 3 metres is required for the front boundary and 1 metre on the side boundaries within frontage area and 1 metre between rear parking areas (including the interface with the verge).
	The minimum 3 metre landscaping strip for the front boundary of Lot 914 and 915 may be reduced to an average of 1 metre.
	The front landscaping strip on Lots 915 to 923 shall incorporate planting of mature trees (minimum 2m specimens).
Building Materials and Facades	All facades to buildings that have road frontage shall be constructed in brick, concrete or masonry. Other materials may be considered where it can be demonstrated that required visual standards can be achieved. Metal cladding (including colorbond®) will generally not be accepted.
	All buildings shall be designed with frontage to the primary setback area.
	All buildings on Lots 915 to 923 that front Inverness Avenue shall not exceed a maximum height of 7.5 metres from natural ground level within the initial 5 metres of the façade.
	All facades to buildings on Lots 915 to 923 must demonstrate articulated building design that is compatible with a residential style.

## 6. REVIEW DETAILS

Review Frequency		2 yearly		
Council Adoption	30/1/2013		C1301/017	
Previous Adoption	10/3/2010		C1003/069	

\* Policy number changed from LPP 7B to LPP 2.2 on the 11th May 2020. The change is administrative only, no resolution by Council required.



City of Busselton  
Geographie Bay

## LOCATION PLAN

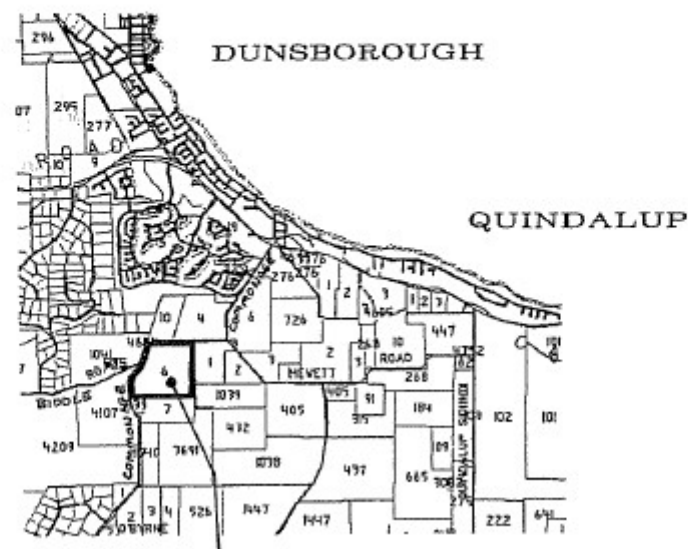
The map illustrates two industrial precincts. The top section, labeled 'STRELLY STREET LIGHT INDUSTRIAL PRECINCT', shows a grid of industrial plots with various street names like 'Strelly Street', 'Preston Street', and 'Barton Street'. A specific site is highlighted with a black dot and a line pointing to it from the bottom. The bottom section, labeled 'BUSSELTON SHOWROOM INDUSTRIAL PRECINCT', shows a larger area with a river and a road. A specific site is highlighted with a black dot and a line pointing to it from the bottom. The map is labeled 'STRELLY STREET LIGHT INDUSTRIAL PRECINCT' at the top and 'BUSSELTON SHOWROOM INDUSTRIAL PRECINCT' at the bottom.

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## INDUSTRIAL DEVELOPMENT

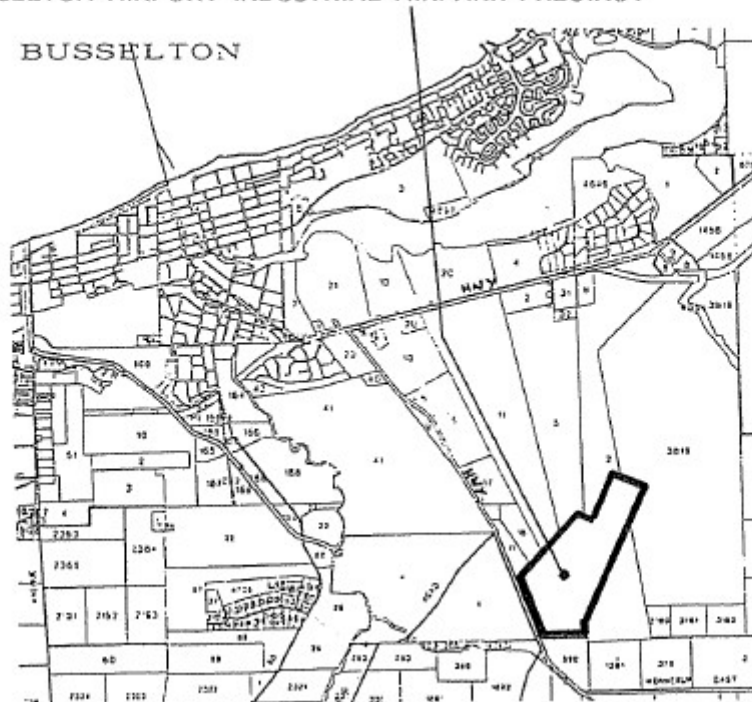
### APPENDIX 2

#### LOCATION PLAN



DUNSBOROUGH SERVICE AND TRADES INDUSTRIAL PARK PRECINCT

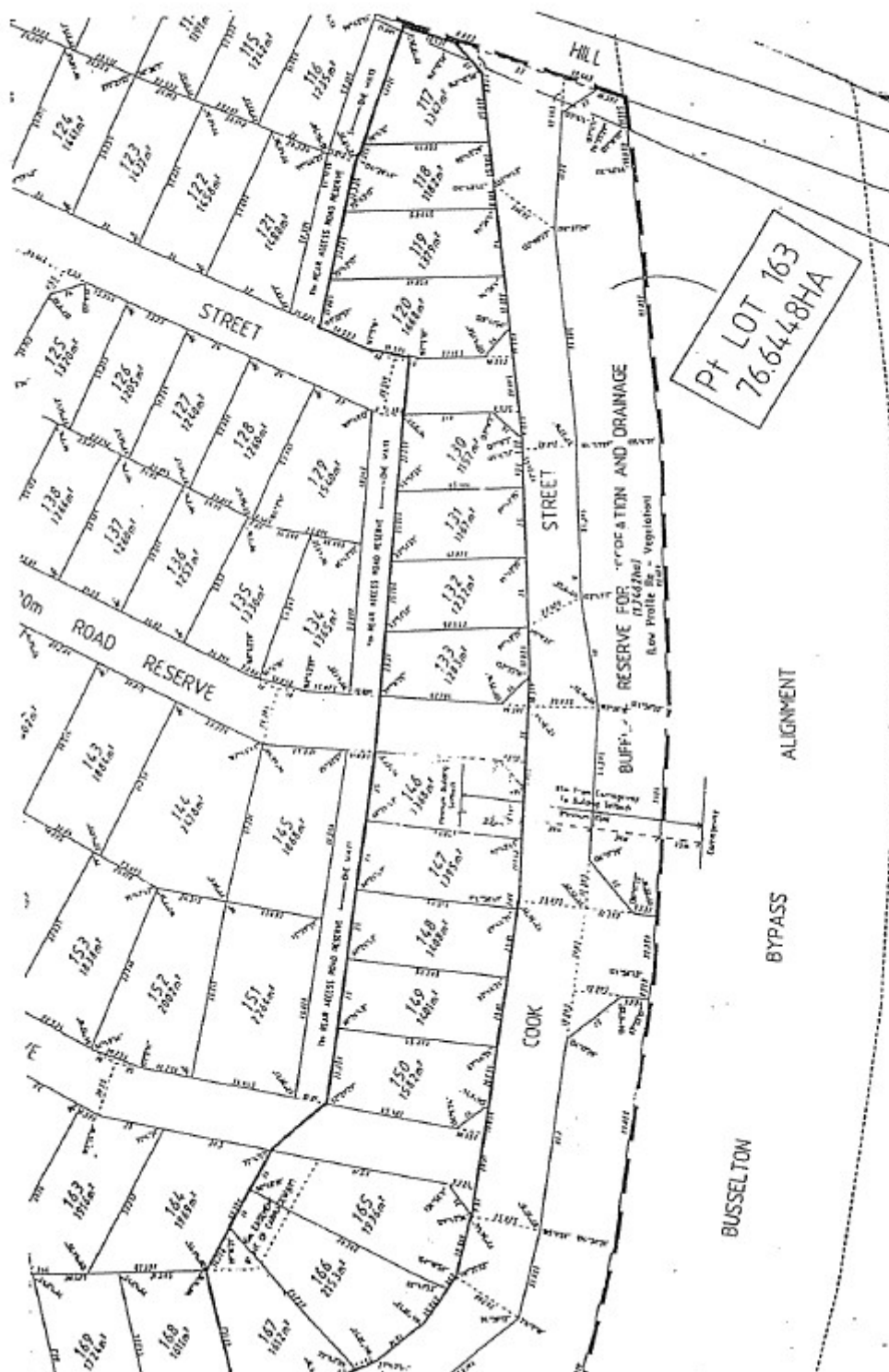
#### BUSSETON AIRPORT INDUSTRIAL AIRPARK PRECINCT



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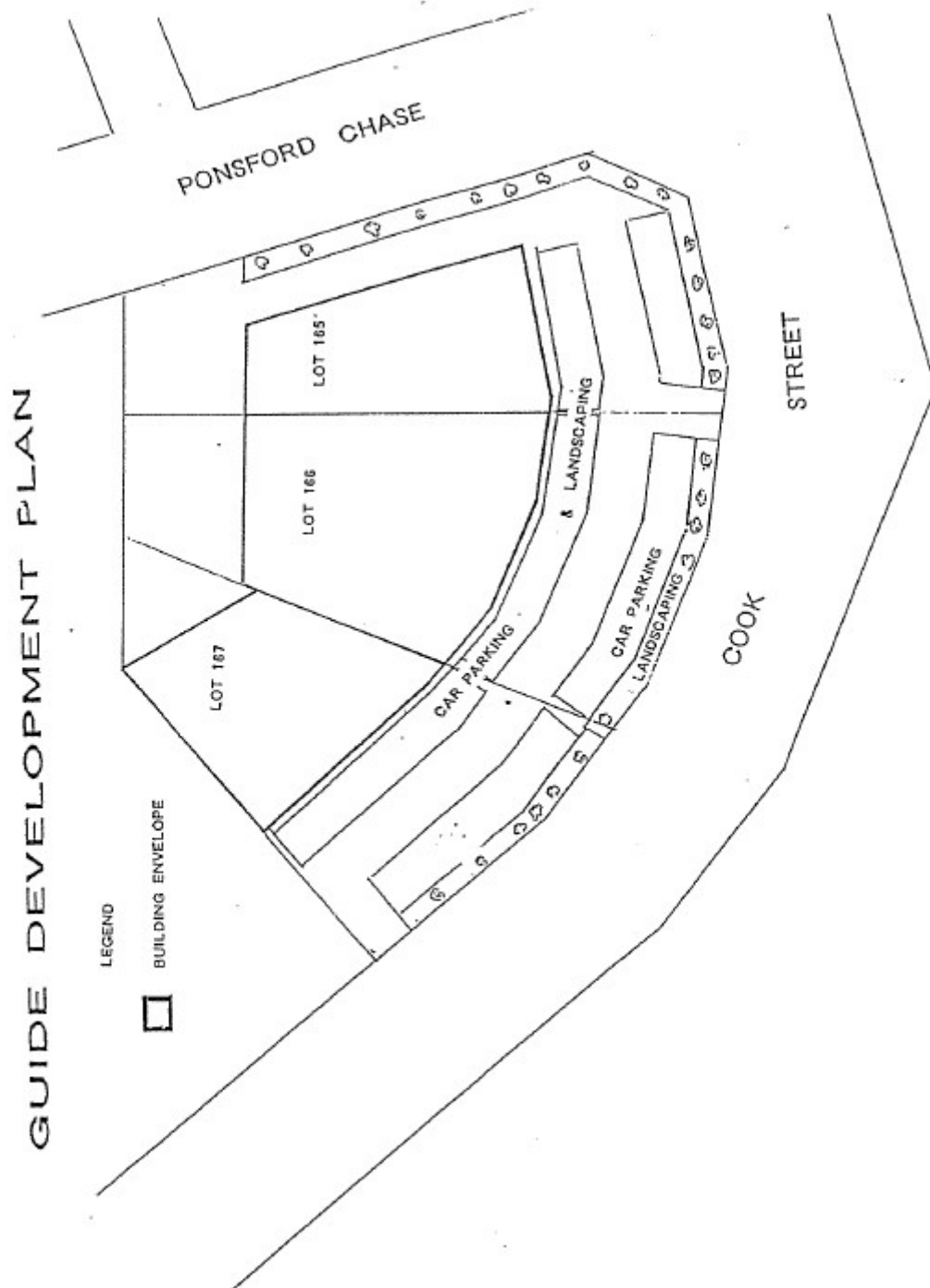
## APPENDIX 3

### BUSSELTON SHOWROOM INDUSTRIAL PRECINCT



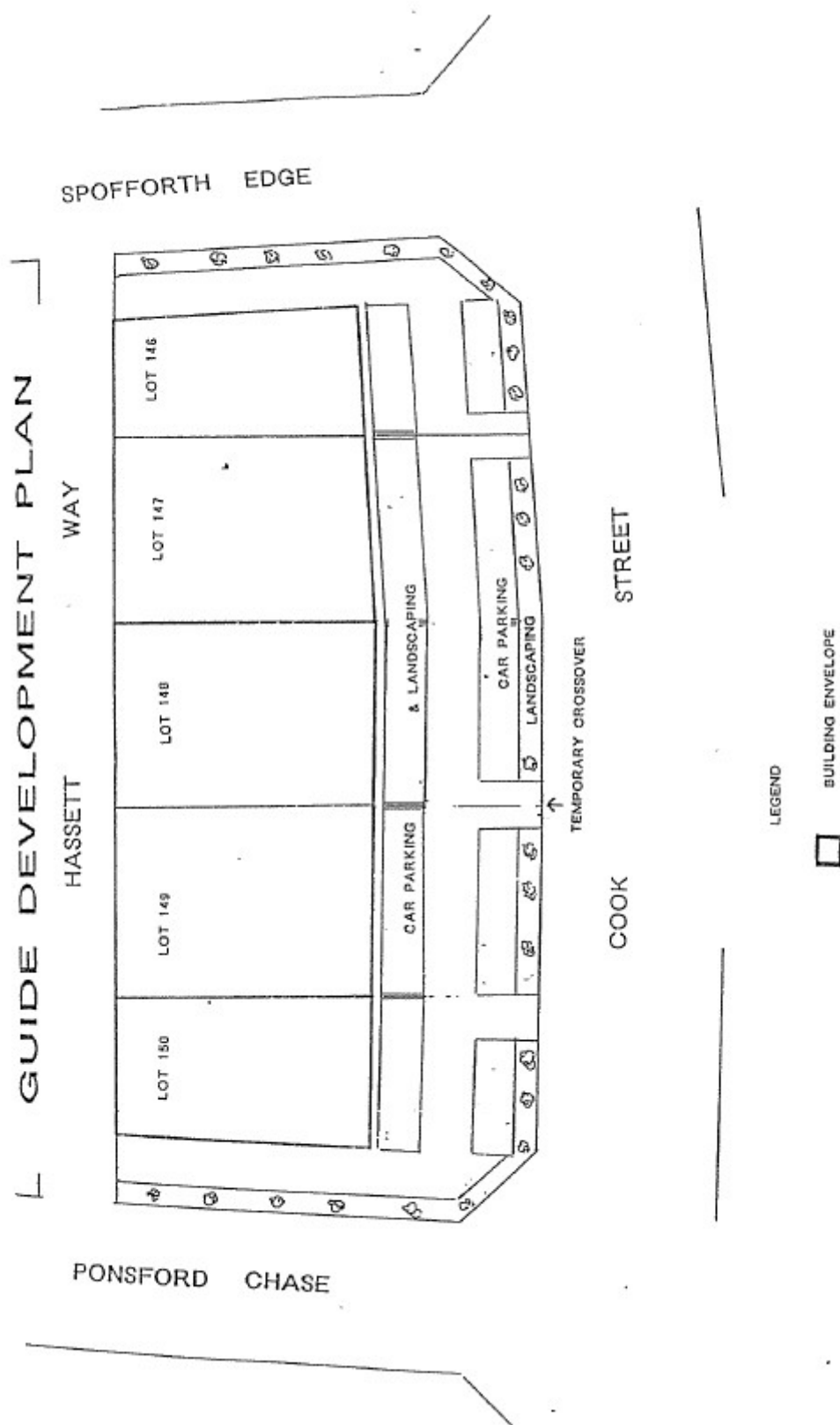
# Local Planning Policy No. 2.2 INDUSTRIAL DEVELOPMENT

## APPENDIX 3A



# Local Planning Policy No. 2.2 INDUSTRIAL DEVELOPMENT

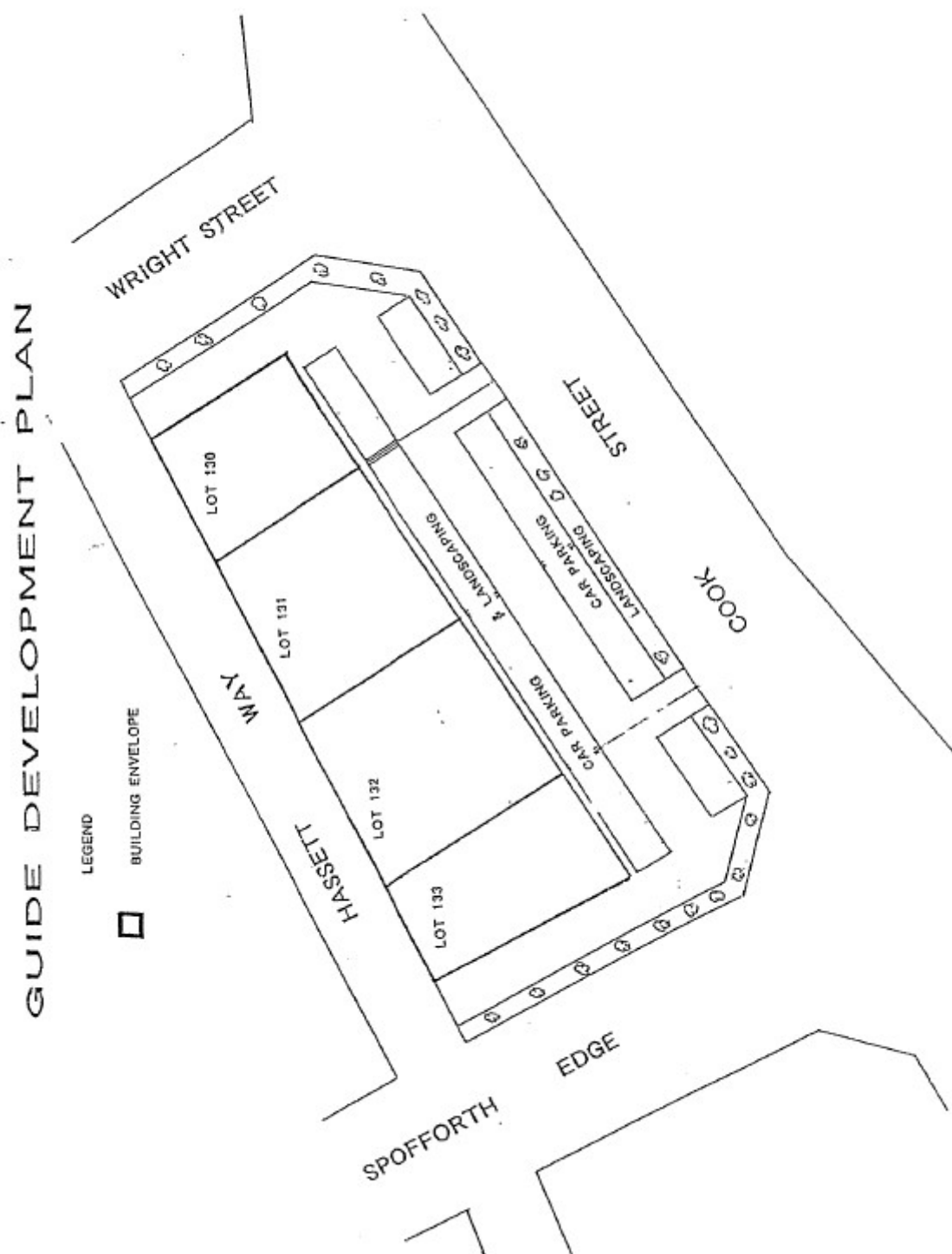
## APPENDIX 3B



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## INDUSTRIAL DEVELOPMENT

### APPENDIX 3C

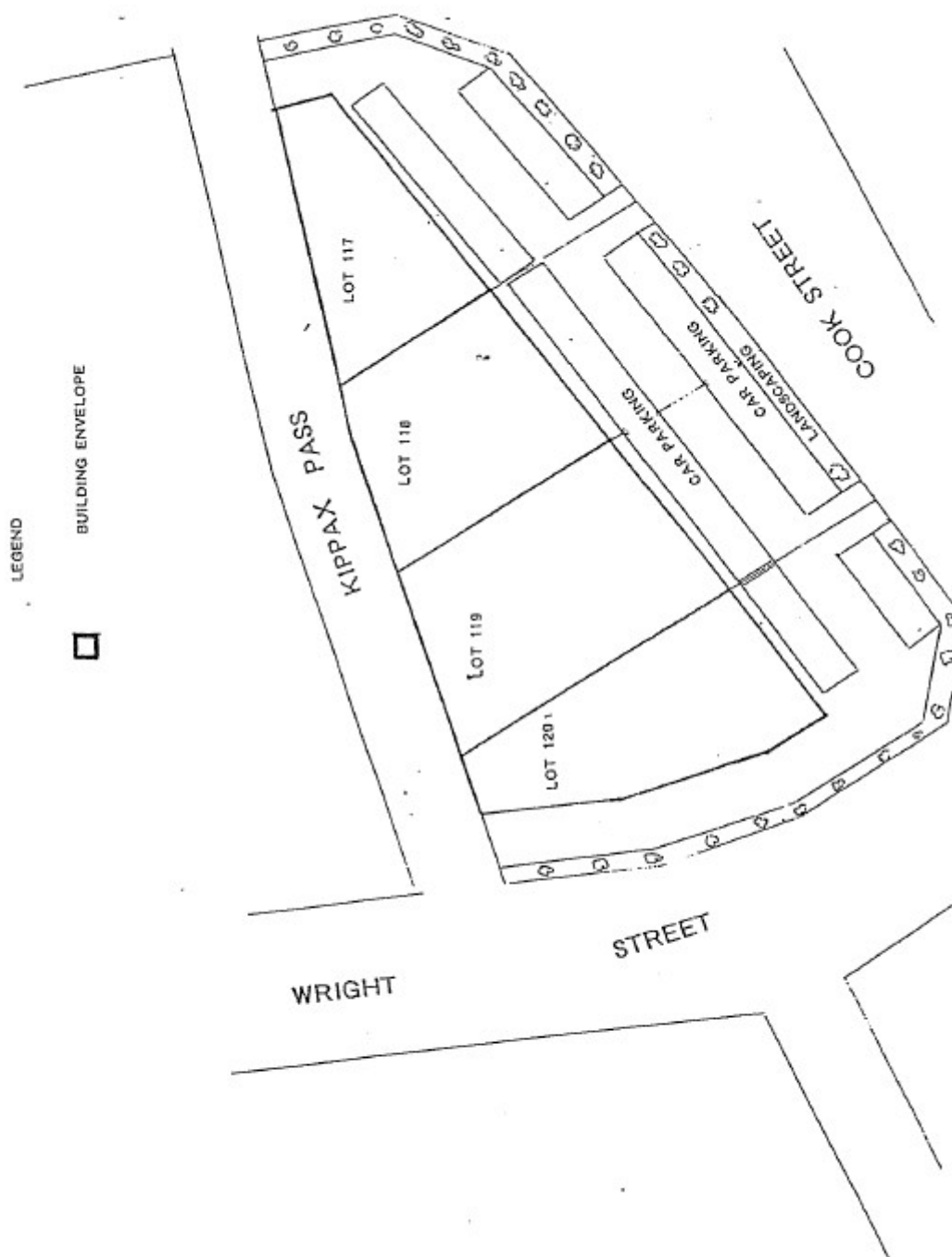


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## INDUSTRIAL DEVELOPMENT

### APPENDIX 3D

#### GUIDE DEVELOPMENT PLAN





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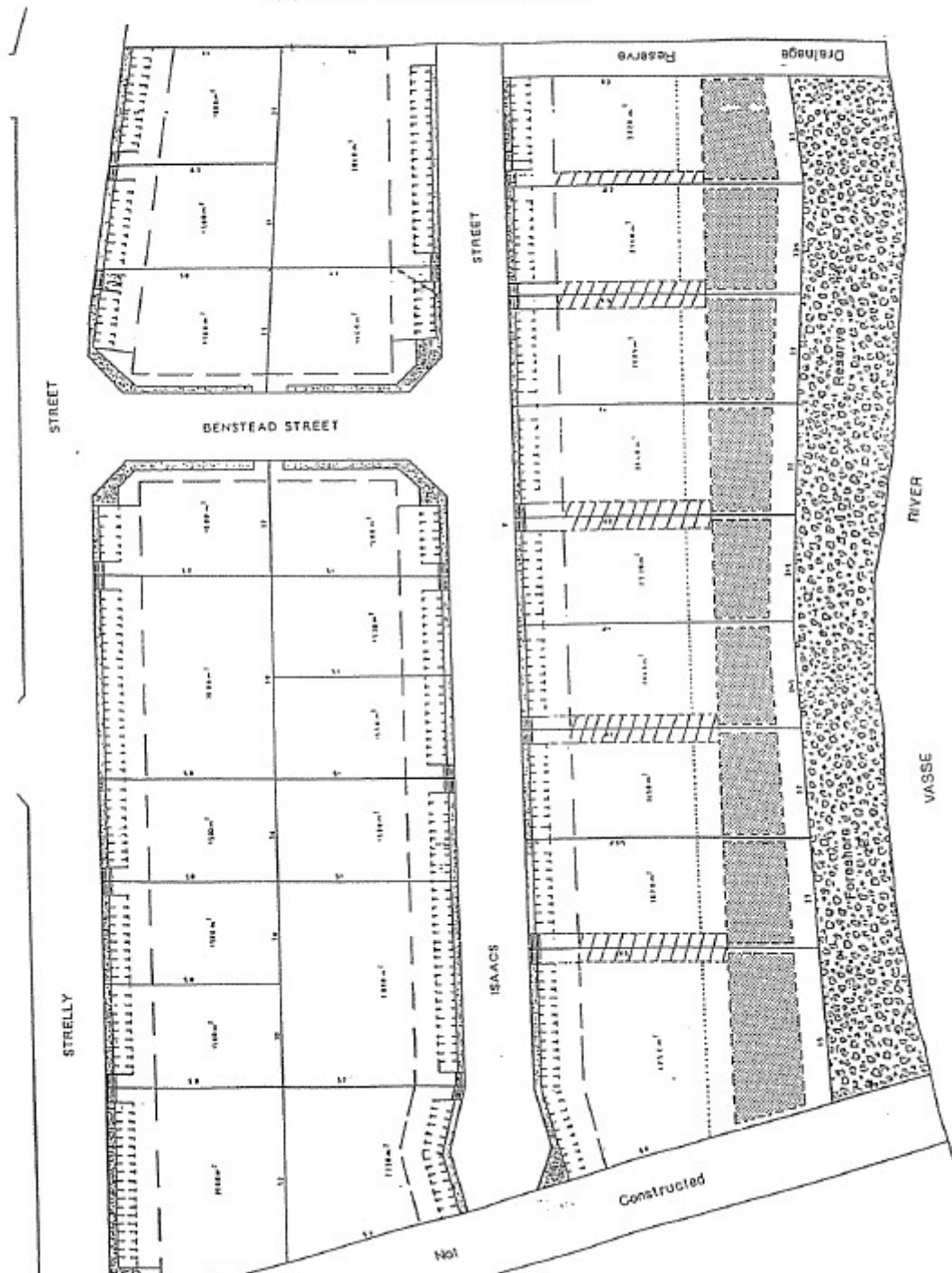
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## APPENDIX 5

### STRELLY STREET LIGHT INDUSTRIAL PRECINCT

#### GUIDE DEVELOPMENT PLAN



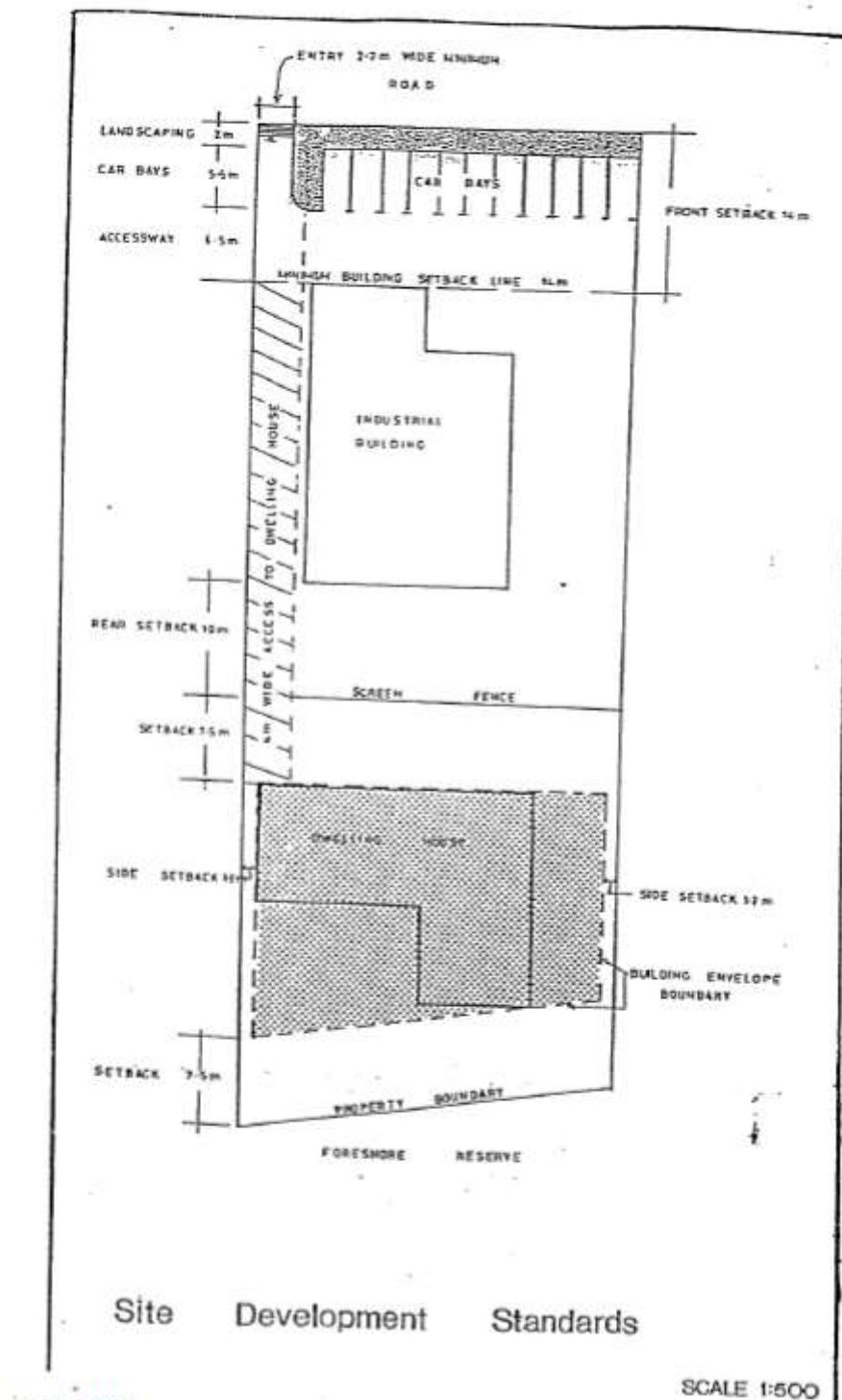
# Local Planning Policy No. 2.2

## INDUSTRIAL DEVELOPMENT

### APPENDIX 6

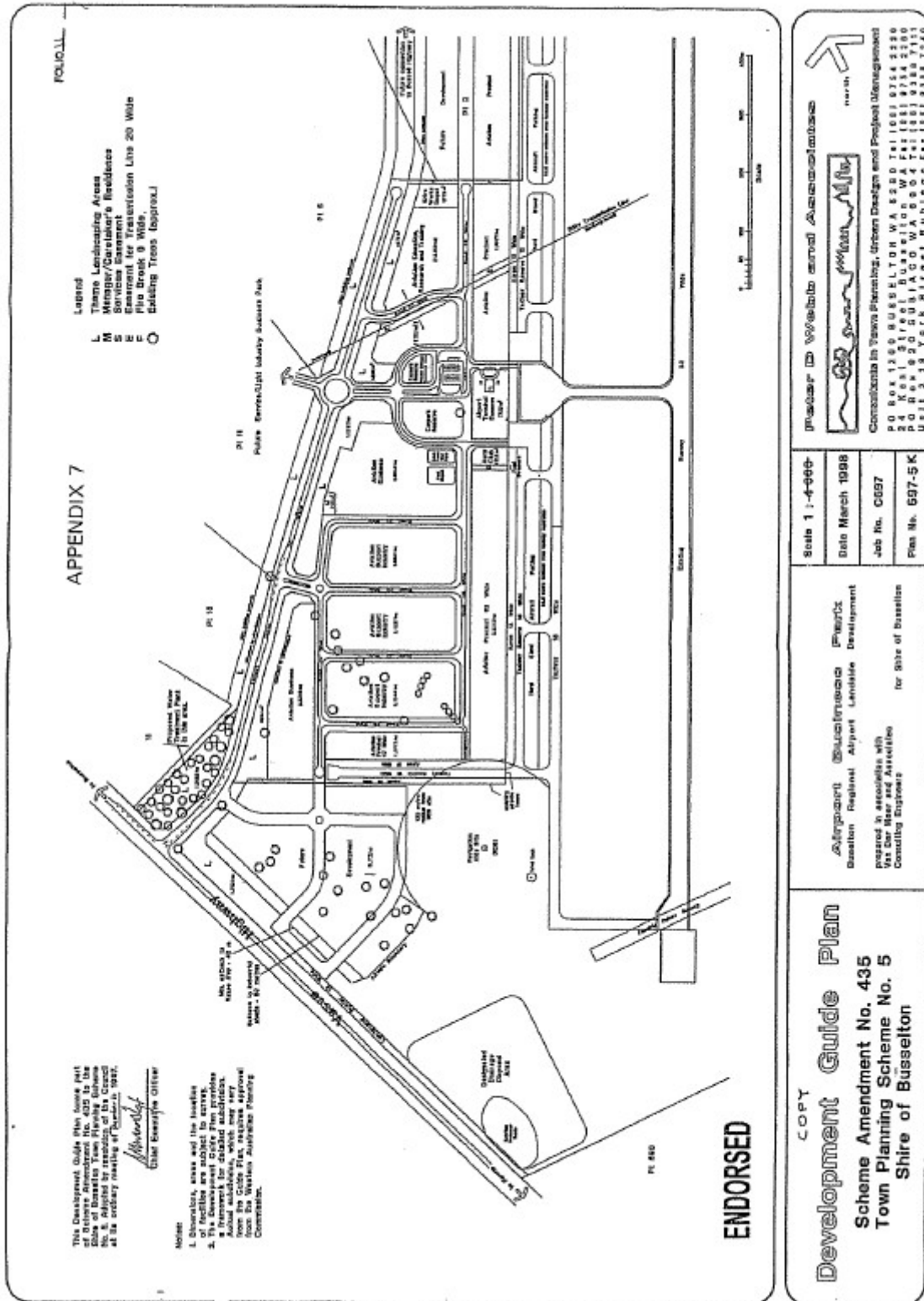
#### STRELLY STREET LIGHT INDUSTRIAL PRECINCT

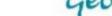
#### GUIDE DEVELOPMENT PLAN



# Local Planning Policy No. 2.2 INDUSTRIAL DEVELOPMENT

## APPENDIX 7





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